



SUBDIVISION REPORT - CONCEPT

▶ **FILE #:** 3-SC-19-C **AGENDA ITEM #:** 6
 POSTPONEMENT(S): 4/11/2019 **AGENDA DATE:** 5/9/2019

▶ **SUBDIVISION:** HISTORIC MIDDLEBROOK PRESERVATION TRUST

▶ **APPLICANT/DEVELOPER:** MARK A. BIALIK / GBS ENGINEERING

OWNER(S): Rick Fox / Historic Middlebrook Preservation Trust, Inc.

TAX IDENTIFICATION: 93 K B 002 [View map on KGIS](#)
 JURISDICTION: City Council District 6
 STREET ADDRESS: 4001 Middlebrook Pike

▶ **LOCATION:** East side of Ed Shouse Dr., West side of I-640, North of Middlebrook Pk.

SECTOR PLAN: Northwest City
 GROWTH POLICY PLAN: Urban Growth Area (Inside City Limits)
 WATERSHED: Third Creek

▶ **APPROXIMATE ACREAGE:** 58.42 acres

▶ **ZONING:** C-6 (General Commercial Park) / F-1 (Floodway)

▶ **EXISTING LAND USE:** Vacant land

▶ **PROPOSED USE:** Business / warehouse development

SURROUNDING LAND USE AND ZONING: This property is located in the industrial and manufacturing area along Middlebrook Pike in the C-6 and I-3 zones. The property is part of the historic Middlebrook property. The historic house (circa 1845) and old barn structures are located in the A-1 zoned property to the south.

▶ **NUMBER OF LOTS:** 2

SURVEYOR/ENGINEER: Mark A. Bialik / GBS Engineering

ACCESSIBILITY: Access is via Ed Shouse Dr., a major arterial with central median within 250' of right-of-way.

▶ **SUBDIVISION VARIANCES REQUIRED:**

STAFF RECOMMENDATION:

▶ **APPROVE the Concept Plan subject to 7 conditions.**

1. Connection to sanitary sewer and meeting any other relevant requirements of the Knox County Health Department.
2. Provision of street names which are consistent with the Uniform Street Naming and Addressing System within Knoxville (City Ord. 0-280-90).
3. Implementing the required recommendations of the Historic Middlebrook Transportation Impact Study as prepared by Ajax Engineering, dated April 22, 2019, and as amended and approved by the City of Knoxville Department of Engineering.
4. Access to all lots shall be from the internal road system for the subdivision, excluding the portion of the property zoned A-1 (General Agricultural).
5. Installing all sidewalks shown on the development plan in accordance with the requirements of the

- Americans with Disabilities Act (ADA) and the City of Knoxville Department of Engineering.
6. Meeting all applicable requirements of the Knoxville Department of Engineering.
 7. Meeting all applicable requirements of the Knoxville Zoning Ordinance.

With the recommended conditions noted above, this concept plan meets all requirements for approval.

COMMENTS:

This proposal is to subdivide this 58.4 acre property into 2 lots, approximately 25 acres will become an office warehouse development with access to Ed Shouse Drive and the remainder will remain with the historic Middlebrook property. There will be one new public road with a minimum 30' pavement width and 50' right-of-way with a 25 mph design speed. The road will terminate in a cul-de-sac and there will be a sidewalk on one side of the street. The subdivision regulations require a 75' radius for ROW and curbs in Commercial and Industrial zones, however, this can be reduced to 50' with approval by the Knoxville Department of Engineering. The radius on the south side of the new road intersection is 55', however, is part of a deceleration taper which effectively functions as a larger turn radius. The applicant proposes to move an existing median curb cut to the south to align with the new road. The proposed office warehouse development is a permitted use within the C-6 zone and the development plan will be administratively reviewed by Planning staff.

The existing median opening proposed to be closed on Ed Shouse Drive is located approximately 400 feet to the south of the CSX Railroad overpass. A new median opening will be constructed on Ed Shouse Drive at the new Historic Middlebrook Driveway and will be approximately 340 feet to the south of the existing median opening. It will also be approximately 435 feet to the north of the existing median opening that is provided for the main KUB driveway. According to TDOT's Roadway Design Guidelines the desirable median opening spacing for an urban environment is 660 feet. However, it also states that a range of 440 feet to 880 feet is acceptable. Based on a TDOT minimum median spacing of 440 feet, the proposed location of the new median opening on Ed Shouse Drive will be 5 feet below the minimum at 435 feet. The traffic engineer that developed the Traffic Impact Study (TIS) states it is not expected that this spacing shortfall of 5 feet will create a safety issue.

The required sight distance along Ed Shouse Drive from the new road intersection is 1,220 feet for semi-trailers on a divided median road with a design speed of 55 mph. The sight distance is available looking to the south is attainable, however, it is not attainable to the north. To remediate the sight distance concern for vehicles turning southbound (left) onto Ed Shouse Drive, the TIS recommends installing a southbound left turn entering median lane to allow semi-trailers to stop in the median as necessary and accelerate before entering the travel lanes. To improve sight distance, the TIS also recommends removing the existing trees in the median that are within the sight distance triangle. This will require approval from the City of Knoxville Urban Forester and potentially remediation by planting trees in other locations of need.

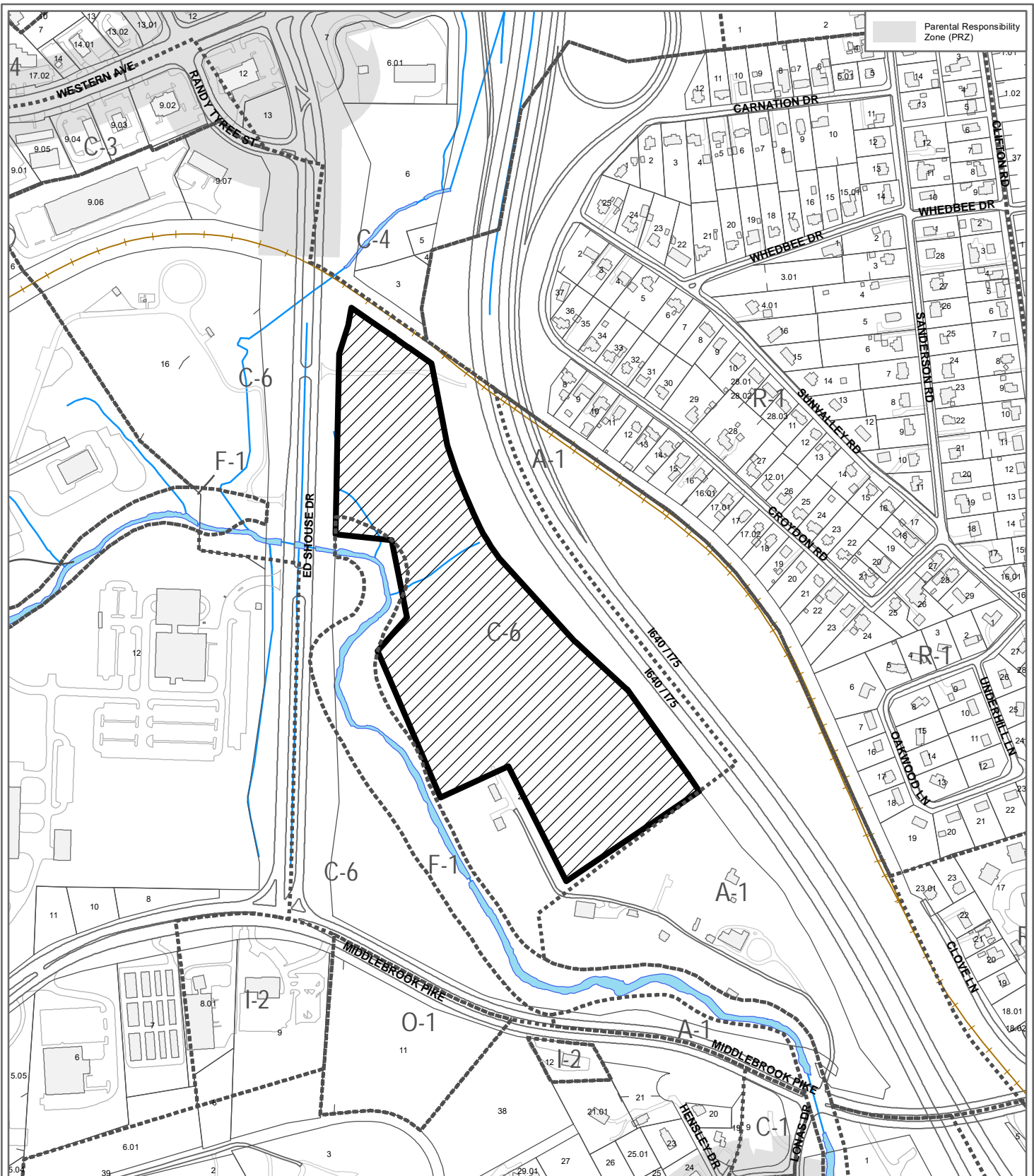
The design of the median opening and turn lanes is similar to the existing opening to the north at Faclonite Way. The median opening separates left turning movements in the median with a triangular-shaped depressed island. It is recommended that the new median opening for Historic Middlebrook be constructed in a similar manner to help separate the left turning movements at the intersection and restrict thru motorists on Ed Shouse Drive from attempting u-turn maneuvers.

Other recommendations of the TIS are a southbound median deceleration lane for left turn movements, a northbound acceleration lane, and a northbound deceleration taper. The TIS design recommendations are all provided on the Concept Plan.

ESTIMATED TRAFFIC IMPACT: A traffic impact study was prepared by the applicant. The findings of that study were used in formulating the recommendations of this staff report.

ESTIMATED STUDENT YIELD: Not applicable.

Knoxville-Knox County Planning Commission's approval or denial of this request is final, unless the action is appealed to Knox County Chancery Court. The date of the Knox County Chancery Court appeal hearing will depend on when the appeal application is filed.



**3-SC-19-C
CONCEPT PLAN**

Subdivision: Historic Middlebrook
Preservation Trust

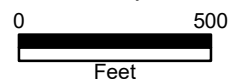


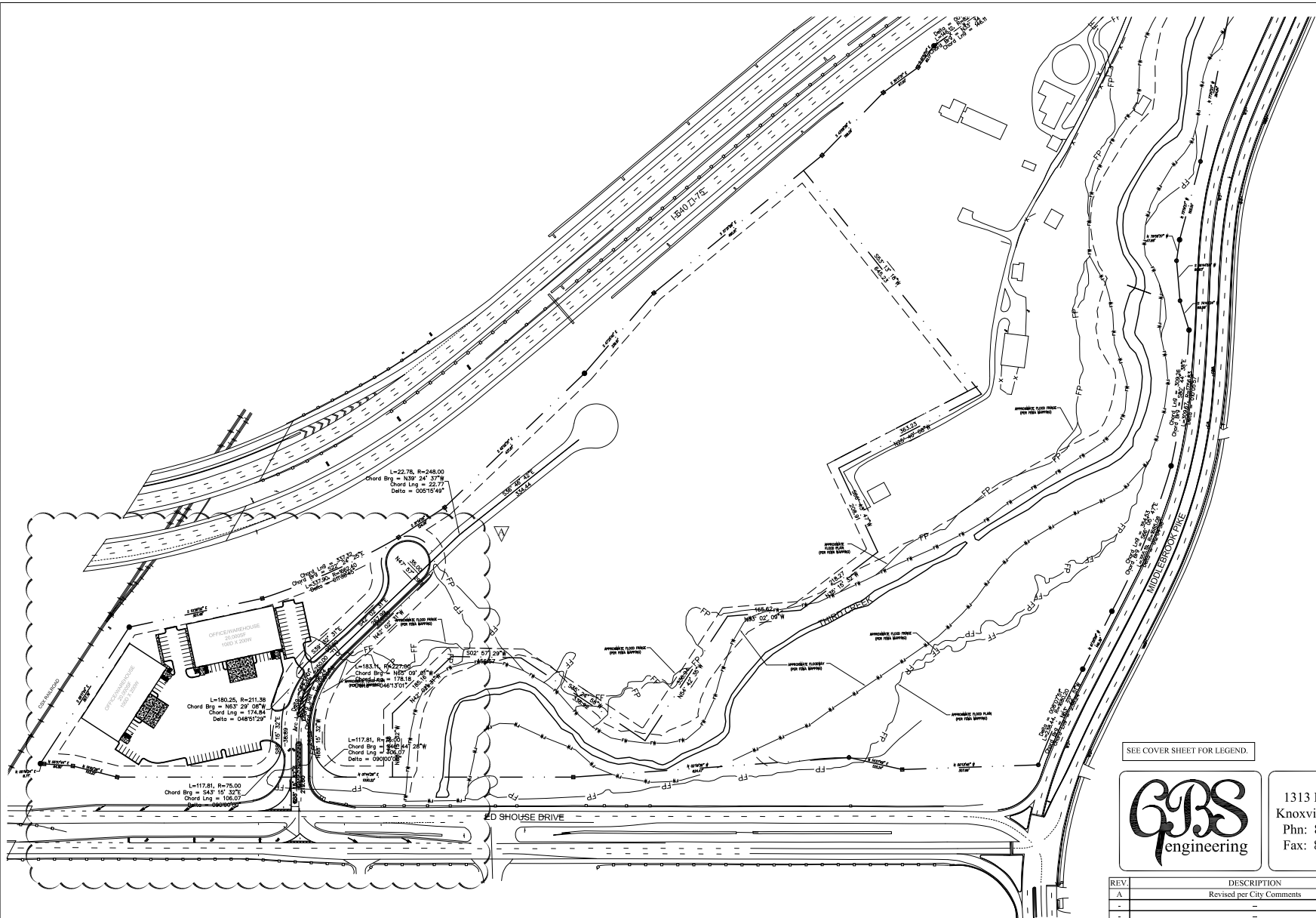
Approval of Concept Plan

Map No: 93

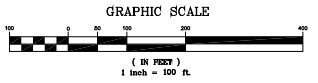
Jurisdiction: City

Original Print Date: 2/19/2019
 Revised:
 Metropolitan Planning Commission * City / County Building * Knoxville, TN 37902





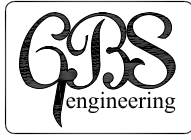
OVERALL SITE PLAN
 Scale: 1" = 100'-0"



3-SC-19-C
 Revised: 3/29/2019



SEE COVER SHEET FOR LEGEND.

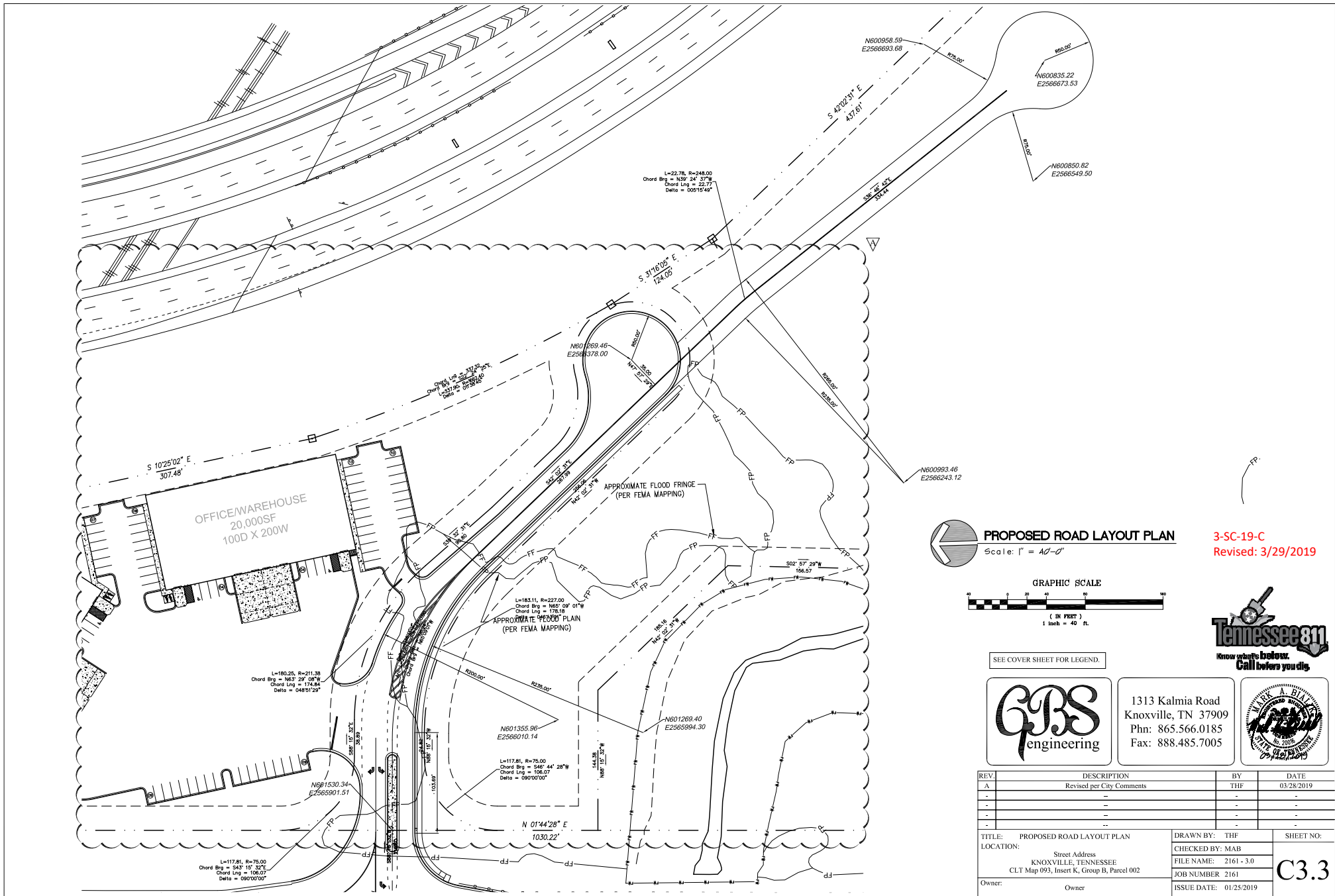


1313 Kalmia Road
 Knoxville, TN 37909
 Phn: 865.566.0185
 Fax: 888.485.7005

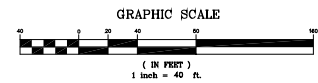


| REV. | DESCRIPTION | BY | DATE |
|------|---------------------------|-----|------------|
| A | Revised per City Comments | THF | 03/28/2019 |
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|-----------|--|-------------|------|-------------|------|------------|
| TITLE: | OVERALL SITE PLAN | DRAWN BY: | THF | SHEET NO.: | C3.0 | |
| LOCATION: | Street Address KNOXVILLE, TENNESSEE CLT Map 093, Insert K, Group B, Parcel 002 | CHECKED BY: | MAB | FILE NAME: | | 2161 - 3.0 |
| Owner: | Owner | JOB NUMBER: | 2161 | ISSUE DATE: | | 01/25/2019 |
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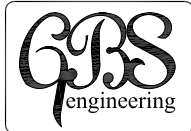


PROPOSED ROAD LAYOUT PLAN
 Scale: 1" = 40'-0"



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SEE COVER SHEET FOR LEGEND.

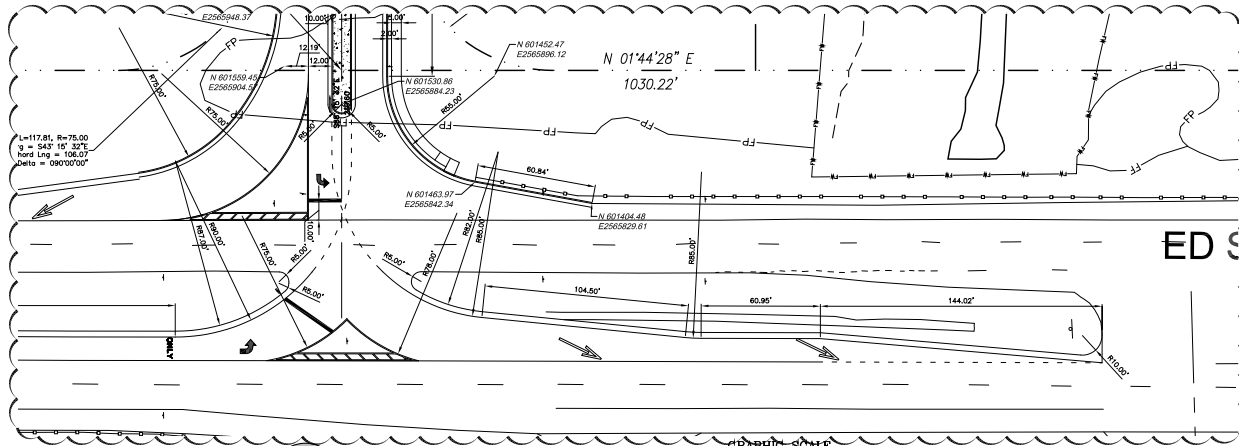


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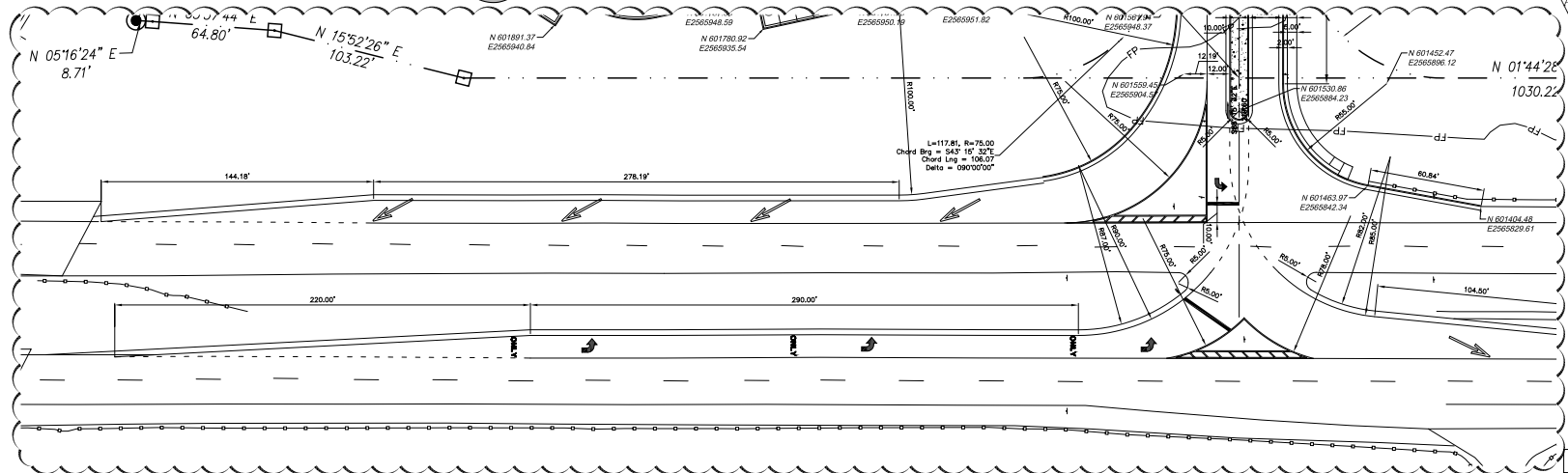
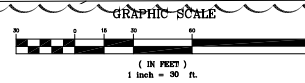
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| TITLE: PROPOSED ROAD LAYOUT PLAN | DRAWN BY: THF | SHEET NO: |
| LOCATION: Street Address KNOXVILLE, TENNESSEE CLT Map 093, Insert K, Group B, Parcel 002 | CHECKED BY: MAB | C3.3 |
| Owner: Owner | FILE NAME: 2161 - 3.0 | |
| | JOB NUMBER 2161 | |
| | ISSUE DATE: 01/25/2019 | |



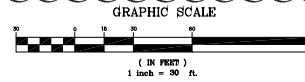
ED SHOUSE IMPROVEMENTS

Scale: 1" = 30'-0"



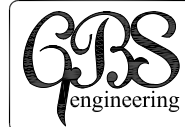
ED SHOUSE IMPROVEMENTS

Scale: 1" = 30'-0"



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| TITLE: Ed Shouse Improvements Layout Plan | DRAWN BY: THF | SHEET NO: |
| LOCATION: Street Address KNOXVILLE, TENNESSEE CLT Map 093, Insert K, Group B, Parcel 002 | CHECKED BY: MAB | C3.4 |
| Owner: Owner | FILE NAME: 2161 - 3.0 | |
| | JOB NUMBER 2161 | |
| | ISSUE DATE: 01/25/2019 | |



SITE GRADING, DRAINAGE & EROSION CONTROL DETAIL REFERENCE LEGEND

MKRK - IMPROVED ITEM
 HW - HEADWALL
 BP - BUSH/PIPE/CULVERT STRUCTURE
 BS - BASSIN/OUTLET STRUCTURE
 CI - CURB/INLET
 CB - CATCH BASIN
 HDH - HIGH DENSITY POLYETHYLENE PIPE
 RP - REINFORCING POLYESTER STRUCTURE
 RCF - REINFORCED CONCRETE PIPE CLASS III
 SMP - SPECIAL OPERATIONAL PIPE ALUMINUM ZINC
 RB - BOTTOM OF CURB
 TC - TOP OF CURB
 HW - BOTTOM CURB WALL
 TW - TOP OF WALL
 TP - ELEVATION OF TOP OF PAVEMENT

STRIPPING AND COMPACTION SCHEDULE

Topsoil stripping depth 2"-3" average. Deeper if required locally.

Required compaction percentages (% max dry density)
 Standard Proctor Method ASTM Test 1-2008
 Building Area: 98% Parking Area: 98%

TESTING:
 Allow 1 compaction test per ea. 2500 S.F. (Max 50' x 50' area) per lift at building areas.
 Allow 1 compaction test per each 10,000 S.F. of cut or natural grade at building area.
 Allow 1 compaction test per ea. 5500 S.F. (Max 75' x 75' area) per lift at parking area.
 Allow 1 compaction test per each 40,000 S.F. of cut or natural grade at parking area.

Maximum loose lift thickness shall be 6", unless noted otherwise.

EXCAVATION AND GRADING NOTES

Comply with local and state ordinances regarding disposal of debris. Do not burn debris on site without specific written approval of both SITE, Inc. and local authority. Clear and grub for new construction as required.

EXCAVATION PREPARATION
 Verify location of existing private utilities. Protect and maintain existing utilities as required. Establish and verify existing lines, grade and dimensions shown on the drawing. Report any errors or inconsistencies to SITE, Inc. before commencement of work.

EXISTING TOPO ACCURACY
 Elevation contours shown are intended to form a general description of surface ground elevations. Topographic information shown herein shall be considered to have not more than the following plus or minus limits of accuracy: 0.10 feet at benchmarks, 0.25 feet at spot grades, 1.0 feet at contour lines.

Contractor is cautioned that straight line interpolation between contours may not exactly reflect existing conditions. Areas of relatively quick changes in contours should not be allowed to unduly influence interpretation of contours.

Report any errors or inconsistencies to GBS Engineering before commencement of work. Any error not so reported cannot be considered for adjustment in contract price.

STRIPPING AND SPREADING TOPSOIL
 Topsoil shall be stripped and temporarily stock piled for later reuse. If no stock pile areas is noted on grading plan, stock pile shall be in such place to provide for ready access to necessary areas of site by other trades throughout progress of work. Particular attention shall be paid to finely textured topsoil that will be reseeded by landscape contractor. General topsoil respreading will be by grading contractor to a minimum depth of 6" at any point except on slope of 3:12 or 1:1 steeper where topsoil depth shall be 2" min. At any point not receiving soil.

PROOF-ROLLING
 All areas to receive new structural fill or other new construction shall be proof rolled with a fully loaded tandem dump truck in presence of soils testing agent before commencing any construction activity. All areas to be compacted shall receive 8 passes with compaction roller. Soil engineer shall notify GBS Engineering that all areas have passed proof roll inspection prior to commencement cut & fill of grading. Do not commence grading prior to such notification.

UNDERCUT-BACKFILL
 Undercut shall mean excavation or other removal of unsuitable material below elevation of proposed final subgrade. In no case shall the term undercut be used to describe material occurring above final subgrade elevation. Excavation of undercut of unsuitable materials, if any must be recommended by the soil engineer for the owner's approval. Soil engineer shall not have unilateral authority to direct undercut activities without approval of the owner's representative. Soil engineer's written field notes will be required to substantiate amounts of undercut. Backfill shall be of approved material only. Money for required testing shall be included in quoted prices concerning undercut and backfill.

When terms of bidding call for an unclassified contract no payment for undercut/backfill will be considered. Such quotes shall be included in the contractor's base bid. Undercut and backfill shall not constitute an extra to the contract in unclassified contracts.

STRUCTURAL FILL
 Structural fill shall be compacted as specified in excavation and grading schedule within 1 to 5% of optimum moisture content. Off site borrow areas shall be approved by GBS Engineering. Place fill and backfill in layers as specified in excavation and grading notes. Use no deleterious material, organic, or rock larger than 6" boulders with any require additional water or may require additional drying to reach optimum moisture content for compaction.

GRADING TOLERANCE
 Grading contractor shall bring subgrade to within an average of 1/10 of one foot of required elevation to obtain finished grade indicated on drawings except at locations where spot elevations are shown and where floor elevations are shown which shall be within 1/10 of 1 foot of required elevations. Final subgrade shall be spot checked by a licensed surveyor with not less than 1 inch per 1.5 Acres and each finished floor elevations and drawing prepared and forwarded to GBS Engineering for approval prior to commencing final grade or paving or building construction.

Contractor shall be responsible for minor adjustments in grades as directed by GBS Engineering. Such changes shall be limited to 1500 cubic yards total. No additional cost will be allowed for this work.

TESTING
 Contractor shall include price of all required testing as set forth herein to be performed by an independent qualified geotechnical testing agent suitable to GBS Engineering. Test reports must use graphic location map at 1" = 200' scale for location of density tests. Original copies of all tests shall be forwarded directly to GBS Engineering from testing agent. All tests shall specifically state that the test either passes or fails to meet specifications. Contractor shall include in its base bid 5 (five) soil compaction tests after completion of the work, located per GBS Engineering.

WATER CONTROL
 The contractor shall furnish all labor, materials and equipment necessary to keep the work free of water either from surface sources or from underground sources or both. Selection of equipment and methods shall be the sole responsibility of the contractor. The contractor shall be and is responsible for all damage incurred in handling water conditions. Additionally, the contractor shall provide means necessary to avoid direct runoff from the project onto adjacent property.

GRASSING AND SODDING
 Grassing and sodding of all disturbed areas not paved or built upon shall be the responsibility of the grading contractor unless noted otherwise. Hardened soil shall be loosened to a depth of 6" and topsoil respread as noted before seeding.

Grass and sodding shall be as specified on the drawings.

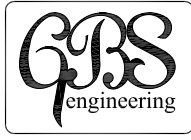
EROSION CONTROL
 Contractor shall refer to Storm Water Pollution Prevention Plan for Erosion Control measures and details.

SAFETY SCREENS
 Safety screens having a maximum opening of 4" shall be provided for any pipe or opening to prevent children or large animals from crawling into the structures. Screens shall be welded 3/8" bars @ 6" O.C. Each way.

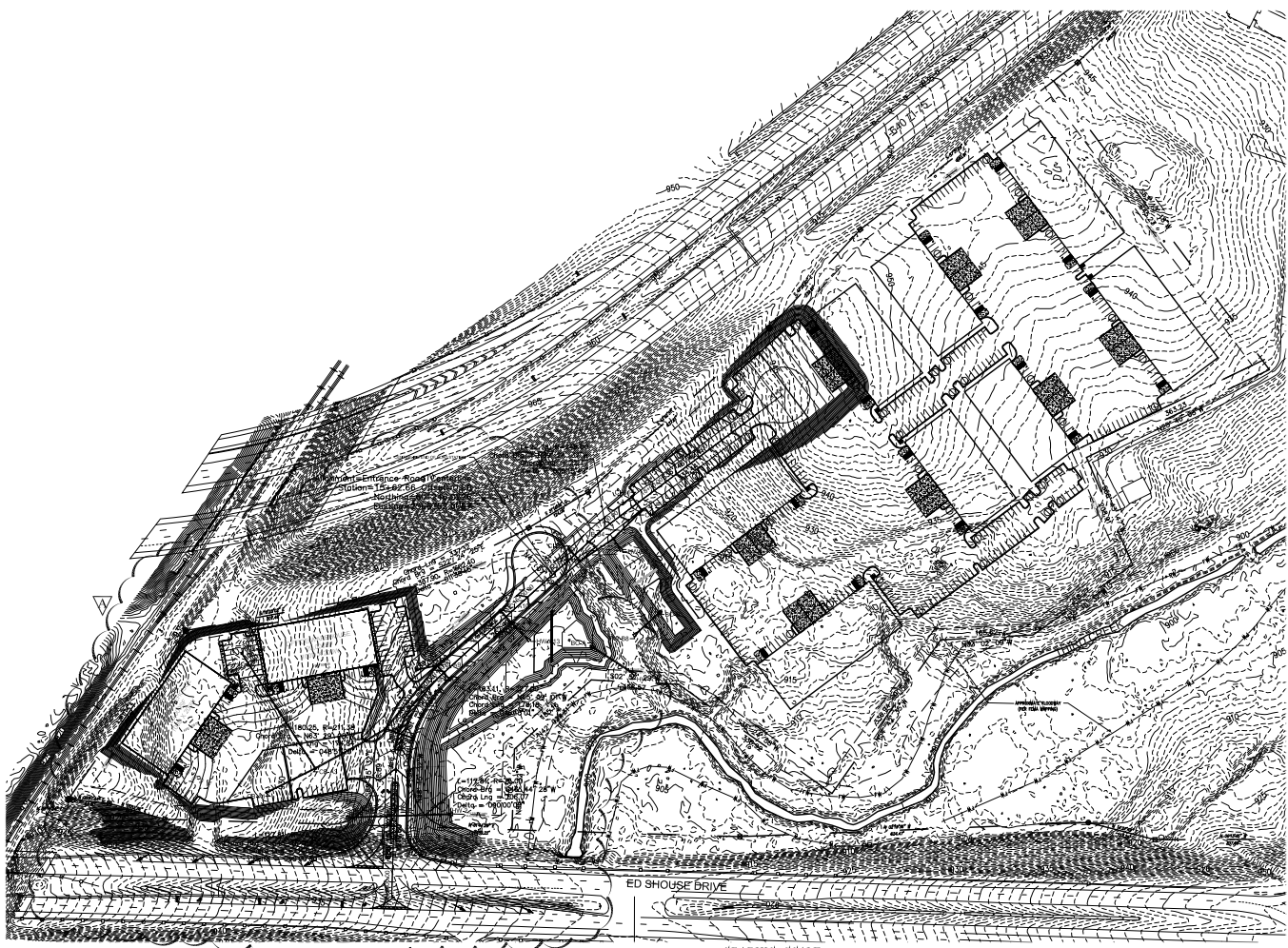
PERMITS
 Contractor shall pay all grading permit fees and post grading bond, if required.

- NOTES:**
- SEE DRAWING C4.4 FOR CENTERLINE PROFILE OF NEW ENTRANCE DRIVE AND TYPICAL ENTRANCE DRIVE SECTION.
 - SEE DRAWINGS C4.1 THRU C4.3 FOR BLOWUPS OF VARIOUS AREAS OF NEW GRADINGS FOR MORE DETAIL.

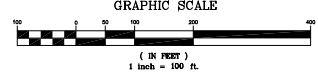
SEE COVER SHEET FOR LEGEND.



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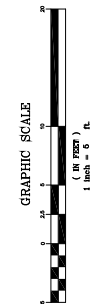
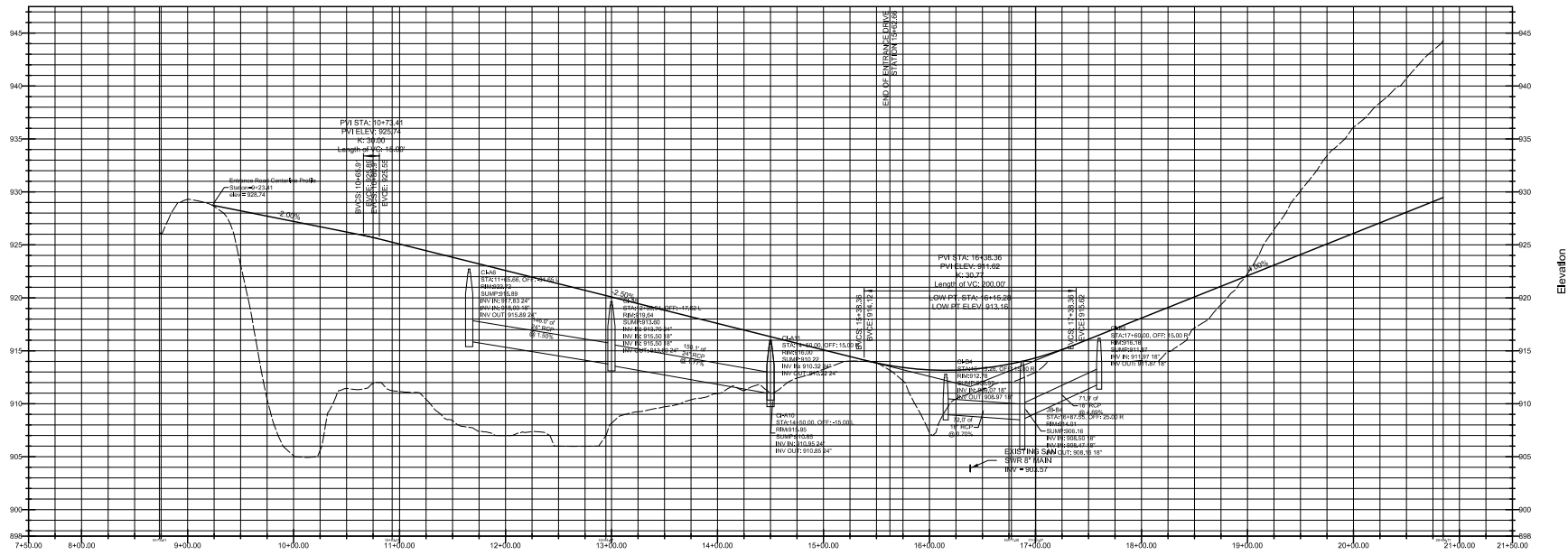
OVERALL GRADING PLAN
 Scale: 1" = 100'-0"



3-SC-19-C
 Revised: 3/29/2019

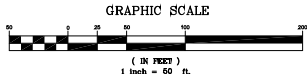
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|---|---|-----------|
| TITLE: OVERALL SITE GRADING PLAN | DRAWN BY: THF | SHEET NO: |
| LOCATION: Street Address KNOXVILLE, TENNESSEE CLT Map 093, Inset K, Group B, Parcel 002 | CHECKED BY: MAB FILE NAME: 2161 - 4.0 JOB NUMBER: 216 | C4.0 |
| Owner: Owner | ISSUE DATE: 01/25/2019 | |

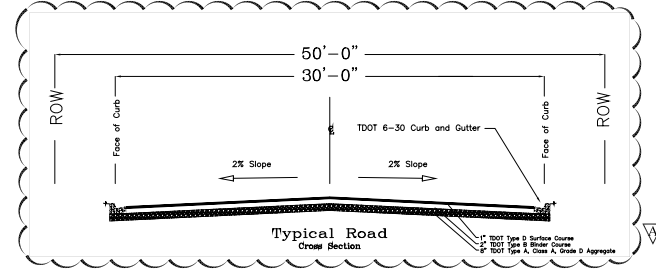


ENTRANCE ROAD CENTERLINE PROFILE

Scale: HORIZ. 1" = 50'-0" & VERT. 1" = 5'-0"

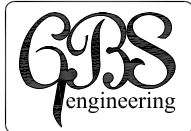


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NOTES:
• SEE DRAWINGS C4.1 THRU C4.3 FOR BLOWUPS OF VARIOUS AREAS OF NEW GRADING FOR MORE DETAIL.

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| TITLE: ENTRANCE ROAD CENTERLINE PROFILE | DRAWN BY: MAB | SHEET NO: |
| LOCATION: Street Address KNOXVILLE, TENNESSEE CLT Map 093, Insert K, Group B, Parcel 002 | CHECKED BY: MAB | C4.4 |
| Owner: Owner | FILE NAME: 2161 - 4.0 | |
| | JOB NUMBER 216 | |
| | ISSUE DATE: 01/25/2019 | |



Sherry Michienzi <sherry.michienzi@knoxplanning.org>

Fwd: 2161 - Historic Middlebrook (3-SC-19-C)

1 message

Mike Reynolds <mike.reynolds@knoxplanning.org>

Tue, Apr 2, 2019 at 4:18 PM

To: Sherry Michienzi <sherry.michienzi@knoxplanning.org>, Dori Caron <dori.caron@knoxplanning.org>, Laura Edmonds <laura.edmonds@knoxplanning.org>

Postponement request for 3-SC-19-C until the May 9, 2019 Planning Commission meeting.

----- Forwarded message -----

From: **Mark Bialik** <Mark@gbs-eng.net>

Date: Tue, Apr 2, 2019 at 4:04 PM

Subject: 2161 - Historic Middlebrook

To: Mike Reynolds <mike.reynolds@knoxplanning.org>

Cc: jimnixon@5881000.com <jimnixon@5881000.com>, David Fiser <dfiser@fiserinc.com>, Travis Ferrell <Travis@gbs-eng.net>, Ricky Fox <renardffo@aol.com>

Mike,

Per our telephone conversation this afternoon, I understand we need to post-pone for one more month, due to the Traffic Study was submitted later than the date required to stay on the agenda for April. We look forward to getting approval in May.

Please call or email me, if you require any additional information, or have any questions.

Mark A. Bialik, PE

(AL, AR, FL, GA, IN, IA, KY, NC, NJ,

OK, PA, SC, TN, TX, UT, VA, WI, WV)

GBS Engineering

Phone: 865.566.0185

Fax: 1.888.485.7005

Cell: 865.607.1246

email: mark@gbs-eng.net

Address: 1313 Kalmia Road,

Knoxville, TN 37909

LEGAL DISCLAIMER

SUBDIVISION - CONCEPT

Name of Applicant: GBS Engineering (Mark A. Bixik)
 Date Filed: 1/28/19 Meeting Date: 3/24/19
 Application Accepted by: Marc Payne
 Fee Amount: 500.00 File Number: Subdivision - Concept 3-SC-19-6
 Fee Amount: Related File Number: Development Plan



PROPERTY INFORMATION

Subdivision Name: Historic Middlebrook Preservation Trust

Unit/Phase Number:

General Location: w/s I-640 E/s Ed. Shouse Dr due north of Middlebrook Pike

Tract Size: 58.42 No. of Lots: 1

Zoning District: C-6, F-1

Existing Land Use: Vacant

Planning Sector: Northwest City

Growth Policy Plan Designation: City

Census Tract: 3802

Traffic Zone: 155

Parcel ID Number(s): 093KB002

Jurisdiction: City Council 6th District
 County Commission District

PROPERTY OWNER/OPTION HOLDER

PLEASE PRINT

Name: Lick Fox

Company: Historic Middlebrook Pres Trust, Inc

Address: 4001 Middlebrook

City: KNOXVILLE State: TN Zip: 37921

Telephone: 494-1199

Fax:

E-mail: BrenardH@nol

PROJECT SURVEYOR/ENGINEER

PLEASE PRINT

Name: Mark A. Bixik

Company: GBS ENGINEERING

Address: 1313 KALMIA RD

City: KNOXVILLE State: TN Zip: 37909

Telephone: 566-0185

Fax: 888-485-7005

E-mail: mark@gbs-eng.net

AVAILABILITY OF UTILITIES

List utility districts proposed to serve this subdivision:

Sewer KUB

Water "

Electricity "

Gas "

Telephone Att

APPLICATION CORRESPONDENCE

All correspondence relating to this application (including plat corrections) should be directed to:

PLEASE PRINT

Name: Mark A. Bixik

Company: GBS ENGINEERING

Address: 1313 KALMIA RD

City: KNOXVILLE State: TN Zip: 37909

Telephone: 566-0185

Fax: 888-485-7005

E-mail: mark@gbs-eng.net

TRAFFIC IMPACT STUDY REQUIRED

No Yes

USE ON REVIEW No Yes

Approval Requested:

Development Plans in Planned District or Zone

Other (be specific):

VARIANCE(S) REQUESTED

No Yes (If Yes, see reverse side of this form)

SIGNATURES OF ALL PROPERTY OWNERS INVOLVED OR HOLDERS OF OPTION ON SAME MUST BE LISTED BELOW:

Please Sign in Black Ink:

(If more space is required attach additional sheet.)

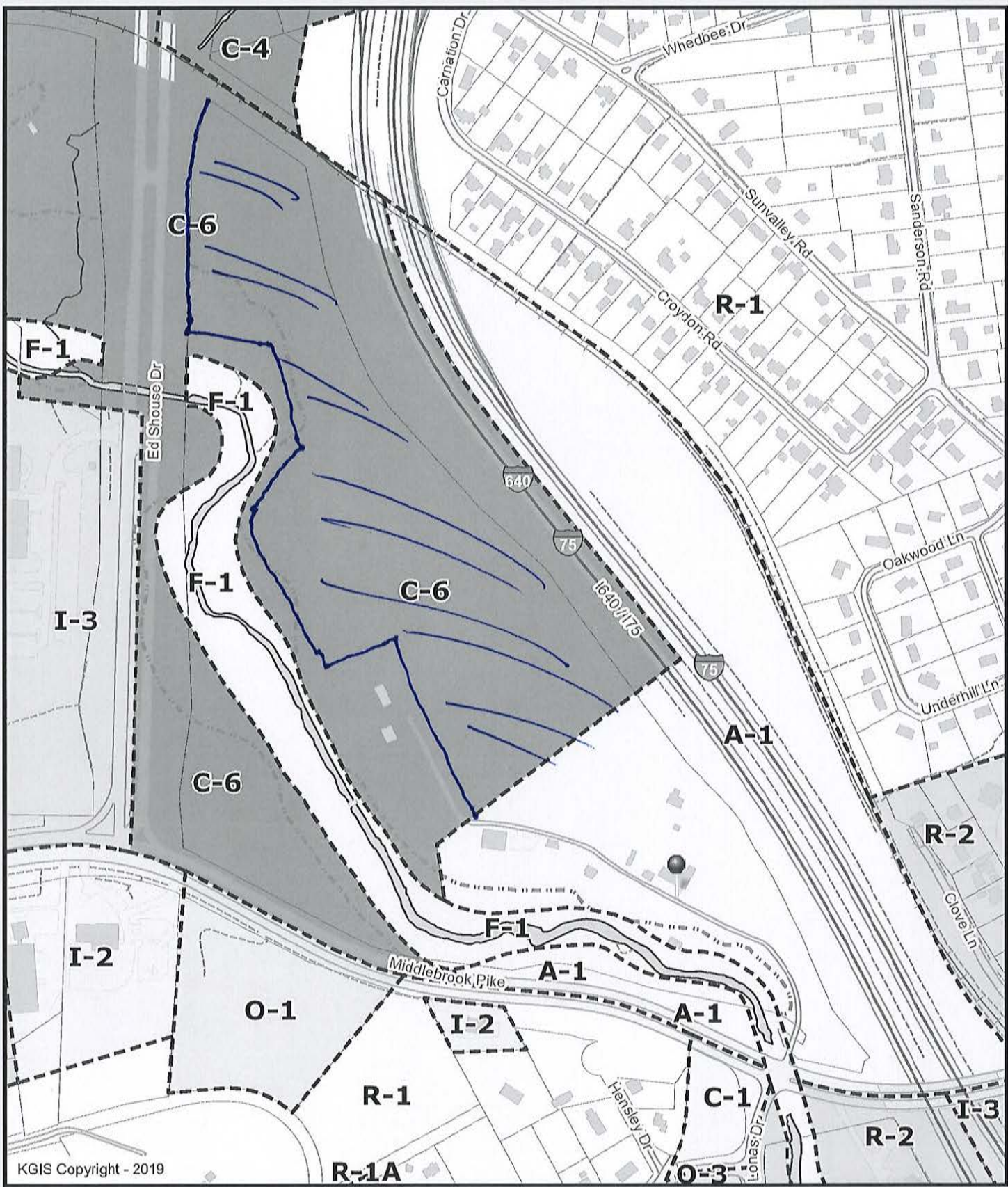
Name Address • City • State • Zip Owner Option

~~Sherwood Williams~~

7019 ROCKINGHAM DRIVE
KNOXVILLE TN 37909

X

Option



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Letter Portrait

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Knoxville - Knox County - KUB Geographic Information System

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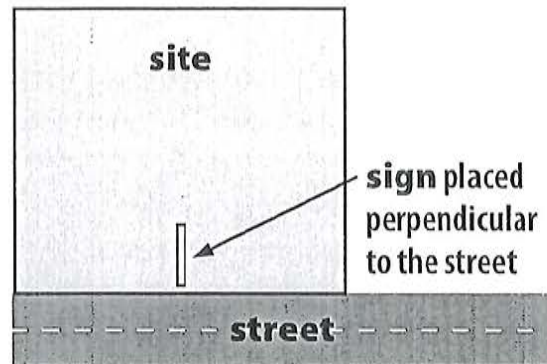
REQUIRED SIGN POSTING AGREEMENT

For all rezoning, plan amendment, concept plan, use on review, right-of-way closure, and street name change applications, a sign must be posted on the subject property, consistent with the adopted MPC Administrative Rules and Procedures.

At the time of application, MPC staff will provide a sign(s) to post on the property as part of the application process. If the sign(s) go missing for any reason and need to be replaced, then the applicant will be responsible for picking up a new sign(s) from the MPC offices. The applicant will be charged a fee of \$10 for each replacement sign.

LOCATION AND VISIBILITY

The sign must be posted in a location that is clearly visible from vehicles traveling in either direction on the nearest adjacent/frontage street. If the property has more than one street frontage, then the sign should be placed along the street that carries more traffic. MPC staff may recommend a preferred location for the sign to be posted at the time of application.



TIMING

The sign(s) must be posted 15 days before the scheduled MPC public hearing and must remain in place until the day after the meeting. In the case of a postponement, the sign can either remain in place or be removed and reposted 15 days before the next MPC meeting.

I hereby agree to post and remove the sign(s) provided on the subject property consistent with the above guidelines and between the dates of:

2/27/19 and 3/15/19
(15 days before the MPC meeting) (the day after the MPC meeting)

Signature: Mark A. Bialik

Printed Name: Mark A. Bialik

Phone: 506-0125 Email: mark@gps-eng.net

Date: 2/28/2019

MPC File Number: 3-SC-19-C