



# DEVELOPMENT PLAN/ USE ON REVIEW REPORT

▶ **FILE #:** 2-H-26-DP **AGENDA ITEM #:** 41  
2-A-26-UR **AGENDA DATE:** 2/12/2026

▶ **APPLICANT:** THOMPSON THRIFT DEVELOPMENT, INC.  
**OWNER(S):** Melinda Massengill KIRBY RALPH L JR & AGNES R CO TRUSTEES

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**TAX ID NUMBER:** 91 206 [View map on KGIS](#)  
**JURISDICTION:** County Commission District 6  
**STREET ADDRESS:** 8007 BALL CAMP PIKE

▶ **LOCATION:** Southeast side of Schaad Rd, north of Ball Camp Pike

▶ **APPX. SIZE OF TRACT:** 18.01 acres

**GROWTH POLICY PLAN:** Planned Growth Area

**ACCESSIBILITY:** Access is via Schaad Road, a four-lane median-divided minor arterial within a right-of-way width that varies from 100-370 ft.

**UTILITIES:** Water Source: West Knox Utility District  
Sewer Source: West Knox Utility District

**FIRE DISTRICT:** Karns Fire Department

**WATERSHED:** Beaver Creek

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▶ **ZONING:** **OB(k) (Office, Medical, and Related Services, with conditions), PR(k) (Planned Residential, with conditions) up to 9 du/ac**

**PLACE TYPE:** CMU (Corridor Mixed-use), SR (Suburban Residential)

▶ **EXISTING LAND USE:** Agriculture/Forestry/Vacant Land

▶ **PROPOSED USE:** Multi-family Development (Revision of 11-D-25-DP / 11-C-25-UR)

**HISTORY OF ZONING:** In 2025 a request to rezone the property from I (Industrial) to PR (Planned Residential) up to 12 du/ac on one portion of the parcel and 24 du/ac on another portion was withdrawn by the applicant (4-J-25-RZ). In 2025 the property was rezoned from I to PR(k) (Planned Residential, with conditions) up to 9 du/ac on one portion of the parcel and OB(k) (Office, Medical, and Related Services, with conditions) on the remainder of the property (6-H-25-RZ).

**SURROUNDING LAND USE AND ZONING:**

North: Single family residential, agriculture/forestry/vacant land - RA (Low Density Residential), I (Industrial)

South: Railroad right-of-way, rural residential, private recreation, single family residential - I (Industrial), A (Agricultural), PR (Planned Residential) up to 7 du/ac

East: Agriculture/forestry/vacant land - I (Industrial)

West: Single family residential- I (Industrial)

**NEIGHBORHOOD CONTEXT:** The surrounding area features single-family houses on large 1+ acre lots and single-family and multi-family subdivisions. The Schaad Road extension

construction was recently completed in front of this parcel, creating a new point of access. The forested slopes of Beaver Ridge lie to the north.

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**STAFF RECOMMENDATION:**

- ▶ **Approve the development plan for a multifamily development with up to 300 units, as shown in the development plan, subject to 7 conditions.**
  1. Connecting to sanitary sewer and meeting other relevant utility provider requirements.
  2. Implementing the recommendations of the Traffic Impact Analysis for the Proposed Schaad Road Apartments by Civil & Environmental Consultants, Inc. (revised 12.29.2025, Exhibit B), as required by Knox County Engineering and Public Works. A Memorandum of Understanding with Knox County Engineering and Public Works may be required for work within the public rights-of-way, per Chapter 54, Article V of the Knox County Code (Ord. O-23-4-102).
  3. If during plat approval or construction of the development, it is discovered that unforeseen off-site improvements within the right-of-way are necessary as caused by this development, the developer will either enter into a Memorandum of Understanding with the County for these improvements or reimburse the County for their direct expenses (if completed by County crews) to make corrections deemed necessary.
  4. Meeting all requirements of the Knox County Department of Engineering and Public Works.
  5. Meeting all applicable requirements of the Knox County Zoning Ordinance.
  6. Installing all landscaping within six months of issuance of an occupancy permit for the project, or posting a bond with the Knox County Department of Engineering and Public Works to guarantee such installation. Existing trees that remain can be counted toward the proposed landscape buffer. Proposed landscaping within stream buffer areas shall be reviewed by the Knox County Engineering and Public Works Department during the permitting phase.
  7. Recording the proposed easement via a final plat for future connection to the abutting eastern property.
  
- ▶ **Approve the use on review for a multi-family development with up to 300 units (with up to 274 units in the OB(k) zone), as shown in the development plan, subject to 1 condition.**
  1. Meeting all the conditions of the associated development plan application (11-D-25-DP).

**COMMENTS:**

BACKGROUND: The Planning Commission has recently approved a 300-unit multifamily development on this 18.1-acre property, which has split zoning of OB(k) and PR(k) at 9 du/ac. The previous approval (11-C-25-UR, 11-D-25-DP) was contingent on obtaining permission for an emergency access drive across the railroad right-of-way south of the property; however, the railway company refused to grant permission. The applicant now proposes an alternative emergency access from Schaad Road. Except for a few minor changes (e.g., incorporation of a Type A landscape buffer along the western boundary, reduction of 15 parking spaces, and removal of some sidewalk connections to the sides of buildings 1, 7, 8, and 11), the proposal is largely identical to the previously approved plans.

The proposed 300 units are distributed among eleven buildings: five three-story buildings (buildings 1, 4, 7, 8, 11 on the site plan), each with 36 units, and six two-story structures (buildings 2, 3, 5, 6, 9, 10), each comprising 20 units. Building 2 spans both the PR and OB zones. There are 274 units within the OB(k) zone, yielding a density of 19.96 du/ac, and the PR(k) zone accommodates the remaining 26 units at a density of 5.95 du/ac. In addition to the open lawn areas spread throughout the development, the community offers amenities such as a clubhouse, fitness center, pool area, pickleball courts, and a dog park.

**DEVELOPMENT PLAN ANALYSIS PER ARTICLE 6, SECTION 6.50.06 (APPROVAL OR DENIAL)**

In the exercise of its administrative judgment, the Planning Commission shall determine if the proposed plan is in harmony with the general purpose and intent of the zoning ordinance and adopted plans.

- 1) ZONING ORDINANCE OB(k) (Office, Medical and Related Services, with conditions) and PR(k) (Planned Residential, with conditions) up to 9 du/ac:
  - A. The OB zone allows any use permitted and as regulated in the RB zone, except for maximum building height, which is regulated by the OB zone. The RB zone allows multi-family developments with a density between 12-24 du/ac as a use on review, subject to Article 5.13.15, which also requires a development plan. The OB zone consists of approximately 13.73 acres of the subject property, and the proposed density of 19.96 du/ac is consistent with the maximum allowable density of 24 du/ac.
  - B. The PR zone allows multi-dwelling developments as a permitted use. The administrative procedures for the PR zone require the Planning Commission to approve the development plan before permits can be issued

(Article 5.13.15). The PR zone comprises 4.37 acres split by the railroad right-of-way, and the proposed density of 5.95 du/ac is well within the approved density of 9 du/ac.

C. The rezoning of this property (6-H-25-RZ) was approved with three conditions: First, primary access must be from Schaad Road, though limited access to Ball Camp Pike may be allowed for fire and emergency services. Second, stub-outs are required to connect to adjacent properties within the CMU Place Type. Third, pedestrian access to Schaad Road must be provided for developments within the CMU place type. The proposal meets all these requirements. A stub-out is provided to the eastern property, and sidewalks are proposed throughout the development.

D. The setbacks and peripheral boundary annotated on the plan meet the standards of the OB(k) and PR(k) zone. The northeastern structure (building 2), which spans both the PR(k) and OB(k) zones, meets the setback requirements for both zones. The proposal also meets the maximum building coverage of 30 percent within the OB(k) zone, as regulated by the RB zone standards.

E. The RB zone requires reserving at least 10% of the gross development area for recreational uses and usable open space areas (Article 5.12.09.F) for a multi-dwelling development with a density of between 8 du/ac and 20/du. Usable open space is not defined, but it should be accessible to the residents, large enough for passive or active uses, and, preferably, relatively flat. The site plan notes that 43% of the OB(k) zone is proposed for recreational uses and open space, comprising amenity areas and recreational areas totaling approximately 1.61 acres, and other open spaces totaling 4.28 acres.

## 2) KNOX COUNTY COMPREHENSIVE PLAN - FUTURE LAND USE MAP

A. The OB(k) zone of the property is classified as the CMU (Corridor Mixed-Use) place type, and the PR(k) portion is classified as the SR (Suburban Residential) place type in the Knox County Comprehensive Plan. Multifamily developments are considered a secondary use in the CMU and SR place types.

B. The CMU place type specifies a building height maximum of 5 stories and setbacks of 20-30 ft. The SR place type specifies a maximum height of 2 stories with similar setbacks. The proposal is consistent with the recommended form attributes.

C. While the SR place type promotes multiplexes similar in scale to a single-family home, only the southernmost building is entirely within the SR area. The development is primarily proposed within the CMU place type and features a cohesive overall layout.

## 3) KNOX COUNTY COMPREHENSIVE PLAN - IMPLEMENTATION POLICIES

A. Coordinate infrastructure improvements with development. (Implementation Policy 9) – The new Schaad Road supports the increase of residential density at this location. The subject property is a quarter-mile west of the planned commercial node at Bakertown Road and has access to multimodal amenities along the Schaad Road.

B. Promote connectivity with new development. (Implementation Policy 11) – The development proposes sidewalk connections to Schaad Road and provides a stub-out for future connection to the abutting eastern property, as required by the conditions of rezoning approval.

C. Improve safety for all users. (Implementation Policy 12) – The incorporation of a right-turn lane on Schaad Road, as recommended in the traffic impact analysis, will ensure safe access to the development. A secondary access is proposed at Schaad Road for fire emergencies.

## 4) KNOXVILLE - FARRAGUT - KNOX COUNTY GROWTH POLICY PLAN

A. The property is within the Planned Growth Area of the Growth Policy Plan. The purposes of the Planned Growth Boundary designation are to encourage a reasonably compact pattern of development, promote expansion of the Knoxville-Knox County economy, offer a wide range of housing choices, and coordinate the actions of the public and private sectors, particularly with regard to provision of adequate roads, utilities, schools, drainage and other public facilities and services. The proposed development is consistent with the growth policy plan.

## DEVELOPMENT STANDARDS FOR USES PERMITTED ON REVIEW (ARTICLE 4.10, SECTION 2)

The planning commission, in the exercise of its administrative judgment, shall be guided by adopted plans and policies, including the general plan and the following general standards:

### 1) THE USE IS CONSISTENT WITH ADOPTED PLANS AND POLICIES, INCLUDING THE COMPREHENSIVE PLAN OF KNOX COUNTY.

A. Multifamily dwellings are considered a secondary use in the CMU (Corridor Mixed-Use) and SR (Suburban Residential) place type of the property, as defined in the Knox County Comprehensive Plan, and the proposal is consistent with the place types' recommended form attributes, as described previously.

B. The proposed development is consistent with the Comprehensive Plan's Implementation Policies 9, 11, and

12, which encourage coordinated infrastructure improvements with development, connectivity with new developments, and improved safety for all users, respectively. The proposed sidewalks, vehicular connectivity to an adjacent property, and access via Schaad Road, along with an additional right-turn lane, support these policies.

C. The proposed development is also compatible with the subject property's location in the Planned Growth Area of the Growth Policy Plan, which encourages a reasonably compact pattern of development offering a wide range of housing choices

**2) THE USE IS IN HARMONY WITH THE GENERAL PURPOSE AND INTENT OF THE ZONING ORDINANCE.**

A. The OB zone is intended to provide areas for professional and business offices and related activities that require separate buildings and building groups surrounded by landscaped yards and open areas. The zone allows multifamily developments at a density between 12-24 du/ac with Planning Commission approval, as regulated in the RB (General Residential) zone. The PR zone is intended to provide flexibility for different types of residential development, including multi-dwelling structures and developments.

B. The development is consistent with the allowable density, dimensional standards, setbacks, and other requirements of the OB(k) and PR(k) zones. It complies with the conditions of the rezoning (6-H-25-RZ), as described previously within the associated development plan analysis.

**3) THE USE IS COMPATIBLE WITH THE CHARACTER OF THE NEIGHBORHOOD WHERE IT IS PROPOSED, AND WITH THE SIZE AND LOCATIONS OF BUILDINGS IN THE VICINITY.**

A. The proposed multifamily development is appropriate for the location of the property along Schaad Road, a minor arterial street. There is a multifamily development of attached structures southwest of the subject property, with access from Ball Camp Pike.

B. There is a two-story house on the adjacent western parcel. The plan shows that the proposed three-story building 11 along the western boundary will be at least 35 ft from the property line, and that a retaining wall separates the building from the property boundary. A Type A landscape screening is proposed along the boundary to provide a buffer for the lower intensity use on the adjacent properties.

C. The abutting eastern properties are zoned I (Industrial) and currently undeveloped. If they are developed for any business or industrial uses under the I zone standards, a buffer yard will be required along the common boundaries with the subject parcel (Article 4.10.11).

**4) THE USE WILL NOT SIGNIFICANTLY INJURE THE VALUE OF ADJACENT PROPERTY BY NOISE, LIGHTS, FUMES, ODORS, VIBRATION, TRAFFIC CONGESTION OR OTHER IMPACTS WHICH MAY DETRACT FROM THE IMMEDIATE ENVIRONMENT.**

A. With the proposed building layout, landscape screening, and required road improvements, the multi-family development is not anticipated to significantly injure the value of adjacent properties.

**5) THE USE IS NOT OF A NATURE OR SO LOCATED AS TO DRAW SUBSTANTIAL ADDITIONAL TRAFFIC THROUGH RESIDENTIAL STREETS.**

A. Access will be via Schaad Road, a minor arterial street with capacity for additional traffic. Therefore, traffic through residential streets would not be required to access this property. A traffic impact analysis was conducted for this development, and the recommendations of the study shall be implemented, as mentioned in condition 2 of the development plan approval.

**6) THE NATURE OF DEVELOPMENT IN THE SURROUNDING AREA IS NOT SUCH AS TO POSE A POTENTIAL HAZARD TO THE PROPOSED USE OR TO CREATE AN UNDESIRABLE ENVIRONMENT FOR THE PROPOSED USE.**

A. There are no known uses in the area that could be a potential hazard or create an undesirable environment for the proposed residential use. As mentioned above, if neighboring property is developed for any business or industrial uses, a buffer yard shall be provided within the adjacent property.

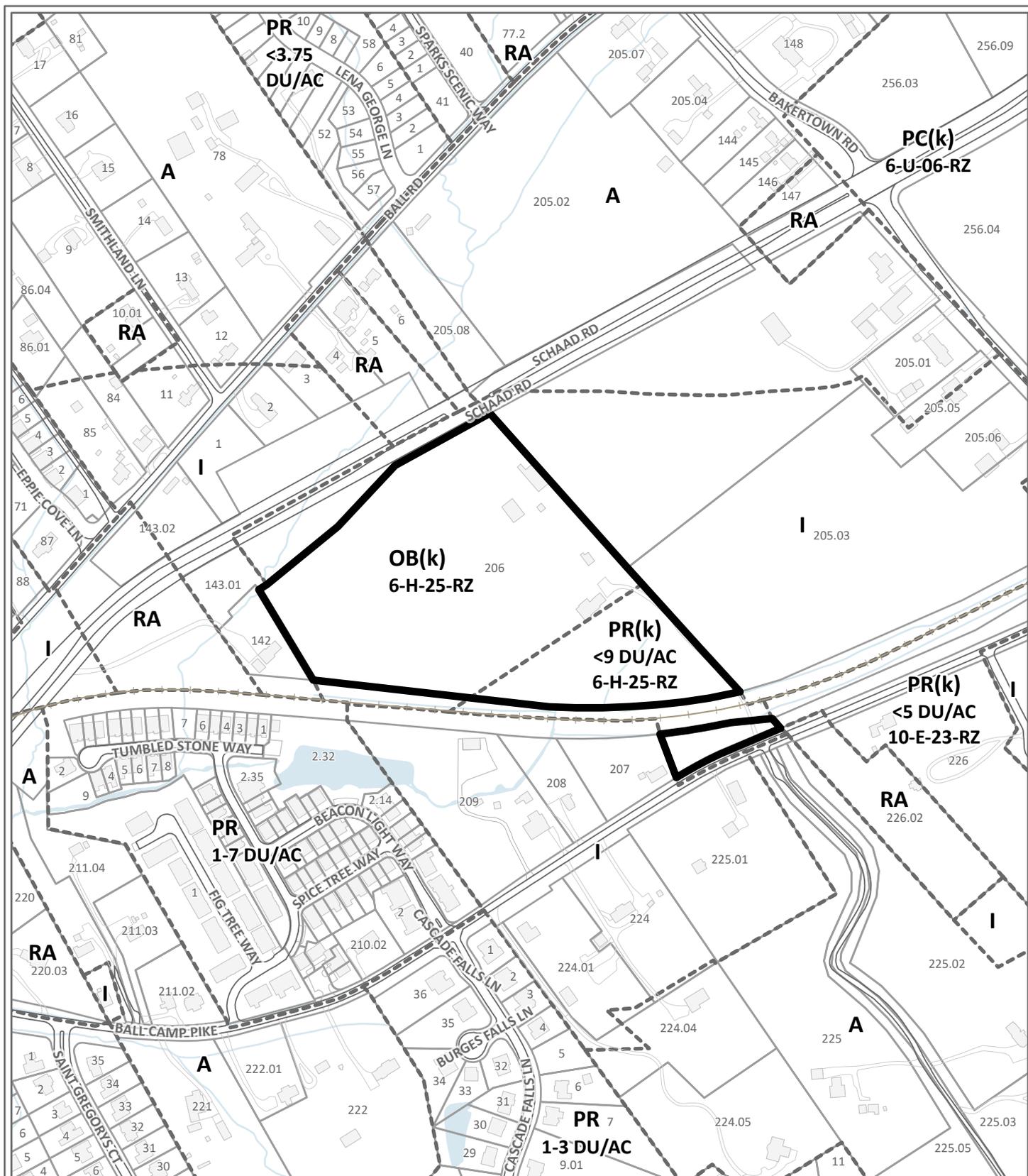
**ESTIMATED TRAFFIC IMPACT:** A traffic impact study was prepared by the applicant. The findings of that study were used in formulating the recommendations of this staff report.

**ESTIMATED STUDENT YIELD:** 26 (public school children, grades K-12)

Schools affected by this proposal: Karns Elementary, Karns Middle, and Karns High.

- Potential new school population is estimated using locally-derived data on public school student yield generated by new housing.
- Students are assigned to schools based on current attendance zones as determined by Knox County Schools. Students may request transfers to different zones, and zone boundaries are subject to change.
- Estimates presume full build-out of the proposed development. Build-out is subject to market forces, and timing varies widely from proposal to proposal.
- Student yields from new development do not reflect a net addition of children in schools. Additions occur incrementally over the build-out period. New students may replace current population that ages through the system or moves from the attendance zone.

The Planning Commission's approval or denial of this request is final, unless the action is appealed to a court of competent jurisdiction within thirty (30) days of the Planning Commission's decision.



**USE ON REVIEW**

**2-A-26-UR / 2-H-26-DP**



Multi-family Development (Revision of 11-D-25-DP / 11-C-25-UR) in OB(k) (Office, Medical, and Related Services, with conditions), PR(k) (Planned Residential, with conditions) at 9 du/ac

Original Print Date: 1/8/2026

Knoxville - Knox County Planning Commission \* City / County Building \* Knoxville, TN 37902

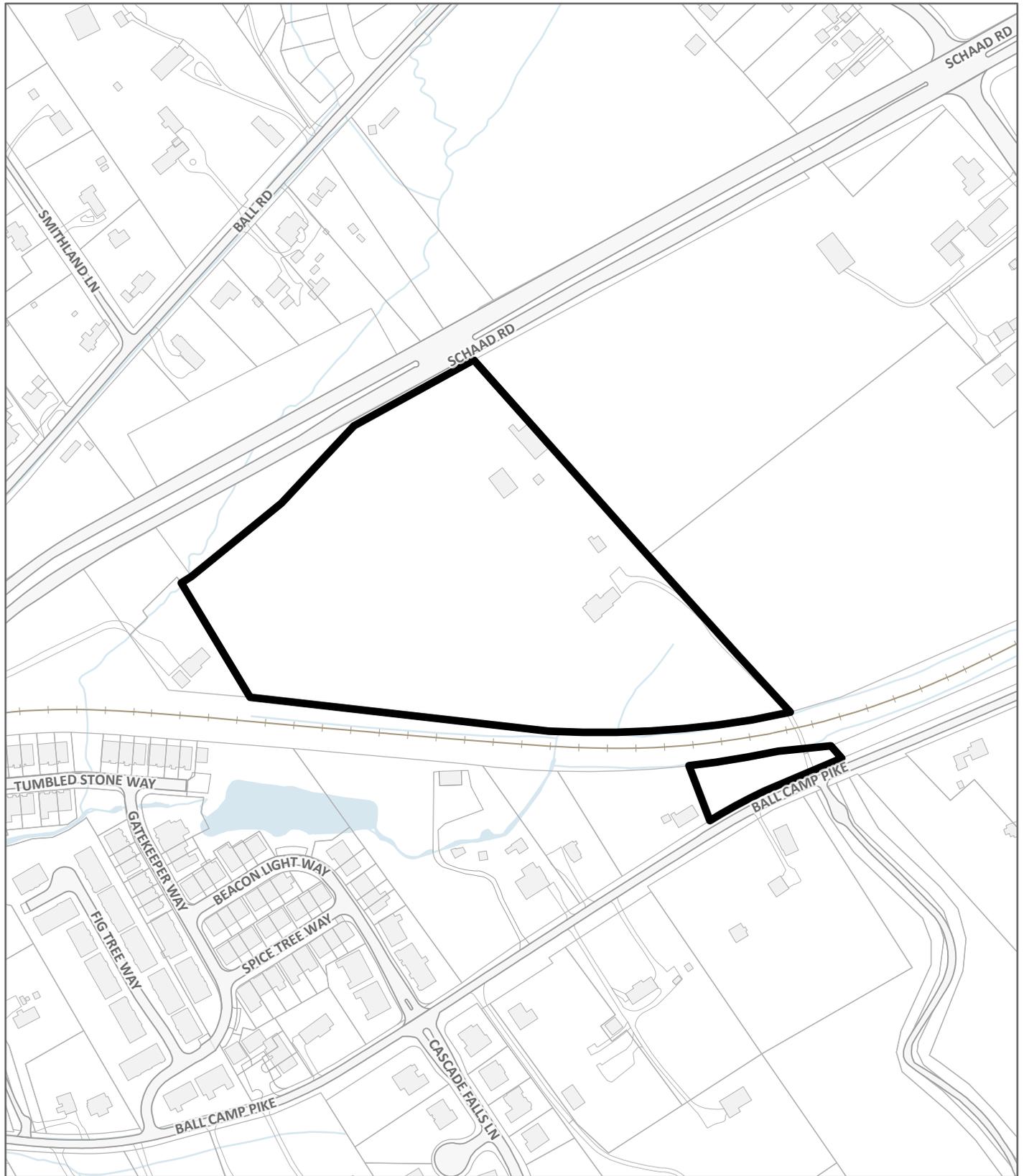
Petitioner: Thompson Thrift Development, Inc.

Map No: 91

Jurisdiction: County



# Exhibit A. Contextual Images

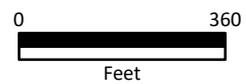


**LOCATION MAP**

**2-A-26-UR / 2-H-26-DP**



Case boundary



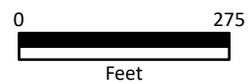
# Exhibit A. Contextual Images



## AERIAL MAP



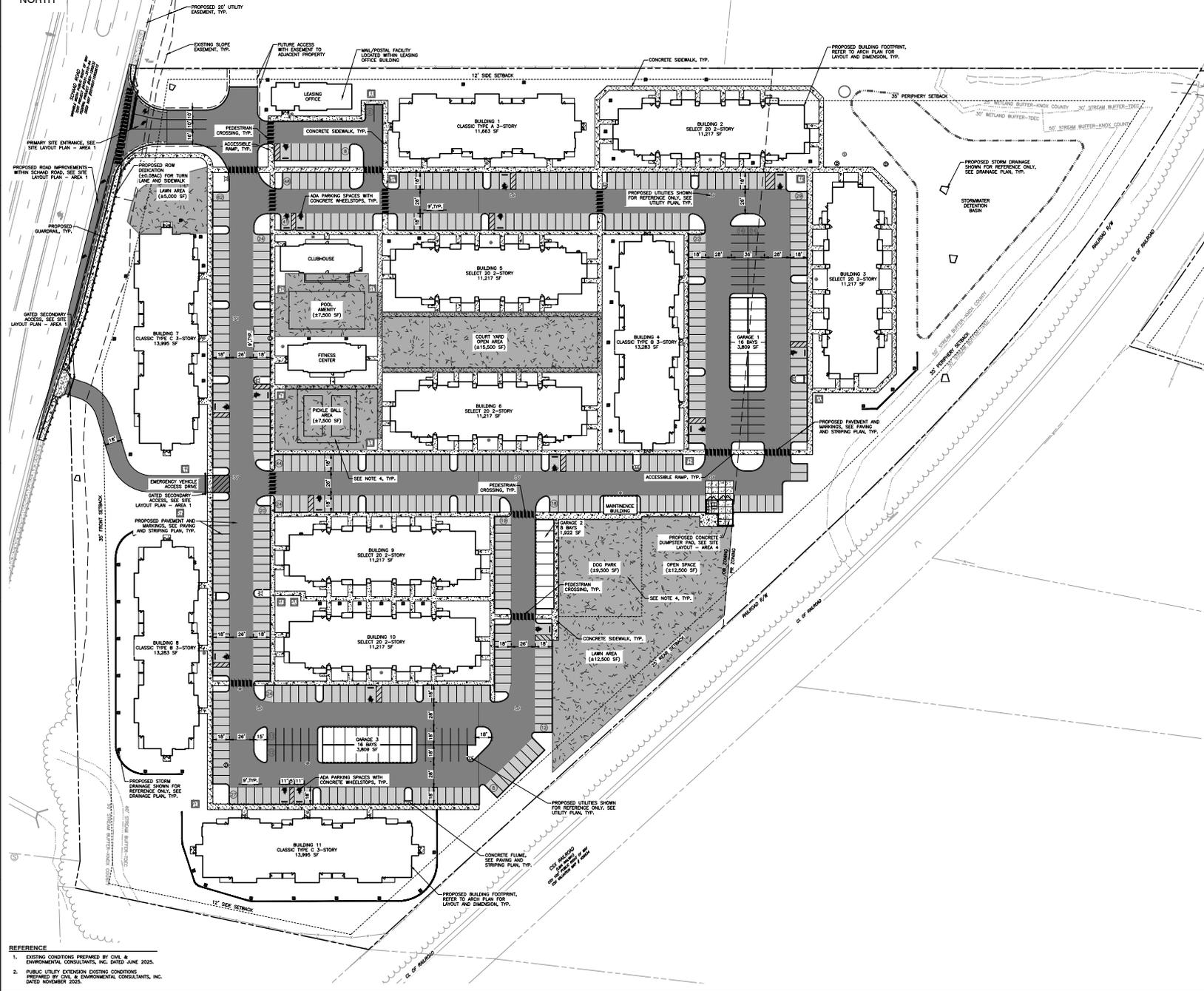
Case boundary





NORTH

2-A-26-UR / 2-H-26-DP  
1.20.2026



**LEGEND**

[Symbol]	EXISTING PROPERTY LINE
[Symbol]	EXISTING ADJACENT PROPERTY LINE
[Symbol]	EXISTING EASEMENT
[Symbol]	EXISTING METLAND/STREAM BUFFER
[Symbol]	EXISTING FENCE LINE
[Symbol]	EXISTING CURB
[Symbol]	EXISTING EDGE OF PAVEMENT
[Symbol]	EXISTING SIDEWALK
[Symbol]	EXISTING FENCE WALL
[Symbol]	EXISTING CURB INLET
[Symbol]	EXISTING SANITARY MANHOLE
[Symbol]	EXISTING GAS BOX (GAS VALVE)
[Symbol]	EXISTING GAS METER KEY
[Symbol]	PROPOSED EDGE OF PAVED DRIVE
[Symbol]	PROPOSED CURB
[Symbol]	PROPOSED REGULAR DUTY ASPHALT PAVEMENT
[Symbol]	PROPOSED HEAVY DUTY ASPHALT PAVEMENT
[Symbol]	PROPOSED STRIPING
[Symbol]	PROPOSED CONCRETE SIDEWALK
[Symbol]	PROPOSED CONCRETE PAVEMENT
[Symbol]	PROPOSED BUILDING
[Symbol]	PROPOSED ENTRANCE GATE
[Symbol]	PROPOSED STORMWATER RETENTION AREA
[Symbol]	PROPOSED AREA DRAIN (AD)
[Symbol]	PROPOSED CURB INLET (CI)
[Symbol]	PROPOSED DRAINAGE MANHOLE (DMH)
[Symbol]	PROPOSED HEADWALL (HW)
[Symbol]	PROPOSED SANITARY MANHOLE (SMH)
[Symbol]	PROPOSED FIRE HYDRANT (FH)
[Symbol]	PROPOSED TRANSFORMER
[Symbol]	OPEN SPACE/RECREATIONAL AREAS

- NOTES**
- SEE SHEET C100 FOR GENERAL NOTES.
  - CONTRACTOR SHALL REFER TO OTHER PLANS WITHIN THIS CONSTRUCTION SET FOR OTHER PERTINENT INFORMATION. IT IS NOT THE ENGINEER'S INTENT THAT ANY SINGLE PLAN SHEET IN THIS SET OF DOCUMENTS FULLY SERVE ALL WORK ASSOCIATED WITH THE PROJECT.
  - REFER TO ARCHITECTURAL PLAN FOR BUILDING FOOTPRINT AND LAYOUT. VERIFY DOOR/SWITCH LOCATIONS AND ELEVATIONS WITH ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.
  - REFER TO LANDSCAPE ARCHITECTURAL PLAN FOR PLANTINGS, ADORNMENTS, AND LANDSCAPING.
  - CURB RADIUS TO BE 4' UNLESS NOTED OTHERWISE.
  - REFER TO SITE LAYOUT PLANS FOR ADDITIONAL SITE DETAIL.
  - REFER TO PAVING AND STRIPING PLAN FOR PAVEMENT, CURB, AND STRIPING INFORMATION.
  - REFER TO UTILITY PLANS FOR UTILITY INFORMATION.
  - REFER TO DRAINAGE PLAN FOR STORM DRAIN INFORMATION.

**PARKING**

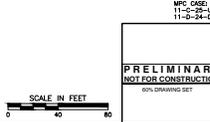
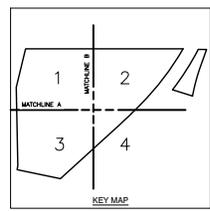
REQUIRED PARKING:

PER ARTICLE 3.50 OF KNOX COUNTY DEVELOPMENT CODE

FIRST 20 UNITS (1.0 X 20 UNITS)	= 20 SPACES
IF 20 MORE OR MORE UNITS (1.5 X 180 UNITS)	= 270 SPACES
TOTAL REQUIRED PARKING	= 290 SPACES
PROVIDED PARKING	465 SPACES
UNCOVERED PARKING	20 SPACES
GARAGE SPACES	325 SPACES
TOTAL PARKING PROVIDED	485 SPACES

**SITE DENSITY**

ON ZONE	15.73 ACRES
PROPOSED DWELLING UNITS	274 UNITS
PROPOSED DENSITY	18.46 DU/AC
PROPOSED BUILDING AREA	134,161 SF / 3.08 AC
PROPOSED BLDG COVERAGE	22%
ALIGNED BUILDING COVERAGE	25%
TOTAL PROPOSED OPEN SPACE	250,267 SF / 5.68 AC
AMENITY SPACE/REC COVERAGE	75,000 SF / 1.61 AC
AMENITY SPACE/REC COVERAGE	128
OTHER OPEN AREA	186,267 SF / 4.28 AC
TOTAL PROPOSED OPEN SPACE COVERAGE	438
REQUIRED OPEN SPACE/AREA	105 MH
FR ZONE	4.37 ACRES
PROPOSED DWELLING UNITS	26 DU'S
PROPOSED DENSITY	5.95 DU/AC
MAXIMUM DENSITY ALLOWED	9 DU/AC



**REFERENCE**

- EXISTING CONDITIONS PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC. DATED JUNE 2025.
- PUBLIC UTILITY EXTENSION EXISTING CONDITIONS PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC. DATED NOVEMBER 2025.

**REVISION RECORD**

NO.	DATE	DESCRIPTION

2704 Cherokee Farm Way  
Suite 101  
Knoxville, TN 37920  
Ph: 865 977 9997  
www.ccecinc.com

**CEC**  
Civil & Environmental  
Consultants, Inc.

**THOMPSON THRIFT RESIDENTIAL  
SCHAAD ROAD APARTMENTS  
8007 BALL CAMP PIKE  
KNOXVILLE, TN 37951**

**OVERALL SITE LAYOUT PLAN**

DATE:	11/18/2025	DESIGNED BY:	
DRAWN BY:		CHECKED BY:	
SCALE:		PROJECT NO.:	2612-26-UR
PROJECT:			

MPC CASE: 11-C-26-UR  
11-0-24-DP

**PRELIMINARY  
NOT FOR CONSTRUCTION**

**C400**





NORTH

**LEGEND**

---	EXISTING PROPERTY LINE
- - - -	EXISTING ADJACENT PROPERTY LINE
---	EXISTING EASEMENT
---	EXISTING WETLAND/STREAM BUFFER
---	EXISTING INDEX (MAJOR) CONTOUR
---	EXISTING INTERMEDIATE (MINOR) CONTOUR
---	EXISTING CURB
---	EXISTING STRUCTURE
---	EXISTING STORM PIPE
---	EXISTING GAS LINE
---	EXISTING OVERHEAD WIRE
---	EXISTING ADA PARKING SYMBOL
---	EXISTING DRAIN
---	EXISTING BOX (GAS VALVE)
---	EXISTING GAS METER KEY
---	PROPOSED EDGE OF PAVED DRIVE
---	PROPOSED CURB
---	PROPOSED REGULAR DUTY ASPHALT PAVEMENT
---	PROPOSED HEAVY DUTY ASPHALT PAVEMENT
---	PROPOSED STRIPING
---	PROPOSED CONCRETE SIDEWALK
---	PROPOSED INDEX (MAJOR) CONTOUR
---	PROPOSED INTERMEDIATE (MINOR) CONTOUR
---	PROPOSED STORM PIPE
---	PROPOSED GATCH BASIN
---	PROPOSED CURB INLET
---	PROPOSED STORM MANHOLE
---	PROPOSED HEADWALL

**NOTES**

- CONTRACTOR SHALL REFER TO OTHER PLANS WITHIN THE CONSTRUCTION SET FOR OTHER PERTINENT INFORMATION. IT IS NOT THE ENGINEER'S INTENT THAT ANY SINGLE PLAN SHEET BE THE BASIS FOR CONSTRUCTION. ALL WORK ASSOCIATED WITH THE PROJECT.
- PROPOSED GRADINGS SHOWN ARE FINAL GRADES, TOP OF GRADE LEVEL, TOP OF PAVEMENT, OR GROUND ELEVATION AT THE BOUNDARY THEREOF.
- THE ACCURACY OF THE GRADINGS IS BASED ON THE DATA PROVIDED BY THE OWNER OR OWNER'S REPRESENTATIVE. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AS NECESSARY.
- REFER TO AND FOLLOW THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT.
- ERTHOUS SHALL INCLUDE CLEARING AND GRUBBING, STRIPPING AND STOCKPILING TOPSOIL, MASS GRADING, EXCAVATION, BACKFILLING, CUT AND FILL, CUT AND FILL, AND COMPACTION, AS RECOMMENDED BY THE GEOTECHNICAL ENGINEER/POB REQUIREMENTS DESCRIBED HEREIN.
- A 6 IN. (MIN.) LAYER OF TOPSOIL SHALL BE PLACED OVER THE AREA TO BE SEED/GRASSED AND TO THE FINISH GRADE ELEVATIONS AS SHOWN ON THE DRAWINGS.
- PAVEMENT SLOPES AROUND ACCESSIBLE PARKING SPACES AND ALLOWING ACCESS AREAS SHALL BE MINIMUM 2% AND SHALL CONFORM TO THE LATEST REGULATIONS OF THE AMERICANS WITH DISABILITIES ACT.
- SLOPES IN NON-PAVED AREAS SHALL BE 3:1 (HORIZONTAL:VERTICAL) UNLESS OTHERWISE NOTED OTHERWISE.
- COMPACTED FILLS ARE TO BE MADE TO A MINIMUM OF THREE FEET ABOVE THE BOTTOM OF THE EXISTING PAVEMENT TO CUTTING OF BENCHES FOR PLACEMENT OF SAND SENSITIVE FILL MATERIAL SHALL BE CONTROLLED, PLACED, COMPACTED, AND INSPECTED BY AN APPROVED TESTING LABORATORY OR AN INSPECTOR FROM THE APPROPRIATE GOVERNMENT AGENCY.
- EXCESS SOIL MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND UNLESS OTHERWISE SPECIFIED, SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE OWNER AND IN ACCORDANCE WITH LOCAL AND STATE REQUIREMENTS.
- THE CONTRACTOR IS RESPONSIBLE FOR BALANCING THE SITE EARTHWORK BY IMPORTING OR EXPORTING AS NECESSARY TO ACHIEVE DESIGN GRADINGS AND SPECIFICATIONS.
- CEC AND THE OWNER ARE NOT RESPONSIBLE FOR THE CONTRACTOR'S INTERPRETATION OF TOPSOIL THICKNESS, SOIL EXPANSION/SHRINKAGE, TRENCH MATERIALS, ETC. WHICH MAY AFFECT THE PROJECT'S EARTHWORK VOLUMES.
- ADJUST RECONSTRUCT ALL EXISTING STRUCTURES, CATCHMENTS, CLEANOUTS, ETC. WITHIN PROJECT AREA TO MATCH PROPOSED GRADINGS AS NECESSARY.
- PROVIDE A SMOOTH TRANSITION BETWEEN EXISTING PAVEMENT AND NEW PAVEMENT. FIELD ADJUST GRADINGS AS NECESSARY. INSTALL ALL UTILITIES, INCLUDING MINOR SLOTTING, PRIOR TO INSTALLATION OF PAVED SURFACES.
- TRIES, STUMPS, WOODS, AND OTHER OBSTRUCTIONS SHALL BE REMOVED TO A DEPTH OF 2' BELOW THE SUBGRADE. EXISTING UTILITIES SHALL BE MAINTAINED TO A DEPTH OF 1' FT BELOW SUBGRADE.
- VERIFY GRADINGS WHEREVER NECESSARY TO BRING THE PROPOSED INDEX ELEVATIONS, SLOPES, AND CROSS-SECTIONS OF THE FINISHING ABOVE OR BELOW THAT AS SHOWN ON THE PLANS. SURFACES TO BE FINISHED TO FINISH GRADINGS 0.04", PAVEMENTS 0.04", AND BUILDINGS 0.04".
- PROVIDE NECESSARY MEASURES TO KEEP THE SITE FREE-DRAINING AND DO NOT ALLOW WATER TO ACCUMULATE IN EXCAVATIONS OR POND ON-SITE.
- ONCE SURFACES HAVE BEEN ACHIEVED, CONTRACTOR SHALL PROTECT AND MAINTAIN GRADINGS UNTIL PLACEMENT OF THE FINAL GRADES.
- ENSURE ADEQUATE OVERHEAD CLEARANCE (14'-0") FROM PROPOSED GRADINGS PROVIDED FOR OVERHEAD UTILITY LINES IN VEHICULAR AREAS, COORDINATE WITH LOCAL UTILITY AUTHORITY IF CLEARANCE IS NOT AVAILABLE.

**GRADING NOTES**

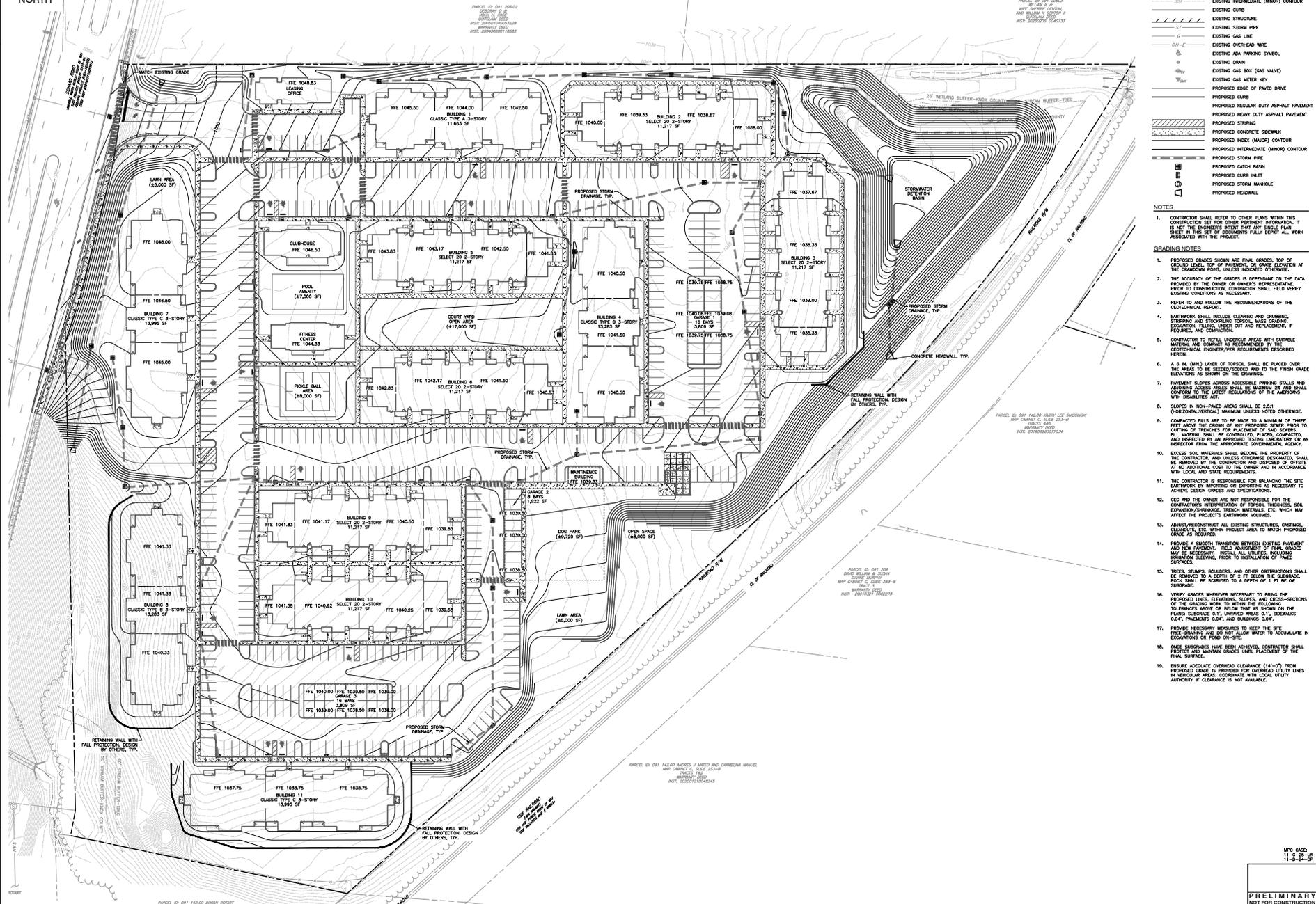
- PROPOSED GRADINGS SHOWN ARE FINAL GRADES, TOP OF GRADE LEVEL, TOP OF PAVEMENT, OR GROUND ELEVATION AT THE BOUNDARY THEREOF.
- THE ACCURACY OF THE GRADINGS IS BASED ON THE DATA PROVIDED BY THE OWNER OR OWNER'S REPRESENTATIVE. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AS NECESSARY.
- REFER TO AND FOLLOW THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT.
- ERTHOUS SHALL INCLUDE CLEARING AND GRUBBING, STRIPPING AND STOCKPILING TOPSOIL, MASS GRADING, EXCAVATION, BACKFILLING, CUT AND FILL, CUT AND FILL, AND COMPACTION, AS RECOMMENDED BY THE GEOTECHNICAL ENGINEER/POB REQUIREMENTS DESCRIBED HEREIN.
- A 6 IN. (MIN.) LAYER OF TOPSOIL SHALL BE PLACED OVER THE AREA TO BE SEED/GRASSED AND TO THE FINISH GRADE ELEVATIONS AS SHOWN ON THE DRAWINGS.
- PAVEMENT SLOPES AROUND ACCESSIBLE PARKING SPACES AND ALLOWING ACCESS AREAS SHALL BE MINIMUM 2% AND SHALL CONFORM TO THE LATEST REGULATIONS OF THE AMERICANS WITH DISABILITIES ACT.
- SLOPES IN NON-PAVED AREAS SHALL BE 3:1 (HORIZONTAL:VERTICAL) UNLESS OTHERWISE NOTED OTHERWISE.
- COMPACTED FILLS ARE TO BE MADE TO A MINIMUM OF THREE FEET ABOVE THE BOTTOM OF THE EXISTING PAVEMENT TO CUTTING OF BENCHES FOR PLACEMENT OF SAND SENSITIVE FILL MATERIAL SHALL BE CONTROLLED, PLACED, COMPACTED, AND INSPECTED BY AN APPROVED TESTING LABORATORY OR AN INSPECTOR FROM THE APPROPRIATE GOVERNMENT AGENCY.
- EXCESS SOIL MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND UNLESS OTHERWISE SPECIFIED, SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE OWNER AND IN ACCORDANCE WITH LOCAL AND STATE REQUIREMENTS.
- THE CONTRACTOR IS RESPONSIBLE FOR BALANCING THE SITE EARTHWORK BY IMPORTING OR EXPORTING AS NECESSARY TO ACHIEVE DESIGN GRADINGS AND SPECIFICATIONS.
- CEC AND THE OWNER ARE NOT RESPONSIBLE FOR THE CONTRACTOR'S INTERPRETATION OF TOPSOIL THICKNESS, SOIL EXPANSION/SHRINKAGE, TRENCH MATERIALS, ETC. WHICH MAY AFFECT THE PROJECT'S EARTHWORK VOLUMES.
- ADJUST RECONSTRUCT ALL EXISTING STRUCTURES, CATCHMENTS, CLEANOUTS, ETC. WITHIN PROJECT AREA TO MATCH PROPOSED GRADINGS AS NECESSARY.
- PROVIDE A SMOOTH TRANSITION BETWEEN EXISTING PAVEMENT AND NEW PAVEMENT. FIELD ADJUST GRADINGS AS NECESSARY. INSTALL ALL UTILITIES, INCLUDING MINOR SLOTTING, PRIOR TO INSTALLATION OF PAVED SURFACES.
- TRIES, STUMPS, WOODS, AND OTHER OBSTRUCTIONS SHALL BE REMOVED TO A DEPTH OF 2' BELOW THE SUBGRADE. EXISTING UTILITIES SHALL BE MAINTAINED TO A DEPTH OF 1' FT BELOW SUBGRADE.
- VERIFY GRADINGS WHEREVER NECESSARY TO BRING THE PROPOSED INDEX ELEVATIONS, SLOPES, AND CROSS-SECTIONS OF THE FINISHING ABOVE OR BELOW THAT AS SHOWN ON THE PLANS. SURFACES TO BE FINISHED TO FINISH GRADINGS 0.04", PAVEMENTS 0.04", AND BUILDINGS 0.04".
- PROVIDE NECESSARY MEASURES TO KEEP THE SITE FREE-DRAINING AND DO NOT ALLOW WATER TO ACCUMULATE IN EXCAVATIONS OR POND ON-SITE.
- ONCE SURFACES HAVE BEEN ACHIEVED, CONTRACTOR SHALL PROTECT AND MAINTAIN GRADINGS UNTIL PLACEMENT OF THE FINAL GRADES.
- ENSURE ADEQUATE OVERHEAD CLEARANCE (14'-0") FROM PROPOSED GRADINGS PROVIDED FOR OVERHEAD UTILITY LINES IN VEHICULAR AREAS, COORDINATE WITH LOCAL UTILITY AUTHORITY IF CLEARANCE IS NOT AVAILABLE.

**SCALE IN FEET**

0 20 40 80

**PRELIMINARY**  
NOT FOR CONSTRUCTION

MFC CASE:  
11-C-24-11  
11-0-24-00



**REFERENCE**

- EXISTING CONDITIONS PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC. DATED JUNE 2025.

**REVISION RECORD**

NO.	DATE	DESCRIPTION

2704 Charlotte Farm Way  
Suite 101  
Knoxville, TN 37920  
Ph: 865 977 9997  
www.cecinc.com

**CEC**  
Civil & Environmental  
Consultants, Inc.

**THOMPSON THRIFT RESIDENTIAL**  
SCHAAD ROAD APARTMENTS  
8007 BALL CAMP PIKE  
KNOXVILLE, TN 37951

**OVERALL GRADING PLAN**

DATE: 12/29/2025  
DRAWN BY: A.S. BROWN  
CHECKED BY: S.M. BROWN  
SCALE: 1/8" = 1'-0"

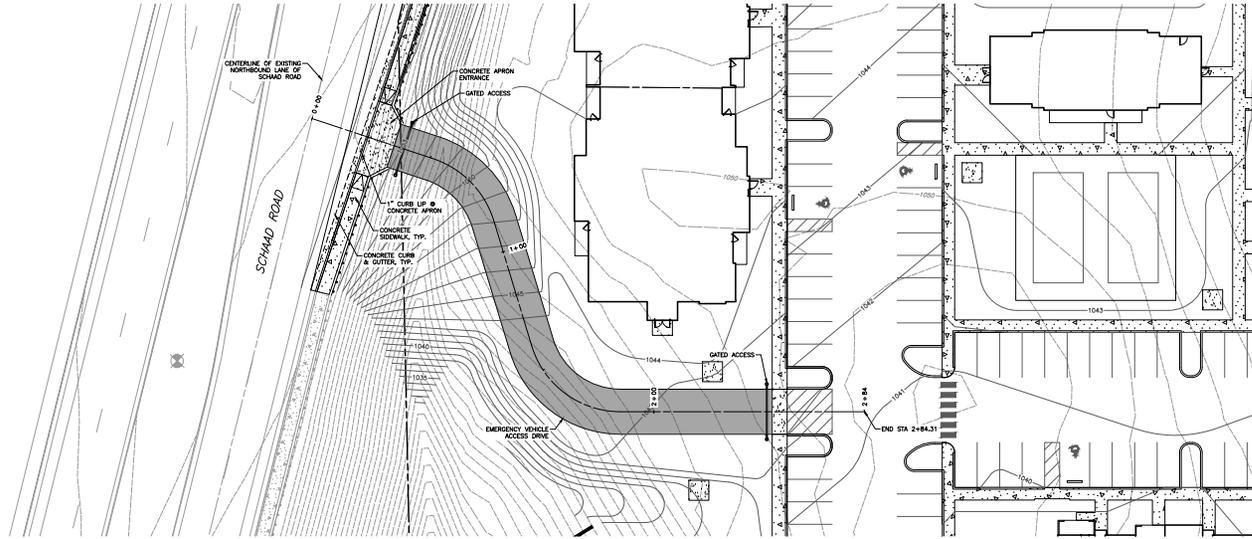
**C500**



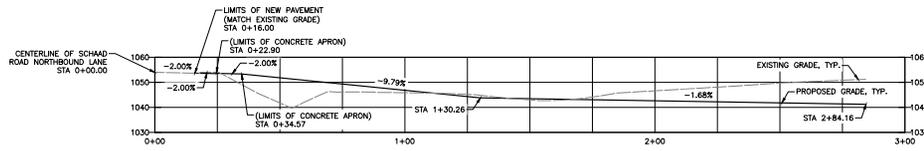
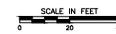




NORTH



EMERGENCY VEHICLE ACCESS DRIVE PLAN  
SCALE 1"=20'  
STA 0+00 TO 3+00



EMERGENCY VEHICLE ACCESS DRIVE PROFILE  
SCALE 1"=20' V=1"=20'  
STA 0+00 TO 3+00

LEGEND

---	EXISTING PROPERTY LINE
---	EXISTING ADJACENT PROPERTY LINE
---	EXISTING INDEX (MAJOR) CONTOUR
---	EXISTING INTERMEDIATE (MINOR) CONTOUR
---	EXISTING CURB
---	EXISTING EDGE OF PAVEMENT
---	EXISTING SIDEWALK
---	EXISTING ROAD CENTERLINE
---	PROPOSED ROAD CENTERLINE
---	PROPOSED CONCRETE SIDEWALK
---	PROPOSED CONCRETE PAVEMENT
---	PROPOSED BUILDING
---	PROPOSED ENTRANCE GATE
---	PROPOSED SUBGRADE
---	PROPOSED INDEX (MAJOR) CONTOUR
---	PROPOSED INTERMEDIATE (MINOR) CONTOUR
---	PROPOSED EMERGENCY VEHICLE ACCESS DRIVE

DATE	11/05/2024
BY	WJL
CHECKED BY	WJL
SCALE	AS SHOWN
PROJECT NO.	11-0-24-04
DRAWING NO.	302.00
TITLE	EMERGENCY VEHICLE ACCESS DRIVE PLAN

2704 Cherokee Firm Way  
Suite 101  
Knoxville, TN 37920  
Ph: 865 977 9997  
www.cecinc.com



THOMPSON THRIFT RESIDENTIAL  
SCHAAD ROAD APARTMENTS  
8007 BALL CAMP PIKE  
KNOXVILLE, TN 37951

EMERGENCY VEHICLE ENTRANCE PLAN AND PROFILE  
DATE: 11/05/2024  
BY: WJL  
CHECKED BY: WJL  
SCALE: AS SHOWN  
PROJECT NO.: 11-0-24-04  
DRAWING NO.: 302.00  
TYPED BY: WJL

MFC CASE:  
11-0-24-04  
11-0-24-04

PRELIMINARY  
NOT FOR CONSTRUCTION

EXHIBIT

REFERENCE  
1. EXISTING CONDITIONS PREPARED BY CIVIL & ENVIRONMENTAL CONSULTANTS, INC. DATED JUNE 2023.

I:\Projects\2024\11-0-24-04\THOMPSON THRIFT RESIDENTIAL\8007 BALL CAMP PIKE\EMERGENCY VEHICLE ACCESS DRIVE PLAN.dwg - 11/05/2024 10:57 AM







VIEW LOOKING AT APARTMENT BUILDING TYPE A 3-STORY FRONT ELEVATION

COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.1a

MPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE A 3-STORY FRONT ELEVATION

COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.1b

MPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE B 3-STORY FRONT ELEVATION

COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TTRÉS AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.2a

REV. DATE  
11-9-25-18  
11-9-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE B 3-STORY FRONT ELEVATION

COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.2b

REVISED  
11-15-24-08  
11-19-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE C 3-STORY FRONT ELEVATION

COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.3a

MPC CASE:  
11-0-24-08  
11-0-24-08



VIEW LOOKING AT APARTMENT BUILDING TYPE C 3-STORY FRONT ELEVATION

COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.3b

MPC CASE:  
11-0-24-08  
11-0-24-08



VIEW LOOKING AT APARTMENT BUILDING TYPE A 3-STORY REAR ELEVATION

COLOR SCHEME 1

**STUDIO M**  
ARCHITECTURE & PLANNING

**THOMPSON  
THRIFT**



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

**A1.4a**

MPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE A 3-STORY REAR ELEVATION

COLOR SCHEME 2

**STUDIO M**  
ARCHITECTURE & PLANNING

**THOMPSON  
THRIFT**



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

**A1.4b**

WPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE B 3-STORY REAR ELEVATION

COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.5a

MPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE B 3-STORY REAR ELEVATION

COLOR SCHEME 2

**STUDIO M**  
ARCHITECTURE & PLANNING

**THOMPSON  
THRIFT**



SCHEMATIC  
DESIGN  
09.29.2025

TRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

**A1.5b**

MPC CASE:  
11-0-24-08  
11-0-24-08



VIEW LOOKING AT APARTMENT BUILDING TYPE C 3-STORY REAR ELEVATION

COLOR SCHEME 1

**STUDIO M**  
ARCHITECTURE & PLANNING

**THOMPSON  
THRIFT**



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

**A1.6a**

WPC: GSK  
11-0-24-108  
11-0-24-09



VIEW LOOKING AT APARTMENT BUILDING TYPE C 3-STORY REAR ELEVATION

COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.6b

MPC CASE:  
11-0-24-08  
11-0-24-08



VIEW LOOKING AT SELECT20 APARTMENT FRONT ELEVATION - COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.7a

WPC: GSK  
11-0-23-08  
11-0-24-09



VIEW LOOKING AT SELECT20 APARTMENT FRONT ELEVATION - COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.7b

MPC CASE:  
11-0-24-08  
11-0-24-08



VIEW LOOKING AT SELECT20 APARTMENT FRONT ELEVATION - COLOR SCHEME 1

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.8a

WPC CASE  
11-0-25-08  
11-0-24-09



VIEW LOOKING AT SELECT20 APARTMENT FRONT ELEVATION - COLOR SCHEME 2

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT



SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.8b

WPC CASE:  
11-0-23-08  
11-0-24-09



VIEW LOOKING TOWARD CLUBHOUSE BUILDING

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TTRRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.9

MPC CASE:  
11-0-24-08  
11-0-24-09



VIEW LOOKING TOWARD LEASING BUILDING

STUDIO M  
ARCHITECTURE & PLANNING

THOMPSON  
THRIFT

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

A1.10

WPC CASE:  
11-0-23-18  
11-0-24-07



VIEW LOOKING AT FITNESS FRONT ELEVATION

**STUDIO M**  
ARCHITECTURE & PLANNING

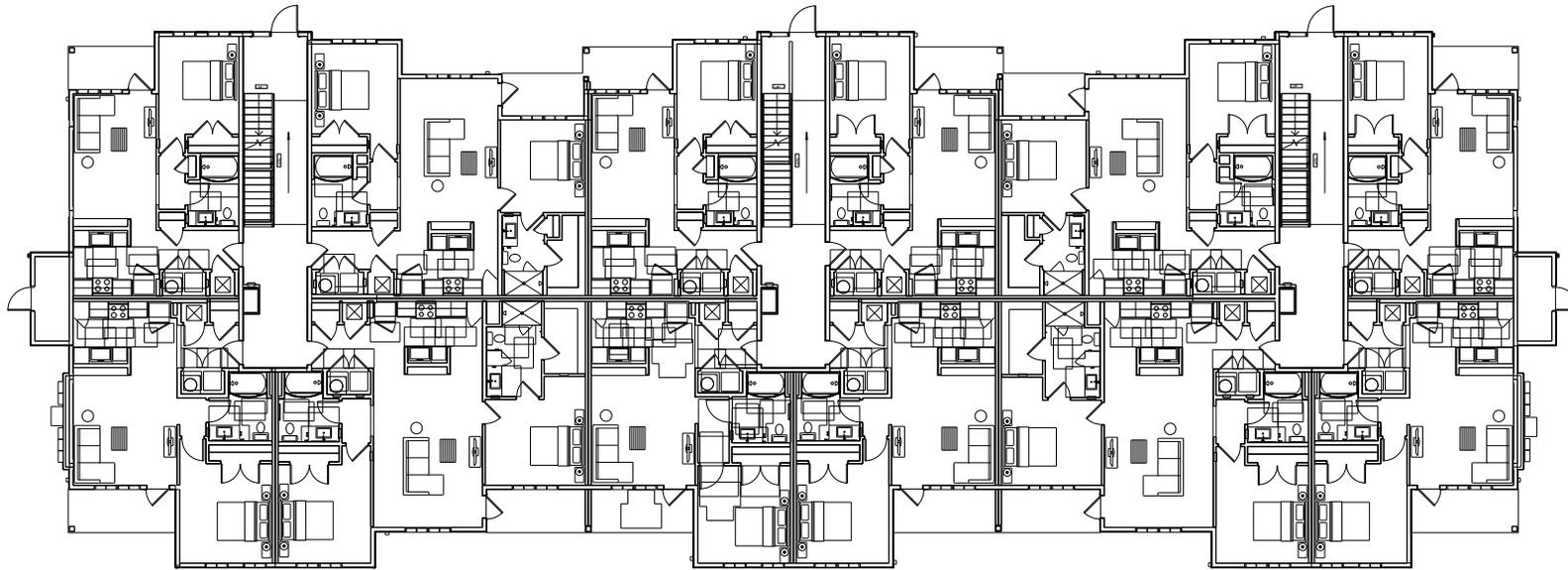
**THOMPSON  
THRIFT**

SCHEMATIC  
DESIGN  
09.29.2025

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

**A1.11**

WPC CASE:  
11-0-23-08  
11-0-24-09



Type A Building First Level Floor Plan

Scale: 1/8" = 1'-0"

A2.1

WPC CASE:  
11-0-24-08  
11-0-24-09

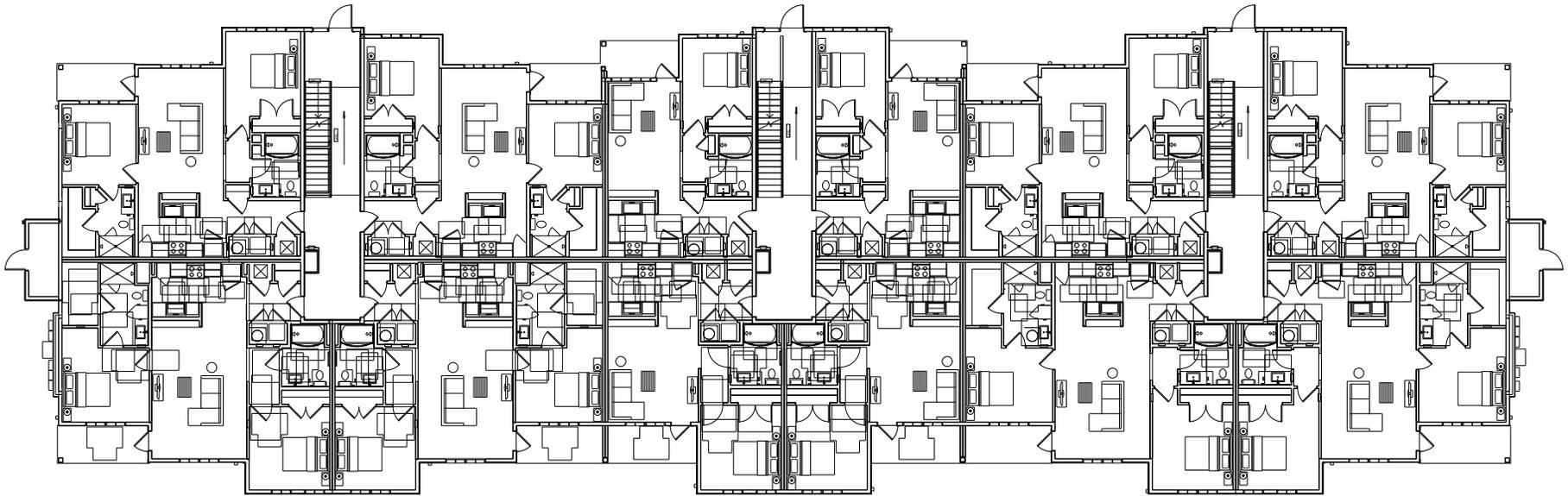
TRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

SCHEMATIC  
DESIGN  
09.29.2025



THOMPSON  
THRIFT

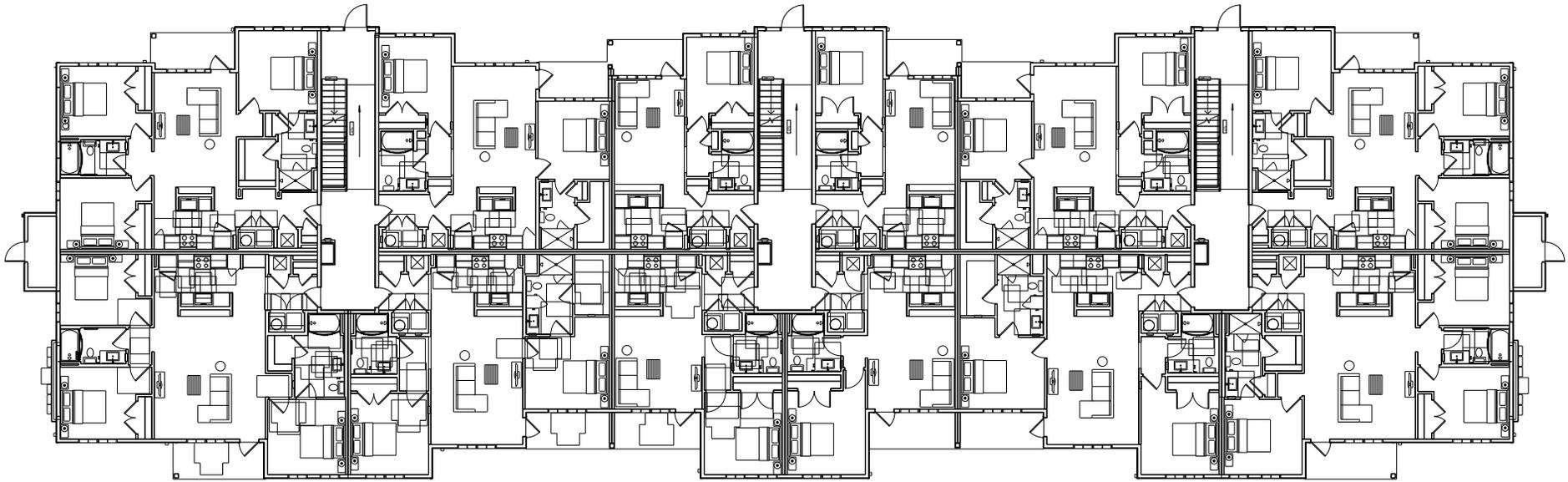
STUDIO M  
ARCHITECTURE & PLANNING



Type B Building First Level Floor Plan

Scale: 1/8" = 1'-0"





Type C Building First Level Floor Plan

Scale: 1/8" = 1'-0"

A2.3

TRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

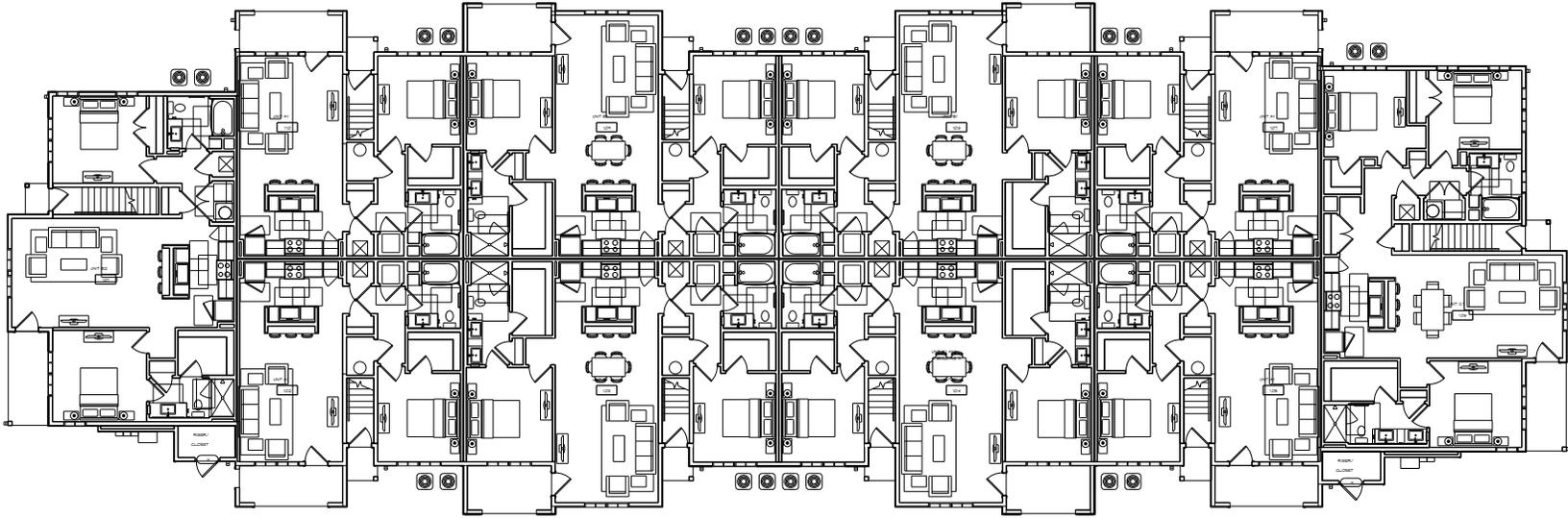
SCHEMATIC  
DESIGN  
09.29.2025



THOMPSON  
THRIFT

STUDIO M  
ARCHITECTURE & PLANNING

WPC CASE:  
11-C-23-08  
11-0-24-09



SELECT20 - FIRST FLOOR PLAN  
Scale: 1/8" = 1'-0"

TITRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

SCHEMATIC  
DESIGN  
09.29.2025

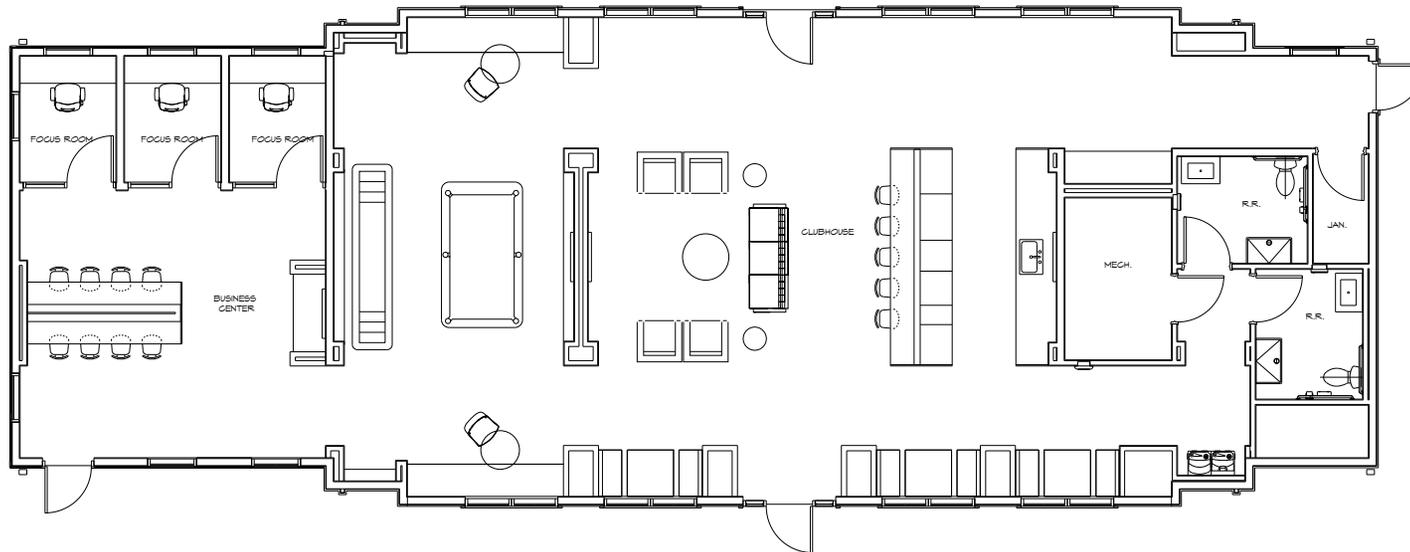


THOMPSON  
THRIFT

STUDIO M  
ARCHITECTURE & PLANNING

A2.4

WPC CASE:  
11-0-24-08  
11-0-24-09



CLUBHOUSE BUILDING FLOOR PLAN

SCALE: 1/4" = 1'-0"

A2.5

WPC DATE:  
11-0-23-18  
11-0-24-09

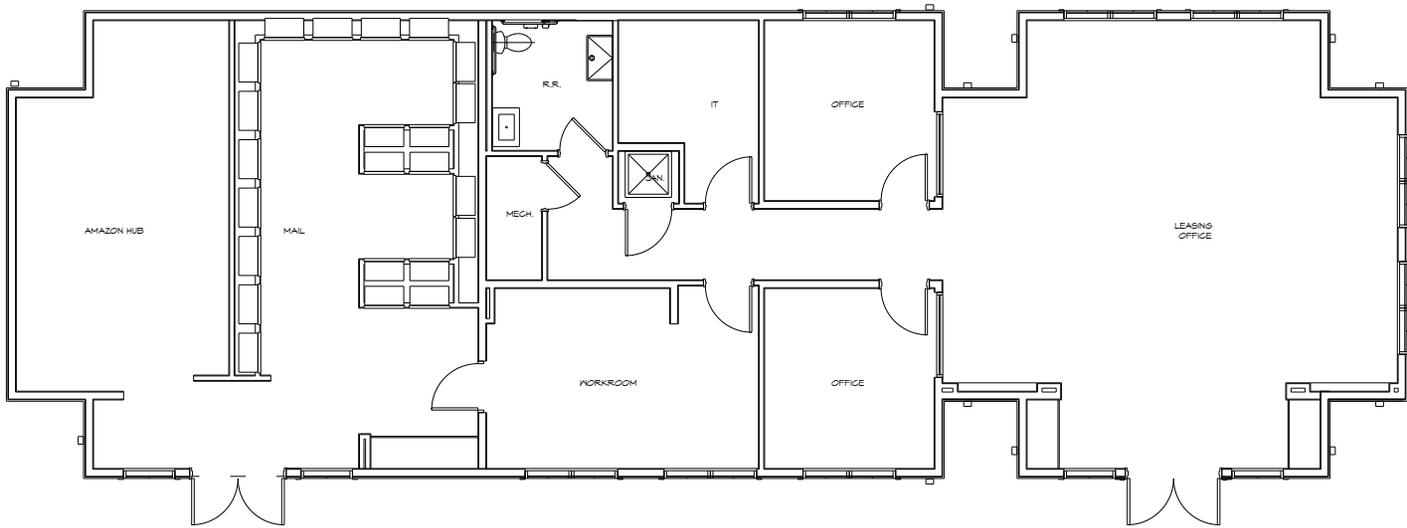
TRES AT WEST  
LAFAYETTE  
WEST LAFAYETTE, INDIANA

SCHEMATIC  
DESIGN  
09.29.2025



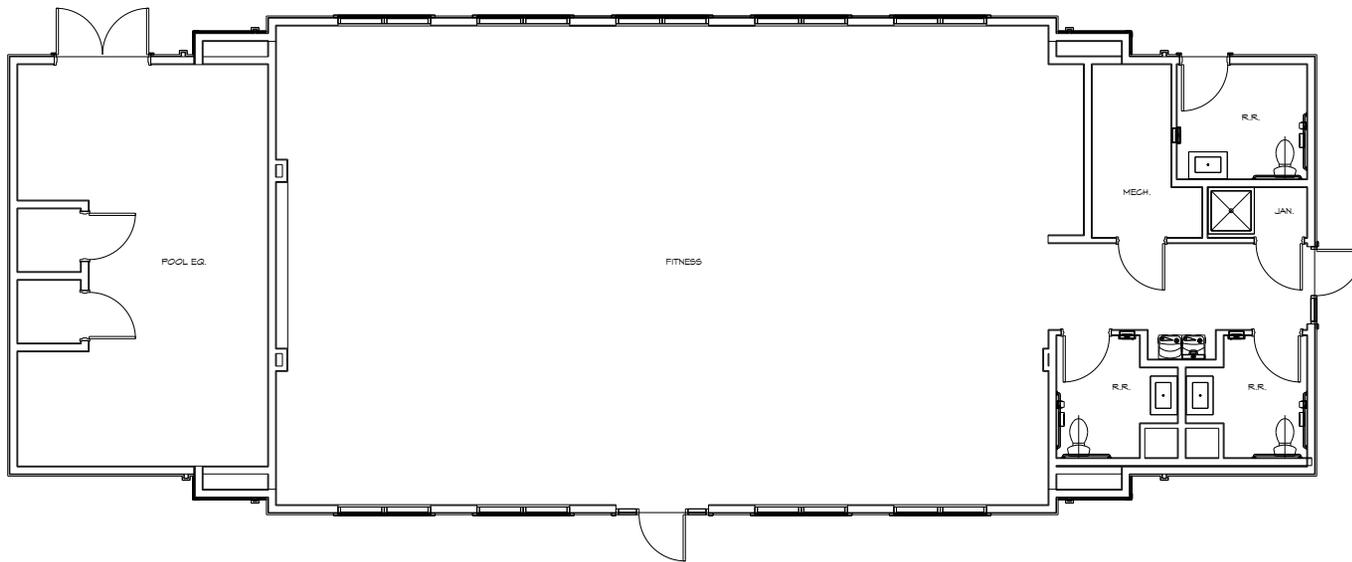
THOMPSON  
THRIFT

STUDIO M  
ARCHITECTURE & PLANNING



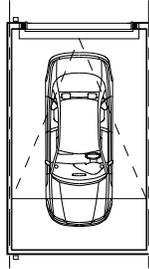
LEASING BUILDING FLOOR PLAN

SCALE: 1/4" = 1'-0"

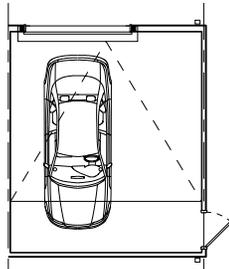


FITNESS BUILDING FLOOR PLAN  
SCALE: 1/4" = 1'-0"

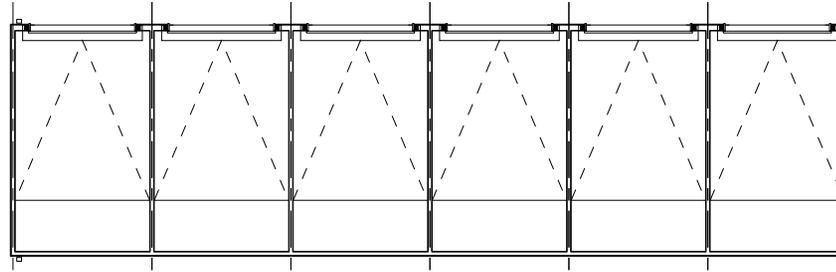




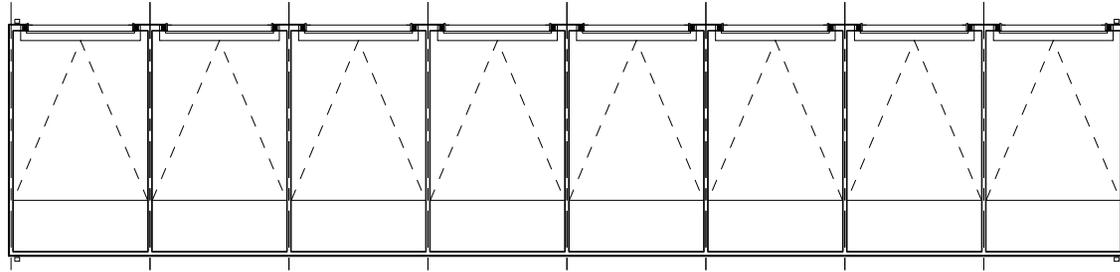
TYPICAL GARAGE BAY FLOOR PLAN  
Scale: 1/4" = 1'-0"



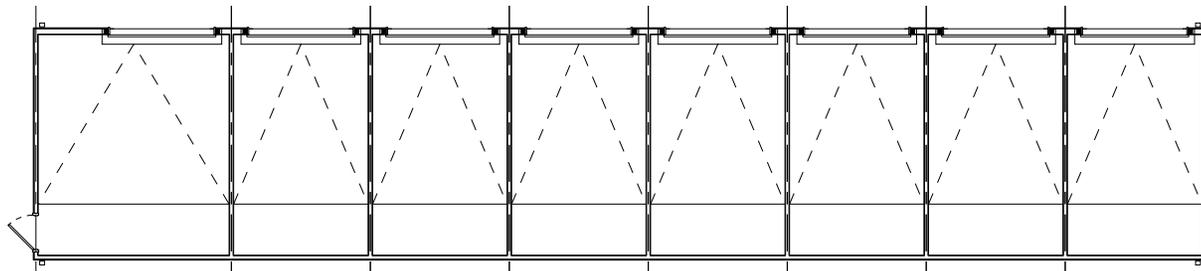
TYPICAL ANSI GARAGE BAY FLOOR PLAN  
Scale: 1/4" = 1'-0"



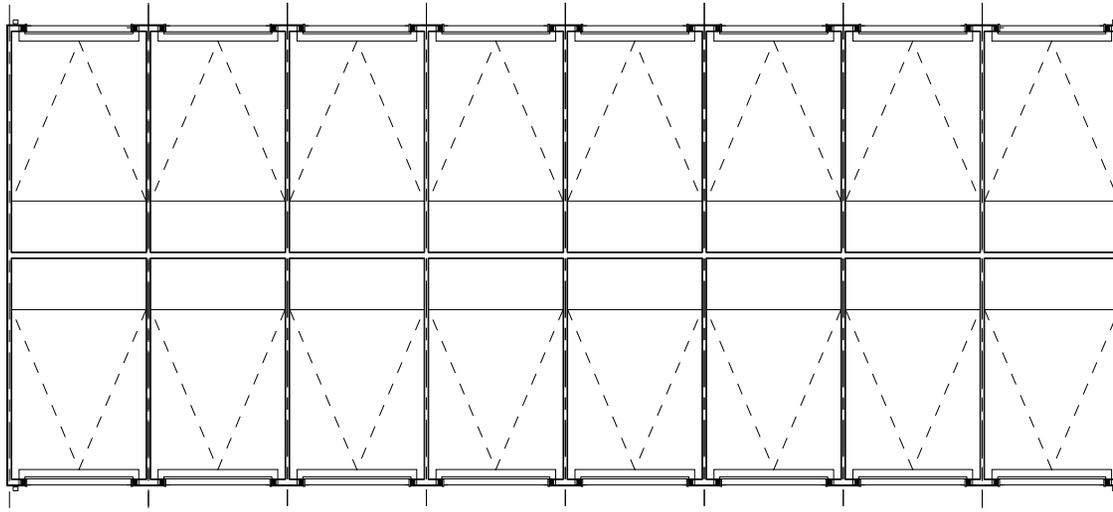
6-BAY GARAGE FLOOR PLAN  
Scale: 3/16" = 1'-0"



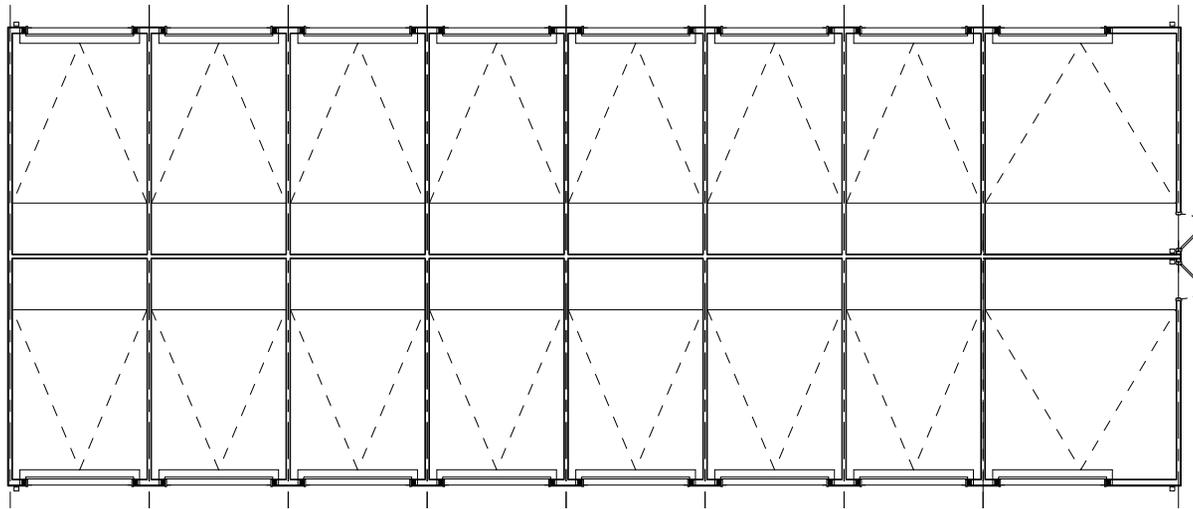
8-BAY GARAGE FLOOR PLAN  
Scale: 3/16" = 1'-0"



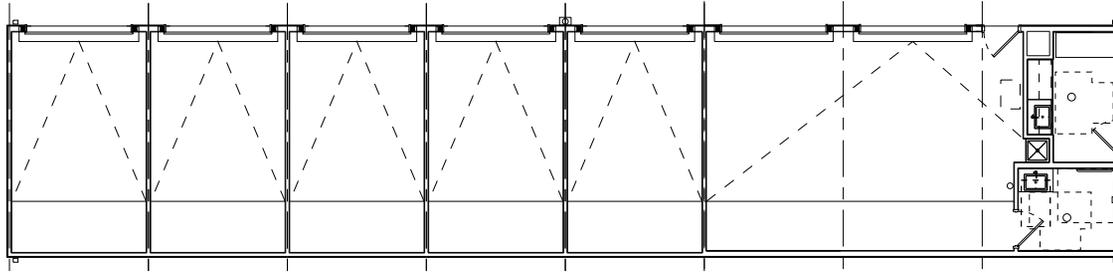
8-BAY ANSI GARAGE FLOOR PLAN  
Scale: 3/16" = 1'-0"



16-BAY GARAGE FLOOR PLAN  
Scale: 3/16" = 1'-0"

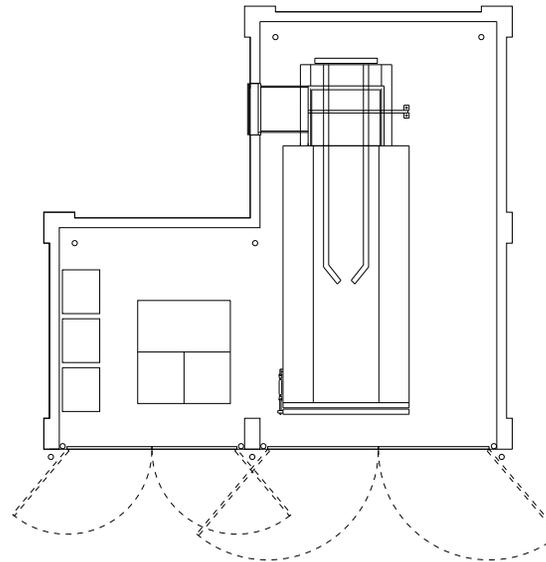


16-BAY ANSI GARAGE FLOOR PLAN  
Scale: 3/16" = 1'-0"



5-BAY GARAGE + PROPERTY MAINTENANCE FACILITY FLOOR PLAN

Scale: 3/16" = 1'-0"



TRASH ENCLOSURE FLOOR PLAN

SCALE: 1/4" = 1'-0"

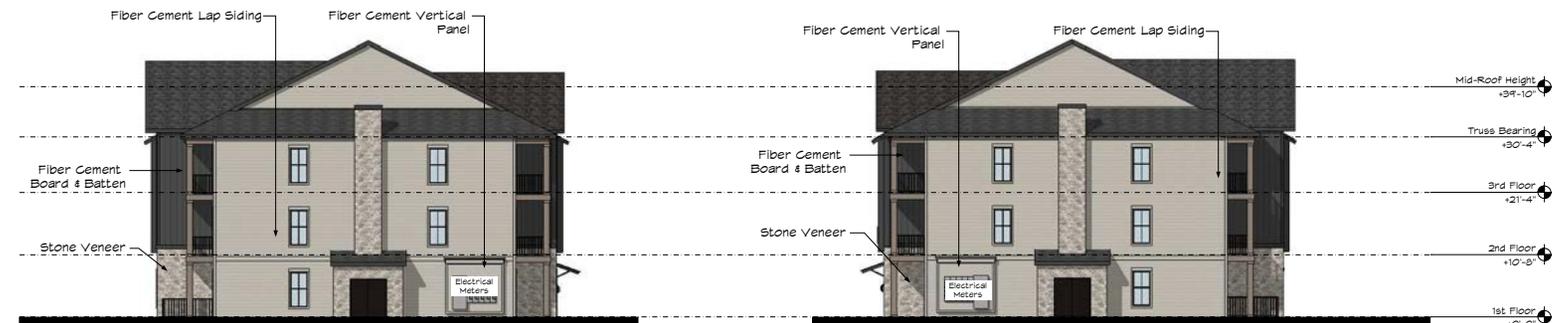




FRONT ELEVATION - BUILDING TYPE A  
Scale: 3/32" = 1'-0"



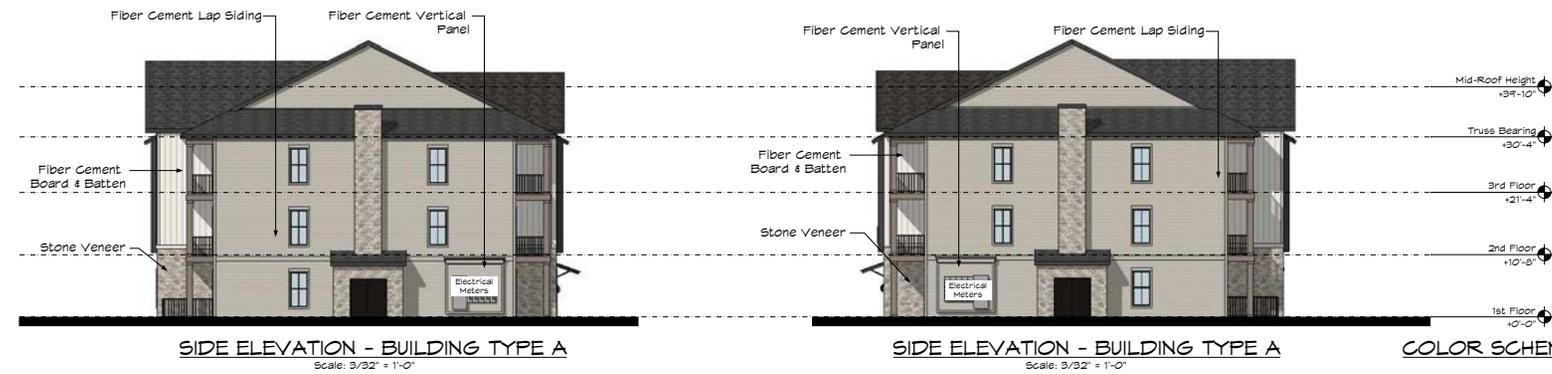
BACK ELEVATION - BUILDING TYPE A  
Scale: 3/32" = 1'-0"



SIDE ELEVATION - BUILDING TYPE A  
Scale: 3/32" = 1'-0"

SIDE ELEVATION - BUILDING TYPE A  
Scale: 3/32" = 1'-0"

COLOR SCHEME 1



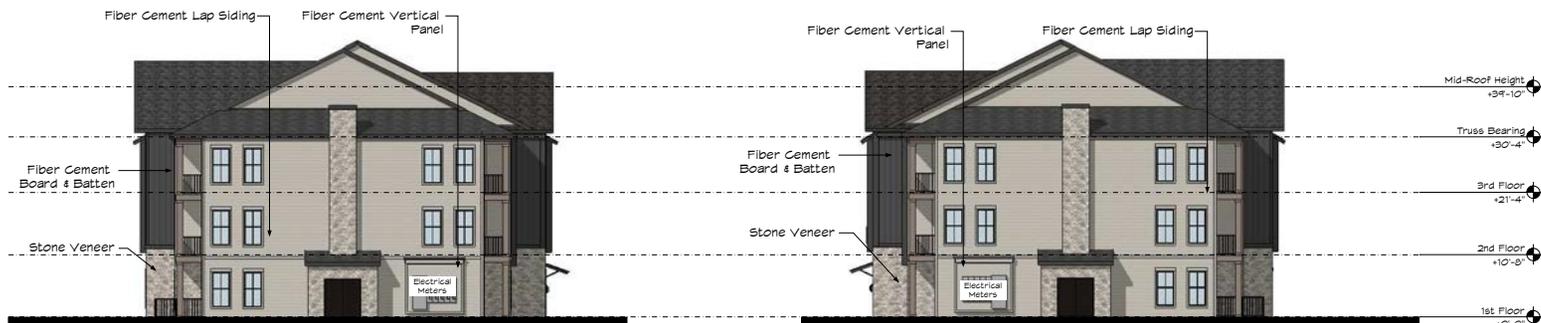
COLOR SCHEME 2



**FRONT ELEVATION - BUILDING TYPE B**  
Scale: 3/32" = 1'-0"



**BACK ELEVATION - BUILDING TYPE B**  
Scale: 3/32" = 1'-0"



**SIDE ELEVATION - BUILDING TYPE B**  
Scale: 3/32" = 1'-0"

**SIDE ELEVATION - BUILDING TYPE B**  
Scale: 3/32" = 1'-0"

**COLOR SCHEME 1**



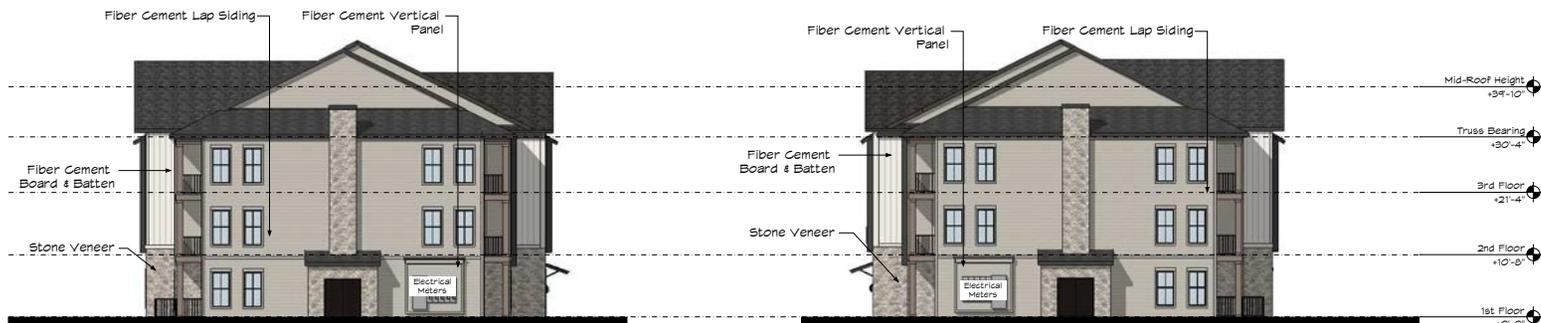
**FRONT ELEVATION - BUILDING TYPE B**

Scale: 3/32" = 1'-0"



**BACK ELEVATION - BUILDING TYPE B**

Scale: 3/32" = 1'-0"



**SIDE ELEVATION - BUILDING TYPE B**

Scale: 3/32" = 1'-0"

**SIDE ELEVATION - BUILDING TYPE B**

Scale: 3/32" = 1'-0"

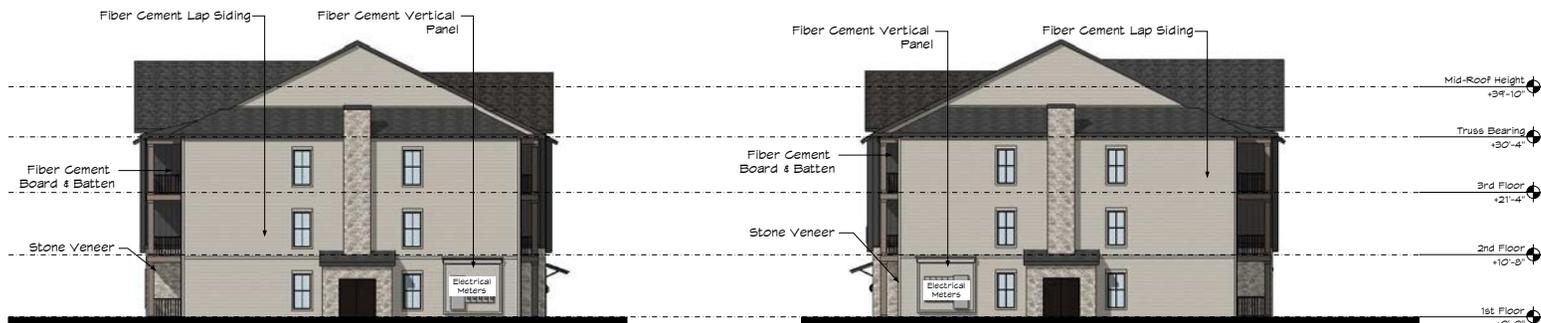
**COLOR SCHEME 2**



FRONT ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"



BACK ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"



SIDE ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"

SIDE ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"

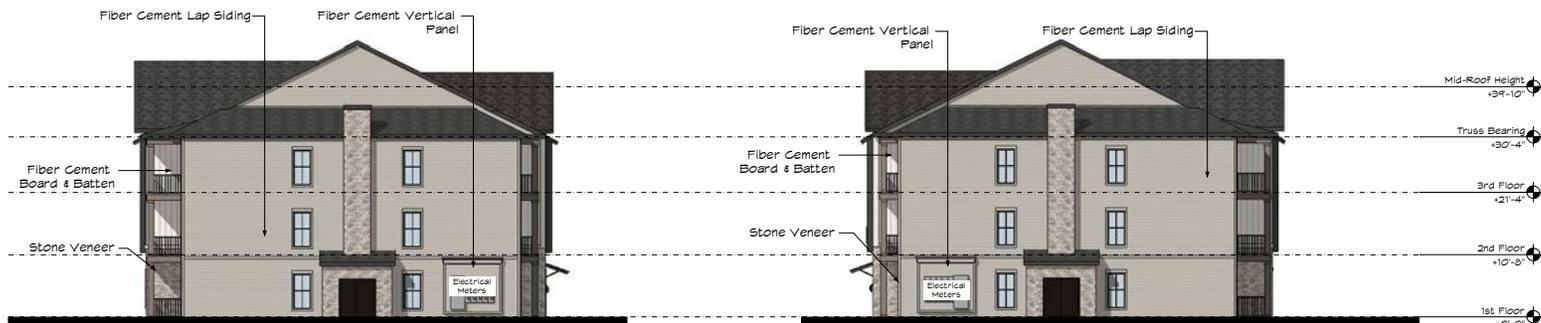
COLOR SCHEME 1



FRONT ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"



BACK ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"



SIDE ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"

SIDE ELEVATION - BUILDING TYPE C  
Scale: 3/32" = 1'-0"

COLOR SCHEME 2



TYPICAL FRONT ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL BACK ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL SIDE ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL SIDE ELEVATION  
Scale: 1/8" = 1'-0"

ELEVATIONS OF SELECT20 APARTMENTS COLOR SCHEME 1



TYPICAL FRONT ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL BACK ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL SIDE ELEVATION  
Scale: 1/8" = 1'-0"



TYPICAL SIDE ELEVATION  
Scale: 1/8" = 1'-0"

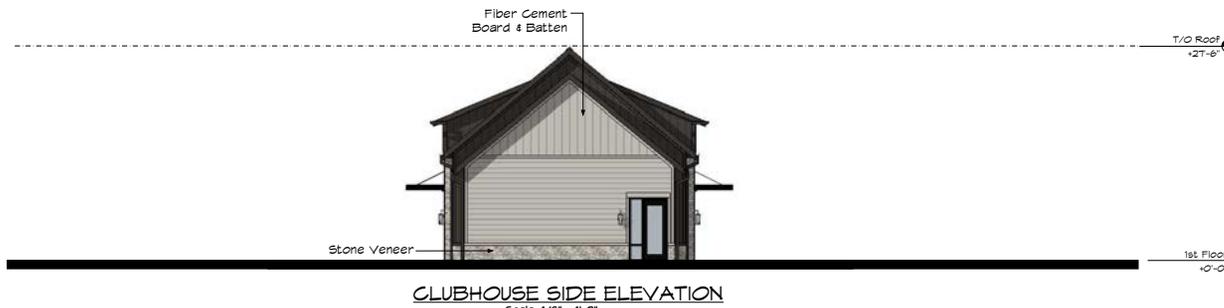
ELEVATIONS OF SELECT20 APARTMENTS COLOR SCHEME 2



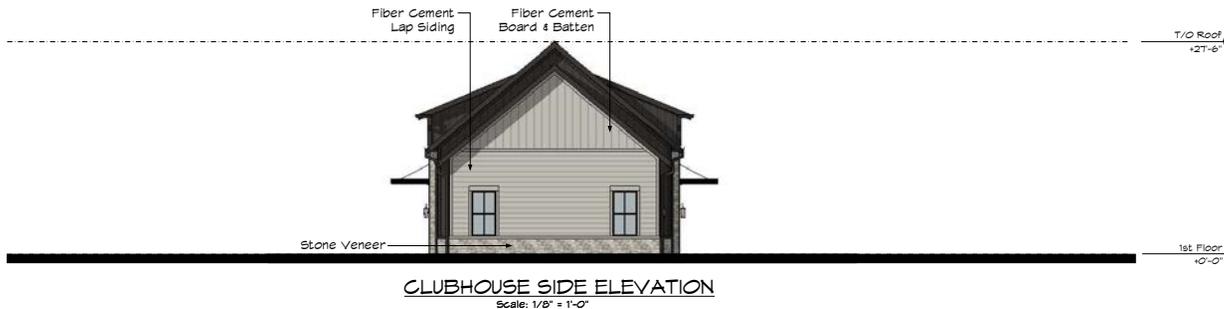
**CLUBHOUSE FRONT ELEVATION**  
Scale: 1/8" = 1'-0"



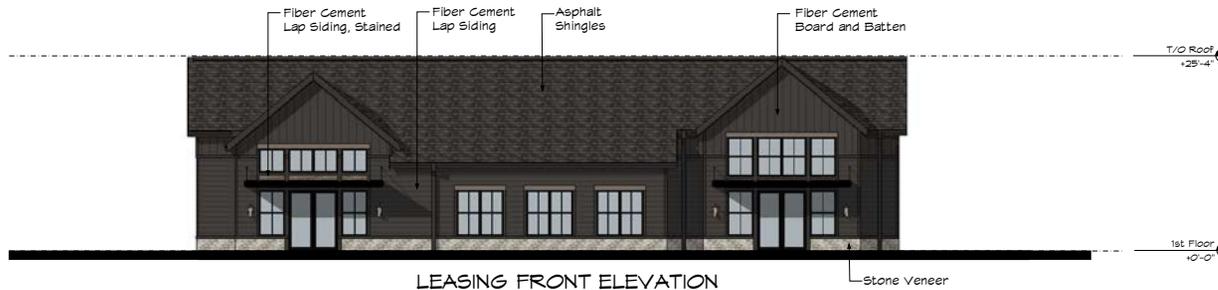
**CLUBHOUSE BACK ELEVATION**  
Scale: 1/8" = 1'-0"



**CLUBHOUSE SIDE ELEVATION**  
Scale: 1/8" = 1'-0"



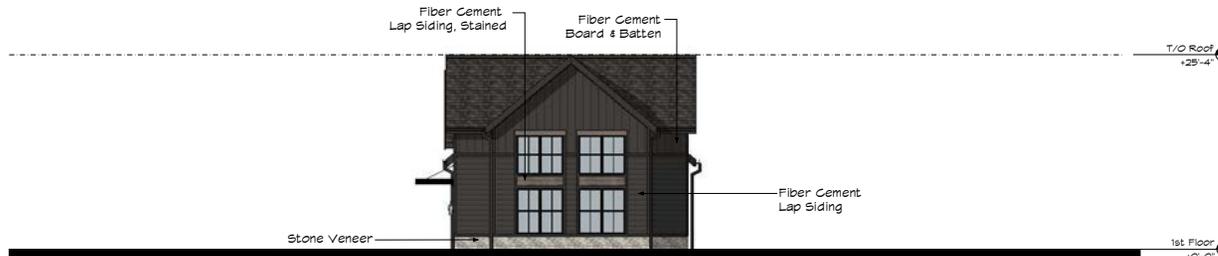
**CLUBHOUSE SIDE ELEVATION**  
Scale: 1/8" = 1'-0"



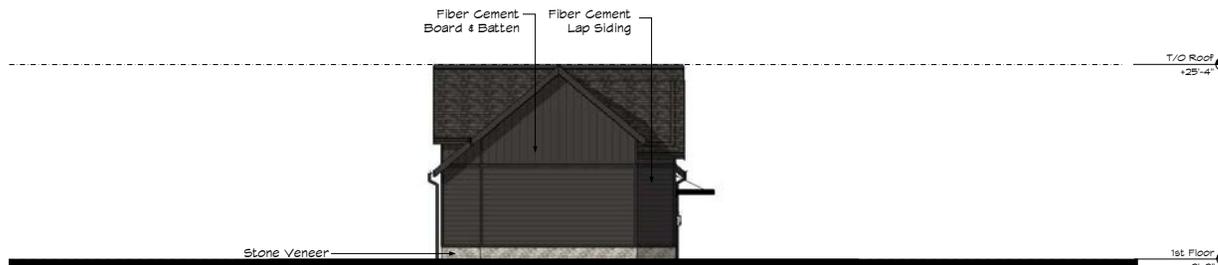
**LEASING FRONT ELEVATION**  
Scale: 1/8" = 1'-0"



**LEASING BACK ELEVATION**  
Scale: 1/8" = 1'-0"



**LEASING SIDE ELEVATION**  
Scale: 1/8" = 1'-0"



**LEASING SIDE ELEVATION**  
Scale: 1/8" = 1'-0"



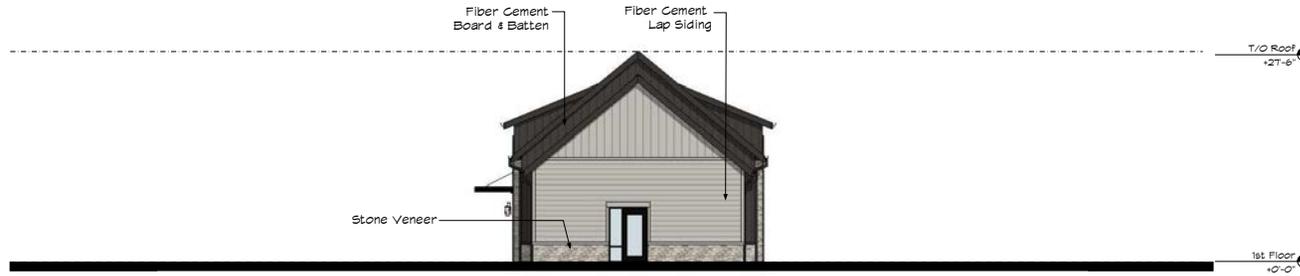
**FITNESS FRONT ELEVATION**

Scale: 1/8" = 1'-0"



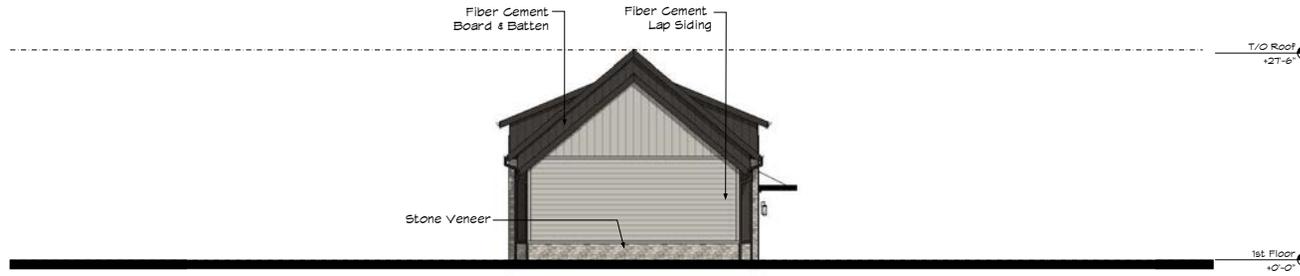
**FITNESS BACK ELEVATION**

Scale: 1/8" = 1'-0"



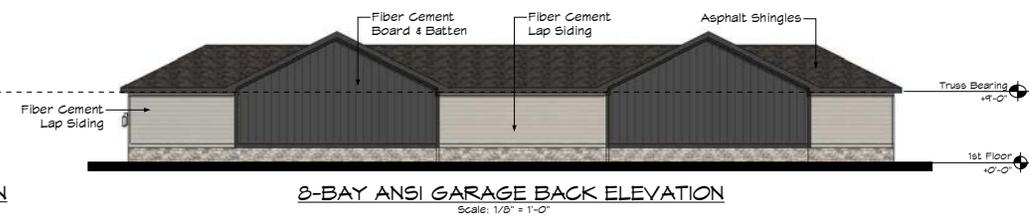
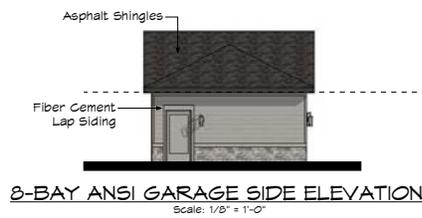
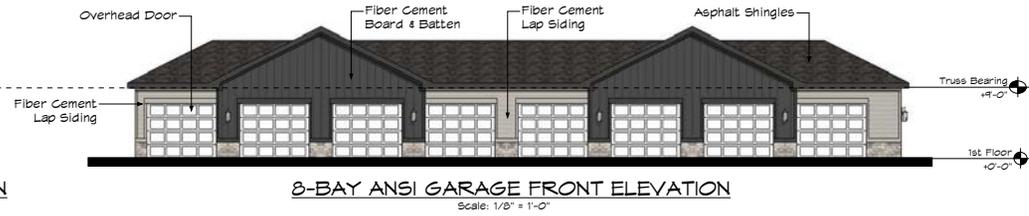
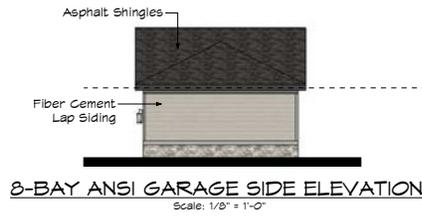
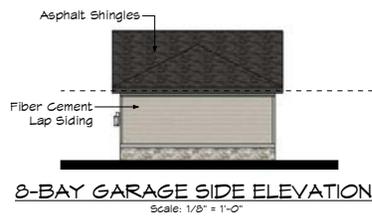
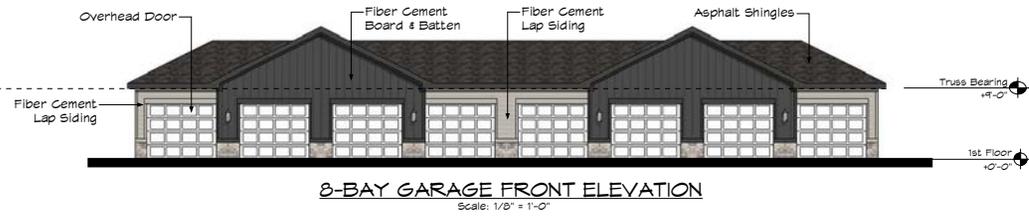
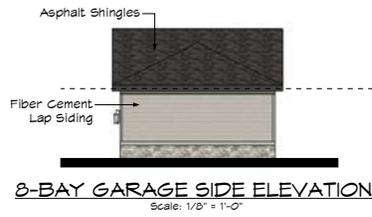
**FITNESS SIDE ELEVATION**

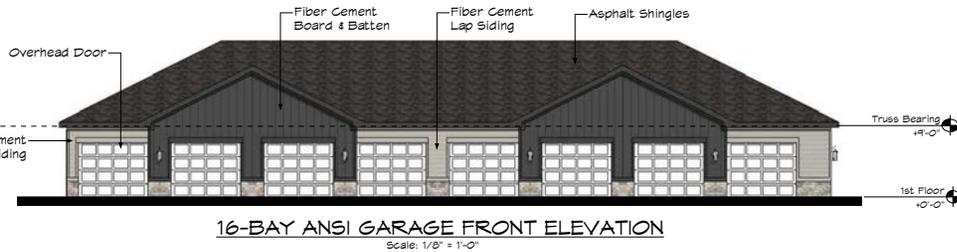
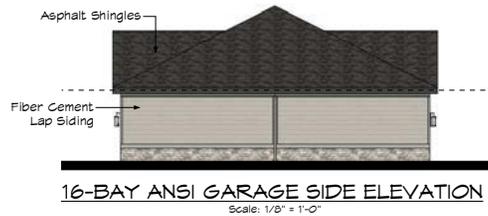
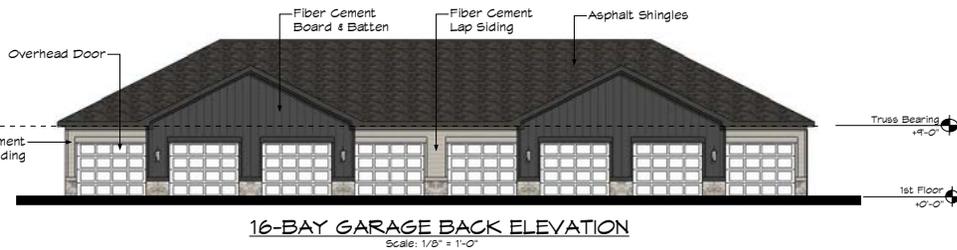
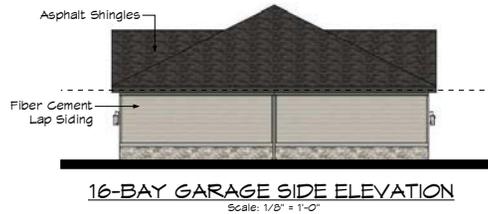
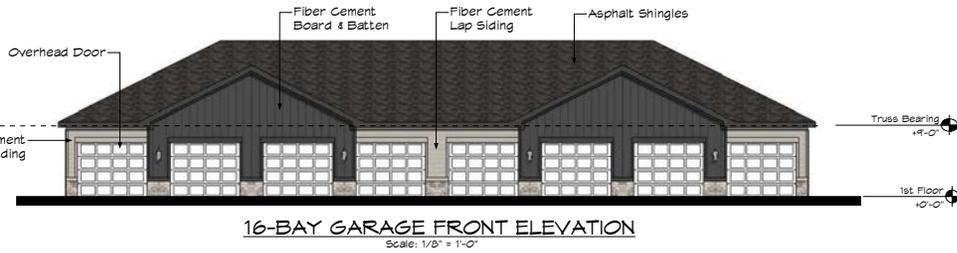
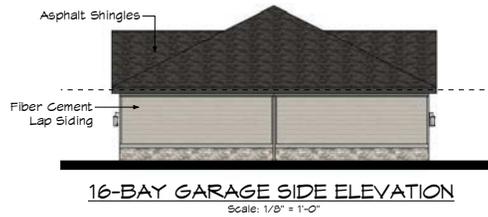
Scale: 1/8" = 1'-0"

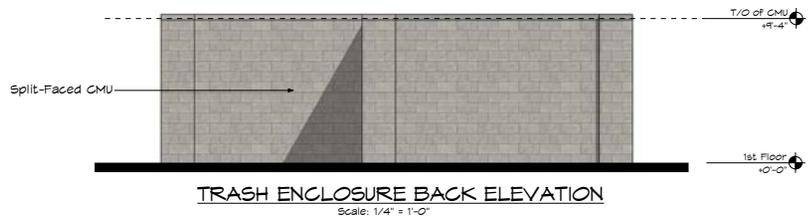
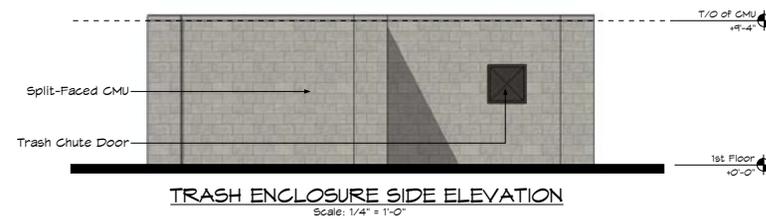
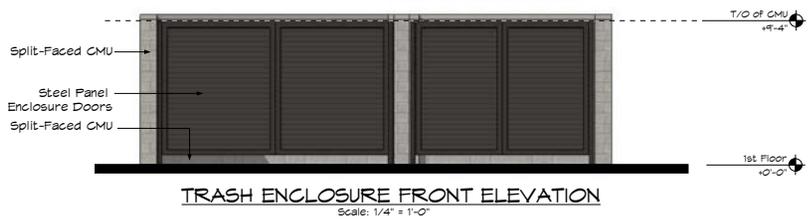
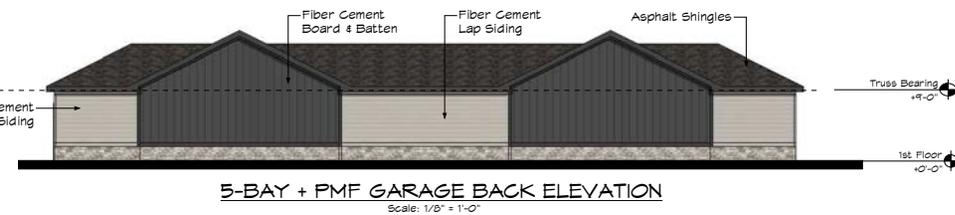
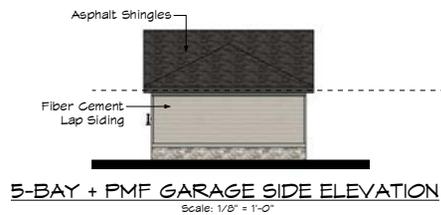
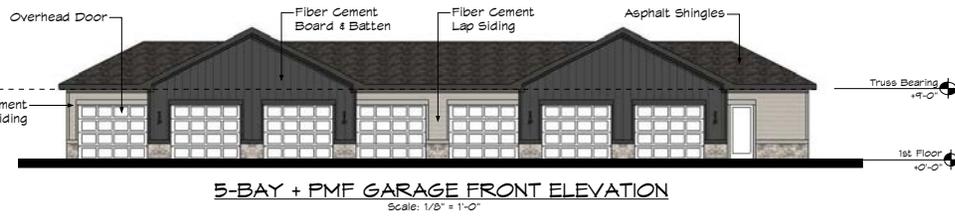


**FITNESS SIDE ELEVATION**

Scale: 1/8" = 1'-0"







# CLASSIC BUILDING STATISTICS

Building Type	# of Buildings	Unit Count	Total Units	1st Floor SF <sup>1</sup>	2nd Floor SF <sup>1</sup>	3rd Floor SF <sup>1</sup>	Total SF <sup>1</sup> Per Building	Total SF <sup>1</sup> Per Building Type	Balcony SF Per Building	Balcony SF Per Building Type	Total GSF <sup>2</sup> Per Building	Total GSF <sup>2</sup> Per Building Type
Three-Story Building A	1	36	36	11,669	11,542	11,542	34,747	34,747	2,490	2,490	37,237	37,237
Three-Story Building B	2	36	72	19,289	19,178	19,178	39,639	79,278	2,460	4,920	42,099	84,198
Three-Story Building C	2	36	72	19,495	19,874	19,874	41,743	83,486	2,254	4,508	49,997	97,994
<b>Totals</b>	<b>5</b>	<b>180</b>	<b>180</b>					<b>197,511</b>				<b>209,429</b>

1. SF taken from outside face of exterior sheathing and includes all units, common corridors, and stairs. Balconies/patios are excluded.  
 2. GSF taken from outside face of exterior sheathing and includes all units, common corridors, stairs, AND balconies/patios.

Unit Matrix	Unit NSF <sup>1</sup>	Total Units	Unit NSF <sup>1</sup> Total	Balcony SF	Balcony SF Total	Unit GSF <sup>2</sup>	Unit GSF <sup>2</sup> Total	Unit Percentage Total
Unit A1	684	15	10,395	64	960	753	11,245	
Unit A1+	701	21	14,721	64	1,344	763	16,065	
Unit A2	761	26	19,786	76	1,976	897	21,762	
Unit A2 ANSI	761	0	0	76	0	897	0	
Unit A2a	764	10	7,640	76	760	843	8,483	40%
Unit B1	1,067	10	10,670	62	620	1,129	11,290	
Unit B1+	1,074	4	4,316	62	248	1,141	4,564	
Unit B1a	1,087	8	8,646	62	496	1,149	9,192	
Unit B1b	1,075	20	21,500	62	1,240	1,197	22,740	
Unit B2	1,193	22	26,246	73	1,606	1,266	27,512	
Unit B2 ANSI	1,193	0	0	73	0	1,266	0	
Unit B2a	1,201	20	24,020	73	1,460	1,274	25,480	47%
Unit C1	1,254	4	5,036	71	284	1,330	5,320	
Unit C1a	1,254	8	10,072	71	568	1,330	10,640	
Unit C2	1,364	12	16,428	77	924	1,446	17,352	
Unit C2 ANSI	1,364	0	0	77	0	1,446	0	19%
<b>Totals</b>		<b>180</b>	<b>179,516</b>		<b>12,486</b>		<b>192,002</b>	<b>100%</b>

1. NSF taken from centerline of demising wall, corridor side of corridor stud, and outside face of exterior sheathing.  
 2. GSF taken from centerline of demising wall, corridor side of corridor stud, and outside face of exterior sheathing AND includes Balcony SF.

Unit Breakdown	A1	A1+	A2	A2a	B1	B1+	B1a	B1b	B2	B2a	C1	C1a	C2	Total per Building
Building A - 1st Floor	1	3	4		2				2					12
Building A - 2nd Floor	1	3	3	1				2		2				12
Building A - 3rd Floor	1	3	3	1				2		2				12
<b>Building A Totals</b>	<b>3</b>	<b>9</b>	<b>10</b>	<b>2</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>2</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>36</b>
Building B - 1st Floor	1	1	2		2	2			4					12
Building B - 2nd Floor	1	1	1	1			2	2	2	2				12
Building B - 3rd Floor	1	1	1	1			2	2	2	2				12
<b>Building B Totals</b>	<b>3</b>	<b>3</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>2</b>	<b>4</b>	<b>4</b>	<b>8</b>	<b>4</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>36</b>
Building C - 1st Floor	1	1	2		2				2		2	0	2	12
Building C - 2nd Floor	1	1	1	1				2		2	0	2	2	12
Building C - 3rd Floor	1	1	1	1				2		2	0	2	2	12
<b>Building C Totals</b>	<b>3</b>	<b>3</b>	<b>4</b>	<b>2</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>4</b>	<b>2</b>	<b>4</b>	<b>2</b>	<b>4</b>	<b>6</b>	<b>36</b>
<b>Totals</b>	<b>15</b>	<b>21</b>	<b>26</b>	<b>10</b>	<b>10</b>	<b>4</b>	<b>8</b>	<b>20</b>	<b>22</b>	<b>20</b>	<b>4</b>	<b>8</b>	<b>12</b>	<b>180</b>
<b>Percentages</b>	<b>40%</b>					<b>47%</b>					<b>19%</b>			

1. ANSI Type A units will need to be provided on this project. See architectural site plan and building plans for locations. ANSI Type A units are included in the Unit Matrix above.

Total Project GSF	Total GSF
Total Housing GSF	359,567
Clubhouse (See Note 1)	2,458
Clubhouse Patios (See Note 1)	-
Leasing (See Note 1)	2,582
Leasing Patios (See Note 1)	-
Fitness (See Note 1)	2,517
Fitness Patios (See Note 1)	-
6-Bay Garages (1,437 ea.)	1,437
8-Bay Garages (1,422 ea.)	3,844
16-Bay Garages (3,809 ea.)	11,427
5-Bay Garage + Maintenance Garage	2,195
<b>Total Project GSF</b>	<b>379,467</b>

1. Clubhouse, Fitness, and Leasing Building SF subject to change based on final site plan configuration

## SELECT20 BUILDING STATISTICS

	# of Buildings	Unit Count	Total Units	1st Floor NSF <sup>1</sup>	2nd Floor NSF <sup>1</sup>	Total NSF <sup>1</sup>	1st Floor GSF <sup>2</sup>	2nd Floor GSF <sup>2</sup>	Total GSF <sup>2</sup>
Typical Apartment Building	1	20	20	11,217	11,140	22,357	12,145	11,070	24,023

1. NSF taken from outside face of exterior sheathing and includes all units, common closets, and stairs. Balconies/patios are excluded.
2. GSF taken from outside face of exterior sheathing and includes all units, common closets, stairs, and unit balconies/patios.
3. Both NSF and GSF are taken with polylines around building footprint.

	# of Buildings	Unit Count	Total Units	1st Floor NSF <sup>1</sup>	2nd Floor NSF <sup>1</sup>	Total NSF <sup>1</sup>	1st Floor GSF <sup>2</sup>	2nd Floor GSF <sup>2</sup>	Total GSF <sup>2</sup>
All Apartment Buildings	6	20	120	67,302	66,040	134,142	72,070	71,260	144,130

1. NSF taken from outside face of exterior sheathing and includes all units, common closets, and stairs. Balconies/patios are excluded.
2. GSF taken from outside face of exterior sheathing and includes all units, common closets, stairs, and unit balconies/patios.
3. Both NSF and GSF are taken with polylines around building footprint.

Unit Type	Unit NSF <sup>1</sup>	Total Units	Unit NSF <sup>1</sup> Total	Balcony SF	Balcony SF Total	Unit GSF <sup>2</sup>	Unit GSF <sup>2</sup> Total	Unit Percentage	Unit Percentage Total
Unit A1	706	24	16,964	82	1,960	860	20,832	20.0%	
Unit A1 ANSI	706	0	0	82	0	860	0	0.0%	
Unit A1u	864	24	20,856	82	1,960	951	22,824	20.0%	40.0%
Unit B1	1,202	23	27,646	65	1,445	1,267	29,141	19.2%	
Unit B1 ANSI	1,202	1	1,202	65	65	1,267	1,267	0.8%	
Unit B1u	1,205	24	30,840	65	1,560	1,350	32,400	20.0%	
Unit B2	1,164	6	7,014	70	420	1,234	7,434	5.0%	
Unit B2u	1,225	6	7,350	70	420	1,235	7,770	5.0%	50.0%
Unit C1	1,455	6	8,730	66	346	1,521	9,126	5.0%	
Unit C1 ANSI	1,455	0	0	66	0	1,521	0	0.0%	
Unit C1u	1,515	6	9,078	66	346	1,574	9,474	5.0%	10.0%
Totals		120	191,500		8,600		140,260	100%	100%

1. NSF taken from centerline of demising wall, corridor side of corridor stud, and outside face of exterior sheathing.
2. GSF taken from centerline of demising wall, corridor side of corridor stud, and outside face of exterior sheathing AND includes Balcony SF.
3. Upper floor unit square footages include area of stairs under bedroom closets.

Building Type Breakdown	Unit Type A1 <sup>1</sup>	Unit Type A1u	Unit Type B1 <sup>1</sup>	Unit Type B1u	Unit Type B2	Unit Type B2u	Unit Type C1 <sup>1</sup>	Unit Type C1u	Total per Building
Typical Building - 1st Floor	4	0	4	0	1	0	1	0	10
Typical Building - 2nd Floor	0	4	0	4	0	1	0	1	10
Typical Building Totals	4	4	4	4	1	1	1	1	20

Project Totals	24	24	24	24	6	6	6	6	120
Percentages	40.0%		50.0%				10.0%		

1. Some A1, B1, and C1 units are to be ANSI Type A units. See architectural site plan and building plans for locations. ANSI Type A units are included in the Unit Matrix above.
2. Project includes 1 total ANSI Type A unit.

Totals	1 BR	2 BR	3 BR	Total Units	Total GSF <sup>1</sup>
Apartments	40	60	12	120	144,130
Clubhouse / Leasing					
Fitness					
0-Bay Garage (1,122 sf each)					
0-Bay ANSI Garage (2,020 sf each)					
Property Maintenance Facility					
Totals	40	60	12	120	144,130

1. GSF taken from outside face of exterior sheathing, includes all patios/balconies and covered porches.

# Exhibit B. Selected pages from Traffic Impact Analysis

Revised December 29, 2025

## TRAFFIC IMPACT ANALYSIS FOR THE PROPOSED SCHAAD ROAD APARTMENTS

8007 Ball Camp Pike

Knoxville, Knox County, Tennessee

Prepared for:

Thompson Thrift Development, Inc.  
Mr. Tyler Royalty  
111 Monument Circle  
Suite 1600  
Indianapolis IN  
3178535430  
troyalty@thompsonthrift.com

Prepared by:

Civil & Environmental Consultants, Inc.  
2704 Cherokee Farm Way  
Knoxville, TN 37920  
(865) 977-9997



Civil & Environmental Consultants, Inc.

# Exhibit B. Selected pages from Traffic Impact Analysis

## SIGHT DISTANCE CALCULATION

Measurements were performed to verify the available sight distance at the proposed site driveway intersection with Schaad Road. The measurements were performed in accordance with the Knoxville-Knox County Minimum Subdivision Regulations. According to the subdivision regulations, intersection sight distance is calculated as 10 times the posted speed. Therefore, for Schaad Road, the intersection sight distance was calculated to be 450 feet.

A summary of the available and the required intersection sight distances at the proposed driveway intersection locations are presented in Table 4. As shown in the table, the available sight distance exceeds the required intersection sight distance at the proposed driveway intersection location.

## CONCLUSIONS/RECOMMENDATIONS

This study has concluded that the construction of the proposed Schaad Road Apartments will have no significant impact on the operation of the adjacent study intersections.

Therefore, CEC recommends the following:

- Construct the proposed site driveway to Schaad Road to provide one (1) lane for ingress traffic and two (2) lanes for egress traffic. The driveway should be controlled by a Stop sign on the westbound site driveway approach to the Schaad Road.
- Construct a 295' northbound right turn lane along Schaad Road at the proposed site driveway. The northbound right turn lane shall be constructed with 145' of taper and 150' of storage.
- The proposed site driveway should be constructed to provide sight triangles that are free of sight obstructions, such as parked vehicles, buildings, walls, hedges, bushes, low growing trees, etc. to maintain a clear line of sight to potential conflicting vehicles.

This report includes a Technical Appendix containing all counts, analyses and calculations.

# Public Notice and Community Engagement

Planning strives to provide community members with information about upcoming cases in a variety of ways. In addition to posting public notice signs, our agency encourages applicants to provide information and offer opportunities for dialogue related to their upcoming case(s). We require applicants to acknowledge their role in this process.

## Sign Posting and Removal

The Administrative Rules and Procedures of the Knoxville-Knox County Planning Commission require a sign to be posted on the property for each application subject to consideration by the Planning Commission.

**Planning staff will post the required sign.** If a replacement sign(s) is needed, the applicant is responsible for picking up the new sign(s) from Planning and will be charged \$10 for each replacement.

### Location and Visibility

The sign must be posted on the nearest adjacent/frontage street and in a location clearly visible to vehicles traveling in either direction. If the property has more than one street frontage, the sign should be placed along the street that carries more traffic. Planning staff may recommend a preferred location for the sign to be posted at the time of application.

### Timing

The sign(s) must be posted not less than 12 days prior to the scheduled Planning Commission public hearing and must remain in place until the day after the meeting. In the case of a postponement, the sign can either remain in place or be removed and reposted not less than 12 days prior to the next Planning Commission meeting. The applicant is responsible for removing the sign after the application has been acted upon by the Planning Commission.

### Acknowledgement

*By signing below, you acknowledge that public notice signs must be posted and visible on the property consistent with the guidelines above and between the dates listed below.*

01/31/2026

02/13/2026

Date to be Posted

Date to be Removed

**Have you engaged the surrounding property owners to discuss your request?**

Yes  No

No, but I plan to prior to the Planning Commission meeting

Signed by:  
*Brian Southworth*  
46277895200F440...

Brian Southworth

12/22/2025

Applicant Signature

Applicant Name

Date