



# SPECIAL USE REPORT

▶ **FILE #:** 3-A-26-SU

**AGENDA ITEM #:** 11

**AGENDA DATE:** 3/5/2026

▶ **APPLICANT:** STEVE YOUNG  
**OWNER(S):** Shreehari Enterprize LLC

**TAX ID NUMBER:** 122 E E 004, 005, 006 [View map on KGIS](#)

**JURISDICTION:** City Council District 1

**STREET ADDRESS:** 1102 MARYVILLE PIKE (0 MARYVILLE PIKE; 4409 SIMS RD)

▶ **LOCATION:** Southeast side of Maryville Pike, southwest side of Sims Rd

▶ **APPX. SIZE OF TRACT:** 0.8 acres

**SECTOR PLAN:** South City

**GROWTH POLICY PLAN:** N/A (Within City Limits)

**ACCESSIBILITY:** Access is via Maryville Pike, a state-owned minor arterial with 20-23 ft of pavement width within a right-of-way width that varies from 55-62 ft. Access is also via Sims Road, an unstriped minor collector with 19-ft of pavement width within a right-of-way width that varies from 40-46 ft.

**UTILITIES:** Water Source: Knoxville Utilities Board

Sewer Source: Knoxville Utilities Board

**FIRE DISTRICT:** Knoxville Fire Department

**WATERSHED:** Goose Creek

▶ **ZONING:** C-G-1 (General Commercial)

▶ **EXISTING LAND USE:** Agriculture/Forestry/Vacant Land, Commercial

▶ **PROPOSED USE:** Gas station / Convenience Store with drive-through window

**HISTORY OF ZONING:** In 2007 the property was rezoned from I-3 (General Industrial) to C-3 (General Commercial) (7-C-07-RZ). In 2025 part of the property was rezoned from I-G (General Industrial) to C-G-1 (General Commercial) (10-K-25-RZ).

**SURROUNDING LAND USE AND ZONING:** North: Commercial - C-N (Neighborhood Commercial), I-G (General Industrial)

South: Office, commercial - I-G (General Industrial)

East: Office - I-G (General Industrial)

West: Industrial - I-G (General Industrial)

**NEIGHBORHOOD CONTEXT:** The surrounding area features a mix of industrial, office, commercial, and wholesale uses interspersed with vacant land and single family houses. The Vestal Neighborhood is nearby to the south across the railroad. Mary Vestal Park and Community Unity Park are within a mile of the subject property.

**STAFF RECOMMENDATION:**

► **Approve the request for a gas station and convenience store with a drive-through window as depicted on the site plan, subject to 6 conditions.**

- 1) Implementing the recommendations of the Traffic Impact Study for Maryville Pike Gas Station by Ajax Engineering, as required by the City of Knoxville Engineering Department and Tennessee Department of Transportation.
- 2) Coordinating with Knoxville Area Transit to provide a dedicated pavement area and seating amenities for the existing bus stop. The final configuration of the bus stop area must be approved by the City of Knoxville Engineering Department. The cost of these improvements is the responsibility of the property owner.
- 3) Installing all sidewalks and crosswalks as identified on the site plan. Sidewalks and ramps shall meet all applicable requirements of the City of Knoxville Engineering Department and Americans with Disabilities Act (ADA). A bond shall be provided to the City of Knoxville Department of Engineering by the developer in an amount sufficient to guarantee the installation.
- 4) Meeting all applicable requirements of the City of Knoxville Zoning Ordinance.
- 5) Meeting all applicable requirements of the City of Knoxville Engineering Department.
- 6) Meeting all applicable requirements of the City of Knoxville Plans Review and Inspections Department.

**COMMENTS:**

The applicant proposes a gas station and convenience store with a drive-through window on this 0.8-acre property, featuring three gas pumps and a 4,524-sq ft building. The existing one-story commercial structure will be demolished and the existing pavement at the front of the site will be removed to make a more defined vehicular entry. Additionally, the development proposes providing a dedicated pavement area and seating amenities for the existing bus stop and adding landscaping along Sims Road and Maryville Pike.

**STANDARDS FOR EVALUATING A SPECIAL USE (ARTICLE 16.2.F.2)**

**1) THE USE IS CONSISTENT WITH ADOPTED PLANS AND POLICIES, INCLUDING THE GENERAL PLAN AND THE ONE-YEAR PLAN.**

- A. There is a blue line stream running along the southwestern boundary of the subject parcel, and the proposed development provides a 30-ft stream buffer. This is consistent with the General Plan's Development Policy 6.3, which encourages development in areas with the fewest environmental constraints.
- B. The subject property's location at the intersection of Maryville Pike (minor arterial) and Sims Road (minor collector) is consistent with the General Plan's Development Policy 8.1, which encourages locating community-serving commercial areas where they can be easily shared by several neighborhoods.
- C. The proposed use is consistent with the One Year Plan's and the South City Sector Plan's GC (General Commercial) land use classification.

**2) THE USE IS IN HARMONY WITH THE GENERAL PURPOSE AND INTENT OF THIS ZONING CODE.**

- A. The C-G-1 district is intended to provide for a heterogeneous mix of retail, personal service, office, and residential uses within and along commercial nodes and corridors in a pedestrian-oriented environment. Retail uses are permitted in the C-G-1 district; however, gas stations and drive-through facilities require Special Use approval by the Planning Commission.
- B. The proposed development conforms to the dimensional standards (Article 5.3), design standards (Article 5.4), and applicable Principal Use Standards (Article 9.3.F and 9.3.O). For example, the proposed structure meets the 30% transparency requirement for the street-facing front elevation and provides the required landscaping along the periphery of the property. These requirements shall be further verified during the permitting process.

**3) THE USE IS COMPATIBLE WITH THE CHARACTER OF THE NEIGHBORHOOD WHERE IT IS PROPOSED, AND WITH THE SIZE AND LOCATION OF BUILDINGS IN THE VICINITY.**

- A. This node comprises a mix of commercial, office, industrial, and warehouse uses. The proposed use is compatible with other nonresidential uses, and the scale of the one-story structure is consistent with other structures at this intersection.

**4) THE USE WILL NOT SIGNIFICANTLY INJURE THE VALUE OF ADJACENT PROPERTY OR BY NOISE, LIGHTS, FUMES, ODORS, VIBRATION, TRAFFIC, CONGESTION, OR OTHER IMPACTS DETRACT FROM THE IMMEDIATE ENVIRONMENT.**

- A. The proposed development is not expected to significantly injure the value of any adjacent properties. On the contrary, the development will provide road improvements, including new sidewalks along Maryville Pike, and enhance the existing KAT bus stop by reconfiguring the bus bay and adding seating amenities.
- B. The site plan notes that underground storage tanks will be located along the property's northern corner,

farthest from the blue line stream running adjacent to the property.

5) THE USE IS NOT OF A NATURE OR SO LOCATED AS TO DRAW SUBSTANTIAL ADDITIONAL TRAFFIC THROUGH RESIDENTIAL STREETS.

A. Traffic will be along Maryville Pike and Sims Road / Edington Road, which are classified streets. As stipulated in condition 1, the development shall implement the recommendations of the Traffic Impact Study conducted for this project (Exhibit B).

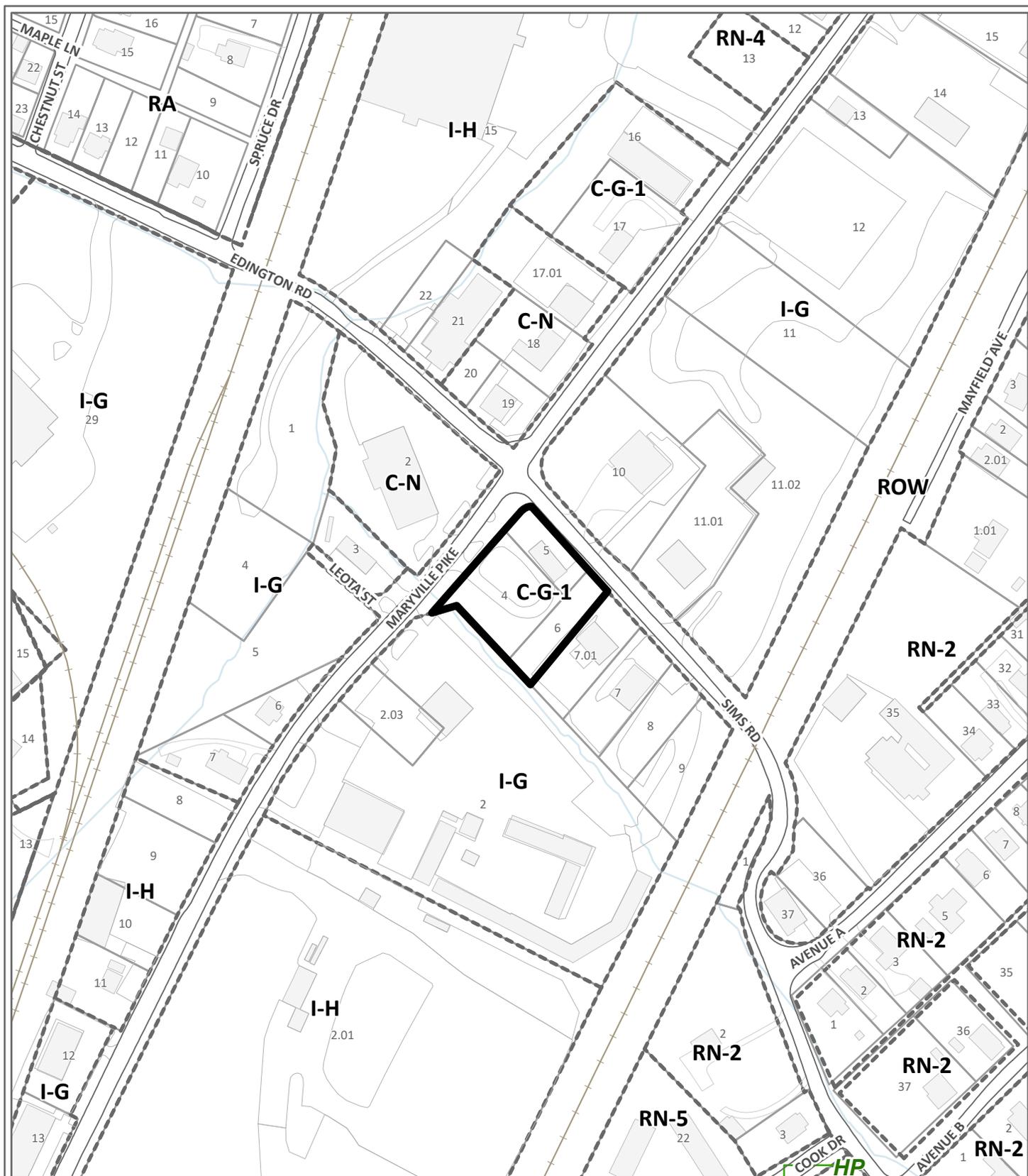
6) THE NATURE OF DEVELOPMENT IN THE SURROUNDING AREA IS NOT SUCH AS TO POSE A POTENTIAL HAZARD TO THE PROPOSED USE OR TO CREATE AN UNDESIRABLE ENVIRONMENT FOR THE PROPOSED USE.

A. There are no known uses immediately surrounding the subject property that would pose a potential hazard or undesirable environment for the proposed use.

ESTIMATED TRAFFIC IMPACT: A traffic impact study was prepared by the applicant. The findings of that study were used in formulating the recommendations of this staff report.

ESTIMATED STUDENT YIELD: Not applicable.

The Planning Commission's approval or denial of this request is final, unless the action is appealed to the Knoxville City Council. The date of the Knoxville City Council hearing will depend on when the appeal application is filed.



**SPECIAL USE**

**3-A-26-SU**

Petitioner: Steve Young



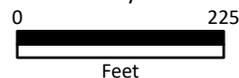
Gast station / Convenience Store with drive through window in C-G-1 (General Commercial)

Original Print Date: 1/26/2026

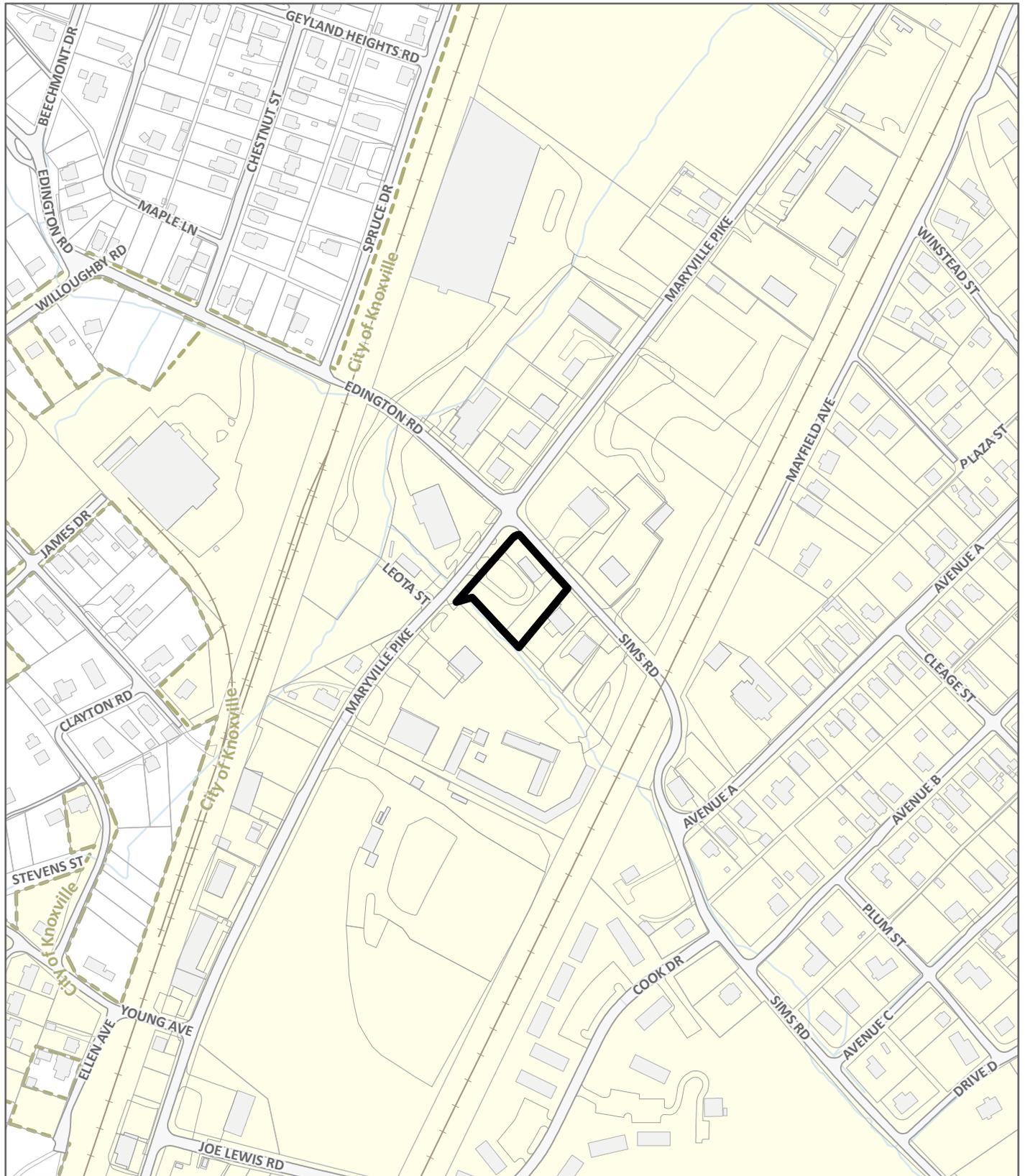
Knoxville - Knox County Planning Commission \* City / County Building \* Knoxville, TN 37902

Map No: 122

Jurisdiction: City



# Exhibit A. Contextual Images

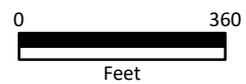


**LOCATION MAP**

**3-A-26-SU**



Case boundary



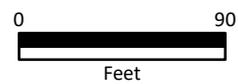
# Exhibit A. Contextual Images



## AERIAL MAP



Case boundary



# MARYVILLE PK., GAS STATION

1102 Maryville Pike, Knoxville, Tn., 37920



1/14/2026

MARYVILLE PK., GAS STATION  
 FOR SAM PATEL  
 1102 Maryville Pike, Knoxville, Tn., 37920

DRAWN BY: KG + AB

PROJECT INFORMATION

G000

PROJECT : 25107

© COPYRIGHT 2025

SYMBOL LEGEND	VICINITY MAP	OWNER	STRUCTURAL ENGINEER	PROJECT INFORMATION	SHEET INDEX																																																																																																																																																																																																																											
<p><b>DETAIL CALLOUT</b></p> <p>DRAIN NO NUMBER SHEET NUMBER</p> <p><b>ELEVATION MARKER</b></p> <p>DIRECTION OF VIEW</p> <p><b>DETAIL SECTION MARKER</b></p> <p>EXTENT/DIRECTION OF SECTION</p> <p><b>BUILDING SECTION MARKER</b></p> <p>EXTENT/DIRECTION OF SECTION</p> <p><b>INTERIOR ELEVATION MARKER</b></p> <p>DIRECTION OF VIEW</p> <p>SHEET NUMBER ELEVATION NUMBER</p> <p><b>NORTH INDICATOR</b></p> <p><b>FLOOR PLAN TAGS</b></p> <p>DENTIF ER WINDOW TYPE IF SCHEDULED</p> <p>DENTIF ER DOOR TYPE F SCHEDULED</p> <p>DENTIF ER, SIZE IN INCHES WINDOW TYPE ON FLOOR PLAN</p> <p>DENTIF ER DOOR TYPE DOOR SIZE, IN INCHES, ON FLOOR PLAN</p> <p>DENTIF ER PARTITION TYPE</p> <p><b>ELEVATION MARKER</b></p> <p>A.F.F.</p> <p><b>SPOT ELEVATION</b></p> <p>F.F.E. = FINISH FLOOR ELEVATION</p> <p><b>REVISIONS</b></p> <p>DENTIF ER REVISION NUMBER</p> <p>REVISION SCOPE</p> <p><b>GENERAL NOTES</b></p> <p>G1 READ (OR CALCULATE) DIMENSIONS FROM PUBLISHED PRINTS OF CONSTRUCTION DRAWINGS. DO NOT "SCALE" FROM DRAWINGS. NEITHER THE R. PUBLISHER OR ELECTRONIC PROVIDER.</p> <p>G2 CONTRACTOR SHALL VERIFY EXISTING CONDITIONS &amp; DIMENSIONS BEFORE BEGINNING CONSTRUCTION. ANY DISCREPANCIES MUST BE REPORTED TO OYSK3 ARCHITECTS FOR JUSTIFICATION AND/OR CORRECTION. CONTRACTOR ASSUMES RESPONSIBILITY FOR CONDITIONS THAT ARE NOT REPORTED.</p> <p>G3 CONTRACTOR SHALL FIELD-VERIFY COMPATIBILITY OF THE BUILDING WITH ALL SITE CONDITIONS, I.E., GRADES, ELEVATIONS, UTILITY LOCATIONS &amp; INVERTS, OTHER EXISTING CONDITIONS, ETC.</p> <p>G4 ALL FLOOR FINISHES MUST REST ON UNDISTURBED OR SUITABLE, COMPACTED SUBSOIL.</p> <p>G5 MANUFACTURED TRUSSES, BEAMS, &amp; OTHER ENGINEERED BUILDING SYSTEMS MUST BE DESIGNED BY THE MANUFACTURER'S ENGINEER WHO SHALL BE REGISTERED IN THE STATE OF TENNESSEE. STAMPED, APPROVED SHOP DRAWINGS SHALL BE ON-SITE BEFORE ERECTION BEGINS.</p>	<p><b>ABBREVIATIONS</b></p> <table border="0"> <tr> <td>AC AIR CONDITIONING</td> <td>MN MANHOLE</td> </tr> <tr> <td>ABV ABOVE</td> <td>MSC MISCELLANEOUS</td> </tr> <tr> <td>ACT ACOUSTICAL CEILING TILE</td> <td>MSP MASONRY OPENING</td> </tr> <tr> <td>ADJ ADJUSTABLE</td> <td>MR MOISTURE RESISTANT</td> </tr> <tr> <td>AFB ABOVE FINISH FLOOR</td> <td>MTL METAL</td> </tr> <tr> <td>ALT ALTERNATE</td> <td>NIC NOT IN CONTRACT</td> </tr> <tr> <td>ALUM ALUMINUM</td> <td>NO NUMBER</td> </tr> <tr> <td>BD BOARD</td> <td>NTS NOT TO SCALE</td> </tr> <tr> <td>BLDG BUILDING</td> <td>OC ON CENTER</td> </tr> <tr> <td>BLKG BLOCKING</td> <td>OH OVERHEAD</td> </tr> <tr> <td>BM BENCHMARK BEAM</td> <td>OO OUT TO OUT</td> </tr> <tr> <td>BOC BOTTOM OF CURB</td> <td>OPNG OPENING</td> </tr> <tr> <td>BOF BOTTOM OF FOOTING</td> <td>OPSE OPENING IN PSE. 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HOLLOW CORE	UNO UNLESS NOTED OTHERWISE	HDR HARDWARE	URN URINAL	HM HOLLOW METAL	VB VAPOR BARRER	HORIZ HORIZONTAL	VERT VERTICAL	HR HOUR	W WIDE WIDTH	HOT HEIGHT	WB WEATHER BARBER	ID INSIDE DIAMETER	W WITH	INSL INSULATION (NON)	WC WATER CLOSET	INT INTERIOR	WO WOOD	JOINT JOINT	WOW WINDOW	KITCHEN KITCHEN	WH WATER HEATER	LONG LENGTH	WR WATER RESISTANT	LAM LAMINATED	WWM WELDED WIRE MESH	L LINE LOAD	ANGLE	LVR LOUVER	AT	MAS MASONRY	C CENTERLINE	MATL MATERIAL	D DIAMETER	MAX MAXIMUM	P PINNY	MECH MECHANICAL	PLATE	MFR MANUFACTURER		<p>Sam Patel 103 Brickmill Rd. Maryville, TN 37801 Contact: Steve Young Phone: (865) 523-8200 Email: greengrocerymk@gmail.com</p> <p>Architect Oysk3 Architects 1545 Western Avenue, Suite 100 Knoxville, TN 37921 Contact: Steve Young Phone: (865) 523-8200 Email: office@oysk3architects.com</p> <p>Contractor Company Name - TBD Company Street Address City, State, Zip code Contact: Name Phone: (615) 341-4444 Email: email@server.com</p> <p>Structural Engineer Smith Engineering Solutions, PLLC Contact: Ryan Smith, P.E. Phone: (865) 742-0788 Email: ryan@smithengineering.com</p> <p>Electrical Engineer Hampton Engineers, LLC Company Street Address Blaine, TN 37709 Contact: Harvey Hampton, PE Phone: (865) 360-3624 Email: hwh@kvoatgo.com</p> <p>Civil Engineer Abbe Jones Consulting 1022 Fontaine Drive Lexington, Kentucky 40502 Contact: Abbe Jones Phone: (859) 559-3443 Email: www.abbe-jones.com</p> <p>Mechanical/Plumbing Engineer Knauss Engineering and Design, LLC 703 Nancy Ferry Rd. Blaine, TN 37721 Contact: Greg Knauss, P.E. Phone: (865) 740-3361 Email: gknauss@comcast.net</p>	<p><b>SCOPE OF WORK</b></p> <p>SINGLE STORY GAS STATION, WOOD FRAME ON SLAB ON GRADE FOUNDATION.</p> <p><b>PARCEL ID</b> PARCEL D: 122EE004, 122EE005, 122EE006 (TO BE ONE LOTTED)</p> <p><b>PROPERTY ZONE</b> GC-1</p> <p><b>PROPERTY SIZE</b> .8 AC</p> <p><b>BUILDING SQUARE FOOTAGE</b> 4,524 SF</p> <p><b>FLOOR LEVELS</b> SINGLE STORY</p> <p>ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL LOCAL CODES, ORDINANCES, AMENDMENTS, &amp; COVENANTS.</p> <p><b>CITY OF KNOXVILLE</b></p> <ul style="list-style-type: none"> <li>2024 INTERNATIONAL BUILDING CODE</li> <li>2024 INTERNATIONAL RESIDENTIAL CODE</li> <li>2024 INTERNATIONAL EXISTING BUILDING CODE</li> <li>2024 INTERNATIONAL FIRE CODE WITH LOCAL AMENDMENTS</li> <li>2018 INTERNATIONAL ENERGY CONSERVATION CODE</li> <li>2024 INTERNATIONAL PLUMBING CODE</li> <li>2024 INTERNATIONAL MECHANICAL CODE</li> <li>2024 INTERNATIONAL FUEL GAS CODE</li> <li>2024 INTERNATIONAL SWIMMING POOL AND SPA CODE WITH LOCAL AMENDMENTS</li> <li>2024 INTERNATIONAL PROPERTY MAINTENANCE CODE</li> <li>2023 NATIONAL ELECTRIC CODE WITH LOCAL AMENDMENTS</li> <li>2024 INTERNATIONAL GREEN CONSTRUCTION CODE</li> <li>2017 ICC ANSIA111.1 ACCESSIBILITY CODE</li> </ul>	<table border="1"> <thead> <tr> <th>SHEET NUMBER</th> <th>SHEET NAME</th> <th>SHEET ISSUE DATE</th> </tr> </thead> <tbody> <tr> <td>01- GENERAL</td> <td>PROJECT INFORMATION</td> <td>1/14/2026</td> </tr> <tr> <td>05- ARCHITECTURAL</td> <td></td> <td></td> </tr> <tr> <td>A100a</td> <td>SURVEY</td> <td>1/14/2026</td> </tr> <tr> <td>A100b</td> <td>SURVEY</td> <td>1/14/2026</td> </tr> <tr> <td>A101</td> <td>ARCHITECTURAL SITE PLAN</td> <td>1/14/2026</td> </tr> <tr> <td>A102</td> <td>SITE DETAILS</td> <td>1/14/2026</td> </tr> <tr> <td>A103</td> <td>GAS PUMP ISLAND &amp; CANOPY PLAN</td> <td>1/14/2026</td> </tr> <tr> <td>A104</td> <td>LANDSCAPE PLAN</td> <td>1/14/2026</td> </tr> <tr> <td>A105</td> <td>PLANTING DETAILS</td> <td>1/14/2026</td> </tr> <tr> <td>A106</td> <td>FLOOR PLAN</td> <td>1/14/2026</td> </tr> <tr> <td>A201</td> <td>EXTERIOR ELEVATIONS</td> <td>1/14/2026</td> </tr> <tr> <td>A202</td> <td>EXTERIOR ELEVATIONS</td> <td>1/14/2026</td> </tr> <tr> <td>A301</td> <td>BUILDING SECTION</td> <td>1/14/2026</td> </tr> <tr> <td>11- ELECTRICAL</td> <td></td> <td></td> </tr> <tr> <td>A31</td> <td>LIGHTING INTENSITY LAYOUT</td> <td>10/16/2025</td> </tr> </tbody> </table>	SHEET NUMBER	SHEET NAME	SHEET ISSUE DATE	01- GENERAL	PROJECT INFORMATION	1/14/2026	05- ARCHITECTURAL			A100a	SURVEY	1/14/2026	A100b	SURVEY	1/14/2026	A101	ARCHITECTURAL SITE PLAN	1/14/2026	A102	SITE DETAILS	1/14/2026	A103	GAS PUMP ISLAND & CANOPY PLAN	1/14/2026	A104	LANDSCAPE PLAN	1/14/2026	A105	PLANTING DETAILS	1/14/2026	A106	FLOOR PLAN	1/14/2026	A201	EXTERIOR ELEVATIONS	1/14/2026	A202	EXTERIOR ELEVATIONS	1/14/2026	A301	BUILDING SECTION	1/14/2026	11- ELECTRICAL			A31	LIGHTING INTENSITY LAYOUT	10/16/2025
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**HISTORIC PROPERTY DESCRIPTIONS**

**TRACT 1**

BEING PART OF AN OLD IRON PIN IN THE SOUTHEASTERN RIGHT OF WAY OF MARYVILLE PIKE AND CORNER TO J & R SOUTHERN (INSTRUMENT NUMBER: 20006130039891), THENCE FROM SAID OLD IRON PIN AND ALONG THE SOUTHWESTERN RIGHT OF WAY OF MARYVILLE PIKE, NORTH 41 DEG. 18 MIN. EAST 172.56 FEET TO A NEW NAIL IN THE SOUTHWESTERN RIGHT OF WAY OF MARYVILLE PIKE, THENCE SOUTH 37 DEG. 30 MIN. WEST 140.45 FEET TO A NEW IRON PIN, THENCE SOUTH 43 DEG. 20 MIN. WEST 130.00 FEET TO AN IRON PIN NEW IN THE LINE OF J & R SOUTHERN, THENCE NORTH 42 DEG. 30 MIN. WEST 120.63 FEET TO AN OLD IRON PIN, CONTAINING 0.43 ACRES, MORE OR LESS, AS SHOWN ON THE SURVEY FOR BHAVNAR BHALODA, BY STANLEY E. HANDEL, RLS# 967, DATED MAY 26, 2010, BEARING DRAWING NUMBER 1005053.

BEING PART OF THE SAME PROPERTY CONVEYED TO BHAVNAR BHALODA, MARRIED, AND ASLA N. PATEL, MARRIED, BY SPECIAL WARRANTY DEED FROM REGIONS BANK, AN ALABAMA BANKING CORPORATION, DATED JUNE 4TH, 2010, AND RECORDED JUNE 11, 2010, AS INSTRUMENT NUMBER: 201006110077604, IN THE REGISTER'S OFFICE FOR KNOX COUNTY, TENNESSEE.

**TRACT 2**

BEING PART OF A NEW NAIL SET IN THE SOUTHEASTERN RIGHT OF WAY OF MARYVILLE PIKE, SAID NEW NAIL BEING IN A SOUTHWESTERLY DIRECTION, 43.26 FEET, MORE OR LESS, FROM THE SOUTHWESTERN RIGHT OF WAY OF SIMS ROAD, THENCE FROM SAID NEW NAIL, SET, AND ALONG THE SOUTHEASTERN RIGHT OF WAY OF MARYVILLE PIKE, NORTH 41 DEG. 18 MIN. EAST 42.44 FEET TO A NEW NAIL SET, THENCE CONTINUING ALONG THE SOUTHWESTERN RIGHT OF WAY OF MARYVILLE PIKE, NORTH 41 DEG. 18 MIN. EAST 134.10 FEET TO AN OLD IRON PIN, THENCE SOUTH 43 DEG. 20 MIN. WEST 71.68 FEET TO A NEW IRON PIN, THENCE NORTH 37 DEG. 30 MIN. WEST 140.45 FEET TO THE POINT OF BEGINNING, CONTAINING 0.21 ACRES, MORE OR LESS, AS SHOWN ON THE SURVEY FOR BHAVNAR BHALODA, BY STANLEY E. HANDEL, RLS# 967, DATED MAY 26, 2010, BEARING DRAWING NUMBER 1005053.

BEING PART OF THE SAME PROPERTY CONVEYED TO BHAVNAR BHALODA, MARRIED, AND ASLA N. PATEL, MARRIED, BY SPECIAL WARRANTY DEED FROM REGIONS BANK, AN ALABAMA BANKING CORPORATION, DATED JUNE 4TH, 2010, AND RECORDED JUNE 11, 2010, AS INSTRUMENT NUMBER: 201006110077604, IN THE REGISTER'S OFFICE FOR KNOX COUNTY, TENNESSEE.

**TRACT 3**

BEING PART OF AN OLD IRON PIN IN THE SOUTHWESTERN RIGHT OF WAY OF SIMS ROAD, IN THE NORTHWESTERN CORNER OF KERRY DEED BOOK 2714, PAGE 11003), THENCE FROM SAID OLD IRON PIN AND ALONG THE LINE OF KERRY, SOUTH 43 DEG. 18 MIN. WEST 201.00 FEET TO A NEW IRON PIN CORNER TO KERRY AND IN THE LINE OF J & R SOUTHERN (INSTRUMENT NUMBER: 20006130039891), THENCE NORTH 43 DEG. 37 MIN. WEST 50.09 FEET TO AN OLD IRON PIN, THENCE NORTH 43 DEG. 20 MIN. EAST 1201.66 FEET TO AN OLD IRON PIN, THENCE NORTH 43 DEG. 20 MIN. WEST 71.68 FEET TO A NEW IRON PIN, THENCE SOUTH 43 DEG. 20 MIN. WEST 140.45 FEET TO THE POINT OF BEGINNING, CONTAINING 2.3 ACRES, MORE OR LESS, AS SHOWN ON THE SURVEY FOR BHAVNAR BHALODA, BY STANLEY E. HANDEL, RLS# 967, DATED MAY 26, 2010, BEARING DRAWING NUMBER 1005053.

BEING PART OF THE SAME PROPERTY CONVEYED TO BHAVNAR BHALODA, MARRIED, AND ASLA N. PATEL, MARRIED, BY SPECIAL WARRANTY DEED FROM REGIONS BANK, AN ALABAMA BANKING CORPORATION, DATED JUNE 4TH, 2010, AND RECORDED JUNE 11, 2010, AS INSTRUMENT NUMBER: 201006110077604, IN THE REGISTER'S OFFICE FOR KNOX COUNTY, TENNESSEE.

**UTILITY OWNERS & AVAILABILITY:**

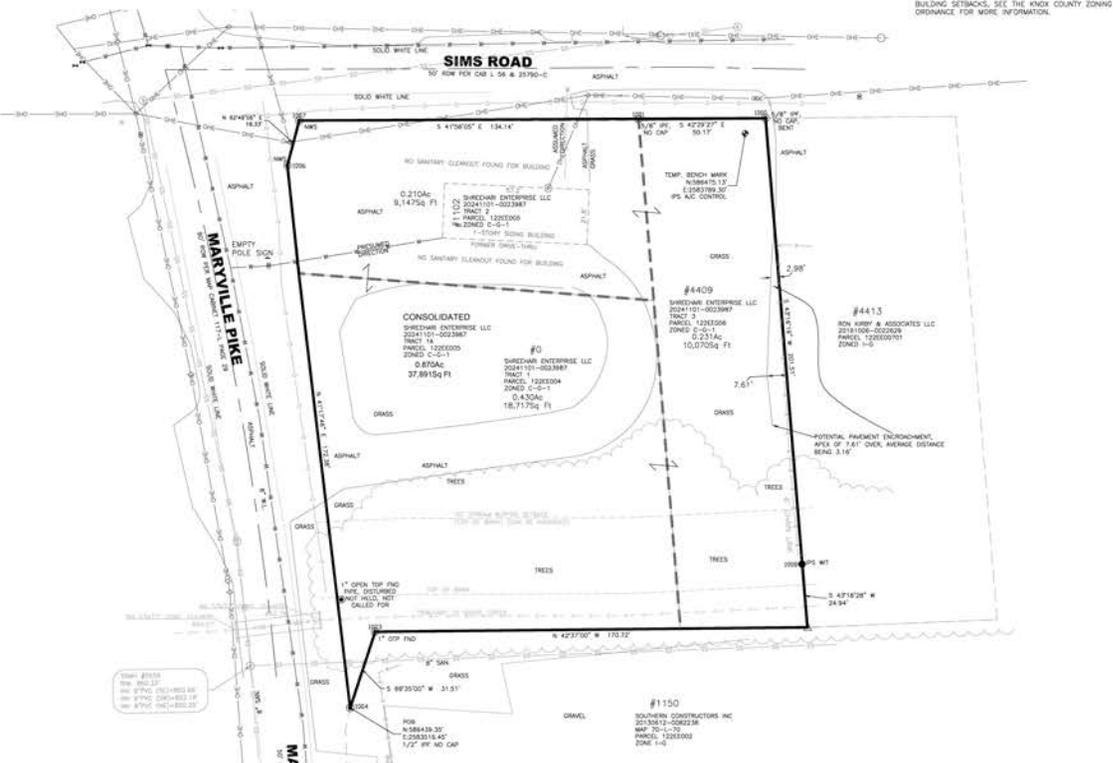
911 CONFIRMATION: 8231881548  
SEWER, GAS, WATER, ELECTRIC, TRINITY SIGNAL, KNOXVILLE UTILITIES BOARD, KNOXVILLE, TN 37924-2911  
LANGLINE PHONE/CATV/FIBER OPTIC AT&T DISTRIBUTION 270.791.2182 COMCAST (ADOLPHUS CABLE COMMUNICATIONS) 800.534.6489 VERIZON 865.352.3468

**SURVEY NOTES:**

- A FIELD SURVEY WAS PERFORMED BY ABRE JONES CONSULTING JULY 11-28. EQUIPMENT USED: TRIMBLE R780 AND TRIMBLE R10 GNSS RECEIVERS.
- THIS SURVEY IS IN ACCORDANCE WITH T.C.A. RULE 1360-03-01. PRECISION CALCULATED WITH RELATIVE POSITIONAL ACCURACY OF THE DUPLICATE SINGLE RECEIVER GNSS OBSERVATIONS ON THE MONUMENTS LOCATED DOES NOT EXCEED  $\pm 0.022'$ , WITH ALLOWABLE TOLERANCE BEING  $\pm 0.034'$  (SOPHA).
- THE BASIS FOR BEARINGS AND REFERENCED MAGNAN IS FROM THE TENNESSEE STATE PLANE COORDINATE SYSTEM, NAD83(GD13), NAD 1983, USING TRIMBLE RTX NETWORK CORRECTIONS.
- UNLESS SPECIFIED OTHERWISE, ALL IPS ARE 5/8" IRON REBAR 18" LONG WITH PINK PLASTIC CAP STAMPED ABRE JONES TN 2924. ALL WMS ARE MAG NAIL WITH ALUMINUM WADDER OF SAME MARKING. WITNESSE CORNERS ARE MARKED AS SUCH.
- NO PORTION OF THIS PROPERTY IS LOCATED IN A SPECIAL FLOOD HAZARD AREA ZONE AS ON FEMA FLOOD PANEL 47030C0219, EFFECTIVE DATE 5/2/2007.
- OTHER UNRECORDED EASEMENTS MAY EXIST THAT AFFECT THE PROPERTY, NO TITLE SEARCH WAS PROVIDED BY AN ATTORNEY BUT RESEARCH WAS CONDUCTED BY LAND SURVEYOR.
- THIS PROPERTY IS ZONED C-5-(GENERAL COMMERCIAL), PER KNOX COUNTY ZONING ORDINANCE. THIS PROPERTY HAS NO CONDITIONS REGARDING BUILDING SETBACKS. SEE THE KNOX COUNTY ZONING ORDINANCE FOR MORE INFORMATION.

**LEGEND:**

- TEMPORARY BENCHMARK
- BUILDING
- HOLE CENTERLINE
- GAS VALVE
- ICE LINE
- MAIL LINE
- DEEL BOOK IN PAGE
- COORDINATED LOT LINE
- ELECT. LINE (NEAR)
- ELECTRIC METER
- FENCE (TYPE LABELLED)
- GRASS LINE
- INSET
- IRON PIN FOUND
- IRON PIN SET
- NAIL AND WADDER FOUND
- NAIL AND WADDER SET
- PLAT BOOK & PAGE
- PLAT CORNER & SLICE
- POINT OF BEGINNING
- POWER POLE
- PROPERTY LINE
- PROPERTY LINE ADJOINING
- GIVEN TOP PIPE FOUND
- RIGHT-OF-WAY
- SEWER LINE
- SANITARY SEWER CLEANOUT
- SANITARY SEWER MANHOLE
- SETBACK LINE
- ASPHALT LINE
- STRIPING/PAVT LINE
- STORM SEWER GATE INLET
- STORM DRAIN LINE AND FLOWLINE
- STORM DRAIN MANHOLE
- TROUSLINE
- LOT CONSOLIDATION SYMBOL
- WATER LINE
- WATER METER
- WATER VALVE
- SOIL



**PURPOSE:**

THE PURPOSE OF THIS CATEGORY 1 SURVEY IS TO CONSOLIDATE THE BOUNDARIES OF 1102 MARYVILLE PIKE.

**PROPERTY MAP REFERENCE**

BEING PARCELS 1221E004, 122E0005, & 122E0011, AS SHOWN ON KNOX COUNTY PROPERTY ASSESSOR'S MAP.

**LAND SURVEYOR'S CERTIFICATION:**

I DO HEREBY CERTIFY THAT THIS RECORD PLAT WAS PREPARED UNDER MY DIRECTION AND BELIEVE THE BOUNDARIES OF THE PROPERTY ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF, AND THAT ALL MONUMENTS SHOWN HEREON ACTUALLY EXIST, AND THAT THEIR LOCATION, SIZE, AND MATERIAL, ARE CORRECTLY SHOWN.

**GLOBAL POSITION SYSTEM SURVEY NOTES**

- I HEREBY CERTIFY THAT THIS IS A CATEGORY 1 SURVEY AND WAS CONDUCTED IN ACCORDANCE WITH RULE 1360-03-01 (GLOBAL POSITION SYSTEMS) PREPARED AND PROMULGATED BY THE TENNESSEE STATE BOARD OF SURVEYORS FOR LAND SURVEYORS. THIS SURVEY WAS CONDUCTED WITH DUAL-FREQUENCY GNSS EQUIPMENT BEING ON THE 2007 CORS NETWORK.
- POSITIONAL ACCURACY: 0.022'
- DATE OF SURVEY: 07/11-28/2025
- HORIZONTAL DATUM/VERTICAL TENNESSEE STATE PLANE (NAD83) COORDINATES FOR CONTROL POINTS AND MONUMENTS ARE 5 METERS EACH.
- VERTICAL DATUM: ANGERS ELEVATIONS GIVEN IN FEET ARE EQUAL TO DYNAMICALLY HEIGHTS DERIVED USING 2020 '81.
- NO CORRECTED GRAY FACTORS WERE USED.

**CERTIFICATE OF ACCURACY**

I HEREBY CERTIFY THAT THE PLAT SHOWS AND DESCRIBES HEREON A NEW AND CORRECT SURVEY TO THE ACCURACY REQUIRED BY THE AREA COUNTY THROUGH THE PLAT. THE MONUMENTS SHOWN HEREON HAVE BEEN PLACED AS SHOWN HEREON, TO THE SUFFICIENCY OF THE COUNTY ENGINEER, COMMISSIONER OF THE COUNTY ENGINEER.

DATE: SURVEYOR:

Point	Northing	Easting	Description
1000	586,473.195	2,583,769.101	IPV / 5/8" BUSHING, NO CAP
1001	586,510.188	2,583,765.216	IPV / 5/8"
1003	586,492.101	2,583,345.325	IPV / 1" OIP
1004	586,433.348	2,583,651.450	IPV / 1/2" OR 1/2" CAP
1006	586,620.972	2,583,658.998	NWS / A.JONES 2924, ALUM.
1007	586,609.337	2,583,675.270	NWS / A.JONES 2924, ALUM.
1008	586,344.748	2,583,677.956	IPS / A.JONES 2924

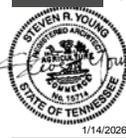
**RECORD PLAT OF 1102 MARYVILLE PIKE**

LOCATED AT: 1102 MARYVILLE PIKE, KNOXVILLE, TN 37920  
KNOX COUNTY  
CLIENT: OYSK3 ARCHITECTS, 1549 WESTERN AVE, KNOXVILLE, TN 37921  
OWNER: SHREKAR ENTERPRISE, LLC, 4853 MASTERS DR, MARYVILLE, TN 37801

REVISIONS	DATE	DESCRIPTION



DRAWN BY: JMK  
CHECKED BY: ANJ  
APPROVED BY: ANJ  
PROJECT: 25107  
DATE: 07/21/2025  
SHEET: 24/30



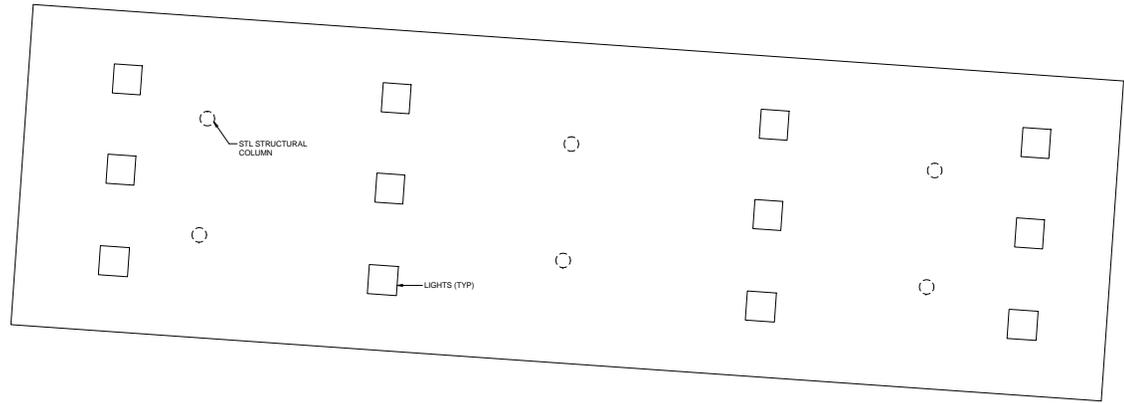
**MARYVILLE PK., GAS STATION FOR SAM PATEL**  
1102 Maryville Pike, Knoxville, TN., 37920

DRAWN BY: KG + AB  
SURVEY

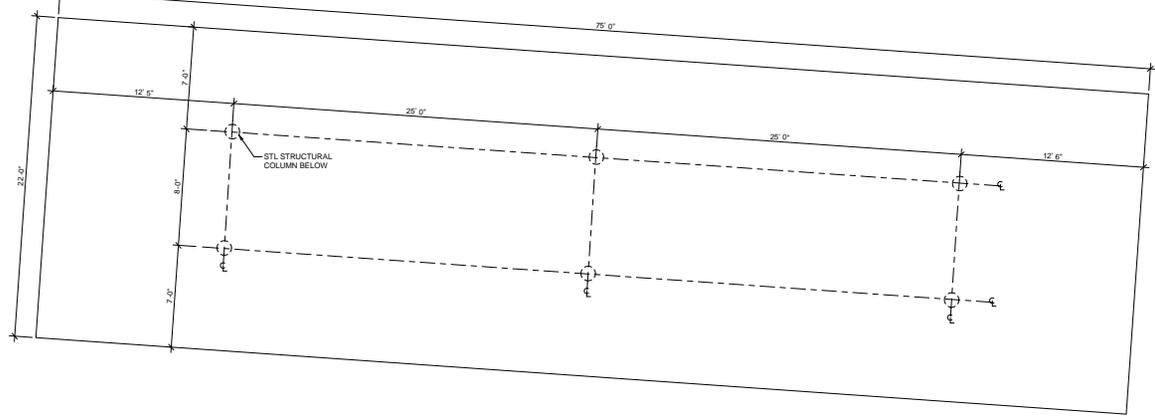
**A100b**



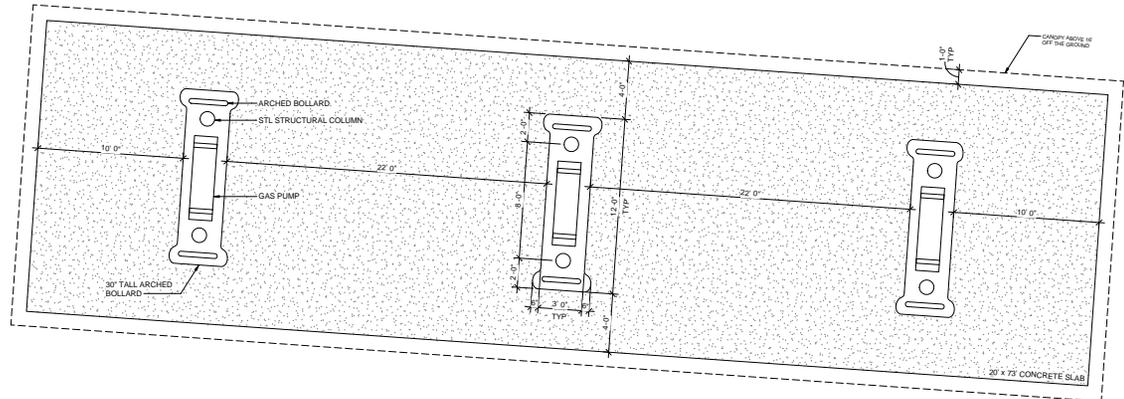




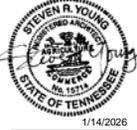
3 GAS PUMP ISLAND CANOPY REFLECTED CEILING PLAN  
 A103 1/4" = 1'-0"



2 GAS PUMP ISLAND CANOPY ROOF PLAN  
 A103 1/4" = 1'-0"



1 GAS PUMP ISLAND CONCRETE PAD & CANOPY PLAN  
 A103 1/4" = 1'-0"



1/14/2026

**MARYVILLE PK., GAS STATION  
 FOR SAM PATEL**  
 1102 Maryville Pike, Knoxville, Tn., 37920

DATE	DESCRIPTION

DRAWN BY: KG + AB

GAS PUMP ISLAND & CANOPY PLAN

**A103**

PROJECT : 25107  
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**LANDSCAPE LEGEND**

	SHADE TREE
	SMALL SHADE TREE
	EVERGREEN
	SHRUB
	EXISTING TREES

**NOTE:**  
ALTERNATE PLANTS AND OTHER APPROVED BUSHES MAY BE PROPOSED BY LANDSCAPER. THEY MUST BE APPROVED BY JURISDICTION. PROVIDE SHOP DRAWINGS FOR APPROVAL.

- FOUNDATION PLANTING**
- (A) 60% OF FRONT AND ISLANDS = 135 x .8 = 81 LIN. FT. REQUIRED  
110 LIN. FT. PROVIDED
  - 1 SHRUB PER 1' = 81/3 = 27 SHRUBS REQUIRED  
29 PROVIDED
  - 1 SHADE TREE PER 150' = 81/30 = 2.7 TREES PROVIDED
  - 60% AREA REQUIRED & PROVIDED GROUND COVER PLANTED IN PERENNIALS
  - (B) COMMERCIAL ZONE ADJACENT TO INDUSTRIAL ZONE - NO BUFFER REQUIRED
  - (C) EXISTING WOODED AREA:  
TREES SHALL BE A TREE PROTECTION ZONE PER ZONE ORDINANCE 12.9 C.
  - (D) PER METER: APPROXIMATE PAVING AREA 12,000 S.F. LESS THAN 20,000 S.F. BUT MORE 10,000 S.F. REQUIRED  
6'-0" PARKING SETBACK  
3 TREES PER 100'  
a. 22/ 7 TREES (4 SHADE; 3 EVERGREEN) PROVIDED, 22 SHRUBS (11 EVERGREEN) PROVIDED  
b. 7 TREES; 2 SPECIES SHADE, EVERGREEN  
c. 22 BUSHES 22; 40% 5 SPECIES  
d. 60% AREA TO BE PLANT AREA W/ GROUND COVER, PERENNIALS  
1,600 S.F. x .6 = 1,008 S.F. REQUIRED & PROVIDED
  - (E) RETAINING WALL BETWEEN STREAM BUFFER AND PAVING SHALL BE PROVIDED. THE DIFFERENCE IN ELEVATION OF GRADES VARIES FROM 1' TO 6'
- NOTE: SEE A105 FOR PLANTING DETAILS**
- (F) REMOVE EXISTING PAVEMENT IN THIS AREA
  - (G) RETAIN PAVEMENT PER "KAT" BUS STOP IF REQUIRED. F NOT, REMOVE PAVEMENT
  - (H) BUS STOP SIGN
  - (J) GRAVEL/RNGS HANDICAP WALK 1 IN 20 SLOPE OR LESS
- GAS STATION**
- |                         |                         |
|-------------------------|-------------------------|
| 9.3-0 GAS PUMP ISLAND   | STREET LOT LINE SETBACK |
| 15' (PARALLEL)          | 15' PERPENDICULAR       |
| 15' ALL OTHER LOT LINES |                         |
- |                       |                |
|-----------------------|----------------|
| 9.3-0 GAS PUMP CANOPY | 15' PROP. LINE |
| 9.3-F                 | 25' TDOT       |
| 9.3-K                 | 3' STACK       |
|                       | 18'            |



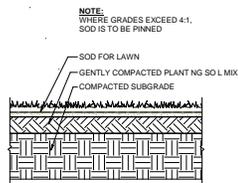
**MARYVILLE PK., GAS STATION  
FOR SAM PATEL**  
1102 Maryville Pike, Knoxville, Tn., 37920

DATE	DESCRIPTION

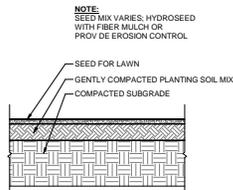
DRAWN BY: KG + AB  
LANDSCAPE PLAN

7 LANDSCAPE PLAN  
A104 1" = 10'-0"

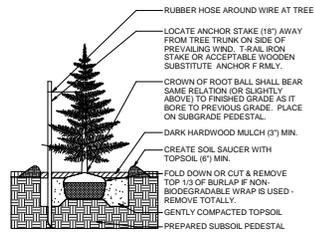




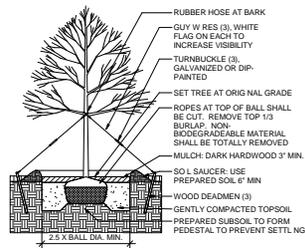
5 SOD DETAIL  
3/8" = 1'-0"



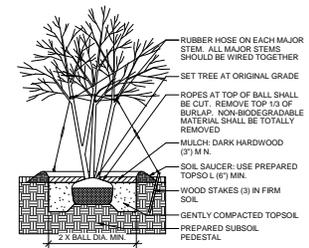
4 SEED DETAIL  
3/8" = 1'-0"



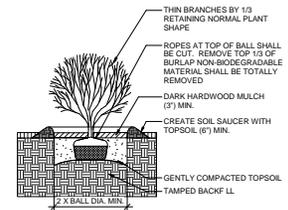
3 CONIFEROUS TREE PLANTING DETAIL  
N.T.S.



2 DECIDUOUS TREE PLANTING DETAIL  
N.T.S.



1 MULTI-STEM TREE PLANTING DETAIL  
N.T.S.



6 SHRUB PLANTING DETAIL  
N.T.S.

**LANDSCAPE PLAN NOTES:**

- L1 ALL GROUND DISTURBED BY CONSTRUCTION SHALL BE REPAIRED/REPLACED WITH TOPSOIL. THIS SHALL BE GRADED, MAILED, SEEDED, MULCHED, & WATERED PER SPECIFICATIONS, UNLESS OTHER LANDSCAPING IS INDICATED.
- L2 ALL TREES RETAINED OR PROVIDED TO MEET THE REQUIREMENTS OF THE JURISDICTION SHALL BE PROPERLY MAINTAINED TO ENSURE THEIR SURVIVAL. RESERVE EXISTING TREES WHERE FEASIBLE. PROVIDE PROTECTIVE BARRIER FOR PRESERVED TREES.
- L3 BETWEEN PARKING LOT & RESIDENTIAL ZONES, PROVIDE PERIMETER SCREENING CONTINUOUS LANDSCAPE (50% EVERGREEN SHRUBS).
- L4 PARKING LOT ISLANDS SHALL BE PLANTED AREA WITH (1) DECIDUOUS TREE, MINIMUM 2" CALIPER & 8'-0" TALL. AREA SHALL BE PLANTED WITH VINES, SHRUBS, GRASS, OR GROUND COVER.

**LANDSCAPE PLANTS:**

- LARGE TREES:**
  - 1 RED MAPLE
  - 2 CHESTNUT OAK
- MEDIUM TREES:**
  - 8 COLORADO BLUE SPRUCE
  - 6 LAKE BARK ELM
- SMALL TREES:**
  - 10 JAPANESE MAPLE
  - 11 EASTERN RED BUD
  - 12 CREPE MYRTLE
  - 13 FLOWER DOGWOOD
- BUSHES ETC:**
  - 16 BURLY BRUNER HOLLY (SCREEN/BUFFER)
  - 20 VIRG NIA SWEETSP RE
  - 21 BARBERRY
  - 22 CR MESON PYGMY
  - 23 DWARF YAUPOH HOLLY
- PLANTING BEDS:**
  - 25 MONDO GRASS, OPHIOPOGON
  - 26 JUNIPER BAR HARBOR JUNIPERUS WILTONII

MARYVILLE PK., GAS STATION  
FOR SAM PATEL

1102 Maryville Pike, Knoxville, Tn., 37920

DATE	
BY	
CHECKED	
SCALE	

DRAWN BY: KG + AB  
PLANTING DETAILS

A105

PROJECT : 25107  
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oysk<sup>3</sup>

architects

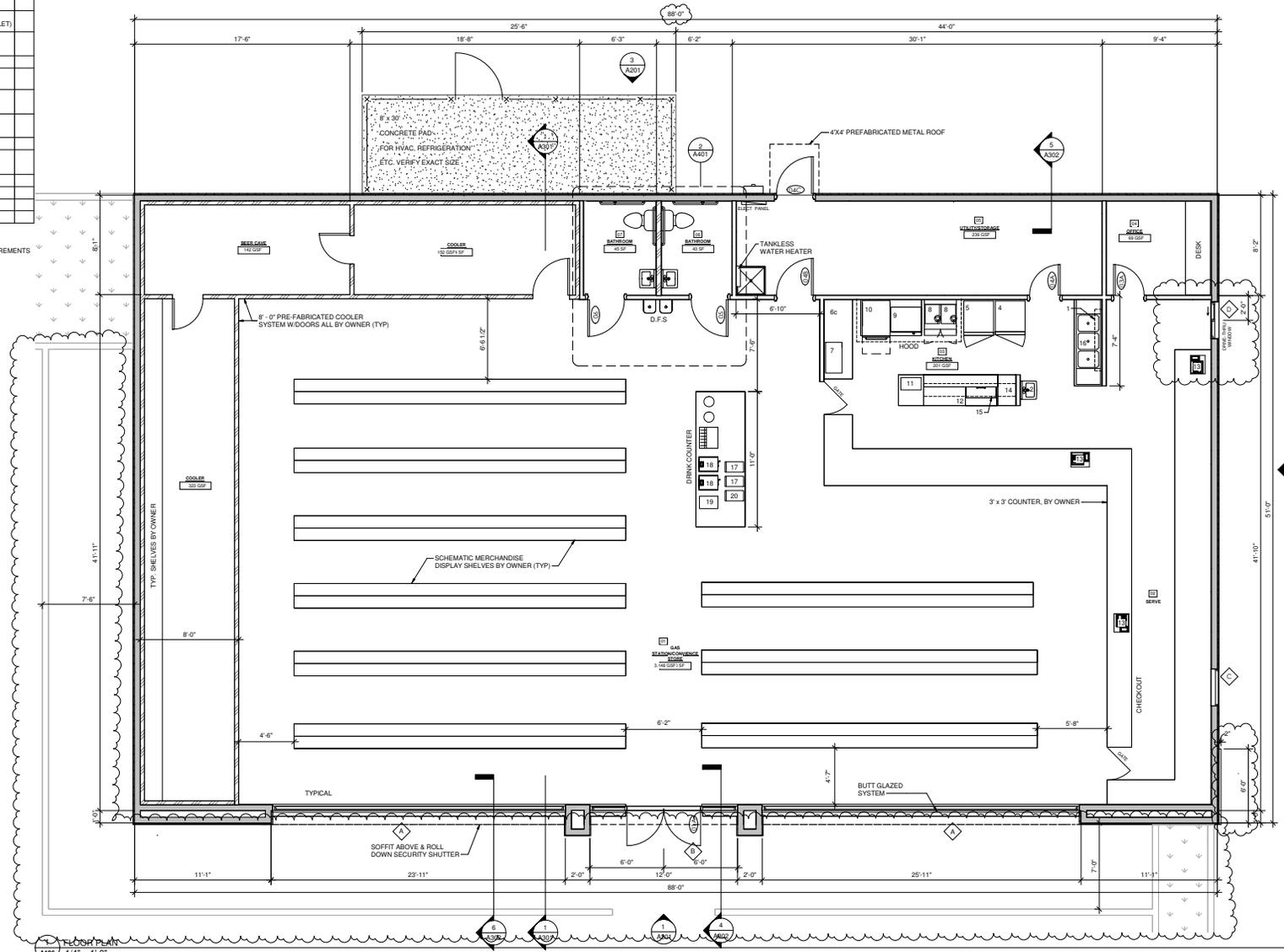
1142 Williams Avenue Suite 100  
Knoxville TN 37901  
office@oyskarchitects.com

1/14/2026

KITCHEN EQUIPMENT SCHEDULE					
EQUIPMENT	ITEM ID	QTY	MODEL #	ELECTRICAL	GAS
POT HOLDER	1				
HAND SINK	2		HS15-CWP		
MOP SINK	3		MS182012-CWP		
FREEZER	4		C-1F-HC	1P-20	
REFRIGERATOR	5		CR14C	1P-20	
WORK TABLE	6				
48"	6A		T2448CWP-3		
60"	6B		T2460CWP-3		
72"	6C		T2472CWP-3		
MICROWAVE	7			1P-20	
FRYER	8	2	C-F40-NG-LP		X
RANGE	9		F3218 24-1HB		X
PIZZA OVEN	10		QWA1CW1 P P031		X
HEATED FRY DUMP STATION	11		HMC-1220-2-FD	1P-20 (5-15P OUTLET)	
SANDWICH PREP TABLE	12		C-SP60-16-1HC-SM	1P-20	
CASH REGISTER	13			1P-20	
DOUBLE PASS THROUGH SHELF	14		PTS-1860-2 PTS-1872-2		
INFRARED STRIP HEATERS	15		UGA-18D	2P-20	
3 COMPARTMENT SINK	16		PA0603		
<b>DRINK COUNTER EQUIPMENT</b>					
COKE MACHINE	17			1P-20	
COFFEE	18			1P-20	
TEA	19			1P-20	
FROZEN BEVERAGE	20			1P-20	

VERIFY W/CLIENT & ACTUAL KITCHEN EQUIPMENT REQUIREMENTS  
 PREFABRICATED COOLERS, BY OWNER; VER FRY ELECTRICAL REQUIREMENTS

FLOOR PLAN NOTES:	WALL LEGEND
ALL DIMENSIONS ARE TO FACE OF STUD UNLESS NOTED OTHERWISE	2X6 EXT. WOOD STUDS @16" O.C. WITH R-20 BATT INSULATION.
EXTERIOR STUDS LINE UP W/CONCRETE SLAB EDGE	5/8" GYP BOARD INTERIOR S DE
	5/8" WOOD SHEATHING
	WEATHER BARR FR & EXTERIOR FINISHES (SEE EXTERIOR ELEVATIONS)
	INT. 2X6 @16" O.C. W/R 20 BATT. INSULATION. FIN. PER FIN. SCHEDULE
	2X4 WOOD STUDS @16" O.C.
	5/8" GYP BOARD BOTH SIDES. INSULATED INTERIOR WALL. SEE FINISH SCHEDULES
	2X4 WOOD STUDS @16" O.C. W/ BUTT GLAZED INSULATED GLASS STONEFRONT W/IFRS BRICK SEE EXTERIOR ELEVATIONS
	SEMI EXT. 2X6 WD. STUD @16" O.C. WALL W/INT. GYP BD & EXT. WD SHEATHING & W. BAR.
	2X4 WOOD STUDS @16" O.C. W/ BUTT GLAZED INSULATED GLASS STONEFRONT W/IFRS BRICK SEE EXTERIOR ELEVATIONS
	8" CHA R LNK FENCE W/CHA NL NK FENCE CE LING
	PREFABRICATED COOLER WALLS & DOORS BY OWNER



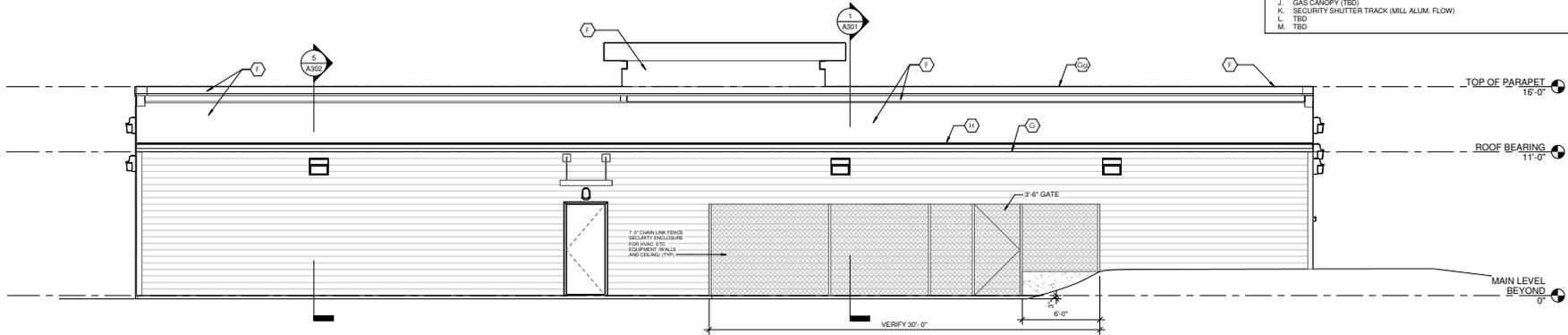
**FLOOR PLAN**  
 1/4" = 1'-0"



**MARYVILLE PK., GAS STATION**  
**FOR SAM PATEL**  
 1102 Maryville Pike, Knoxville, Tn., 37920

DRAWN BY: KG + AB  
 FLOOR PLAN

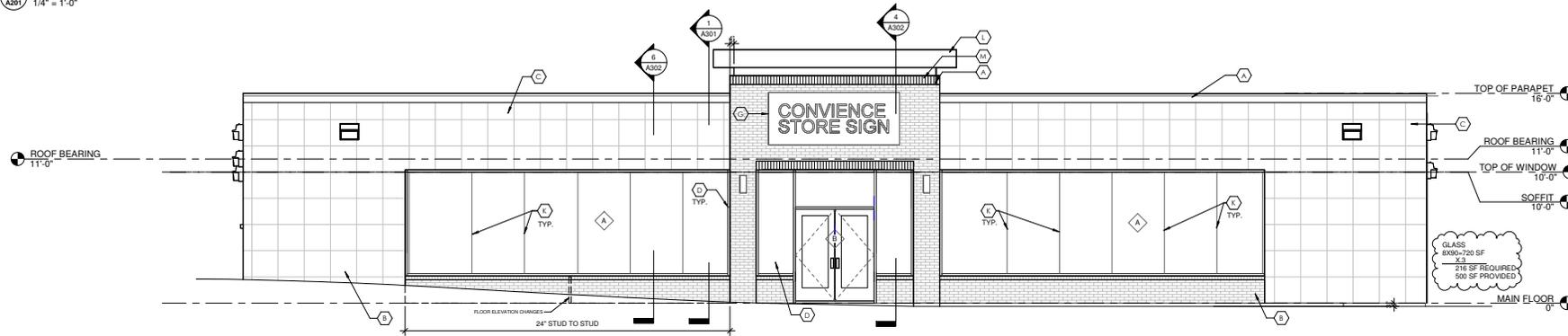
COLOR LEGEND	ELEVATION KEYNOTES
1 LIGHT GREY	A. BRICK VENEER - STO EPS, STO CAST BRICK: SUNDERLAND S94475 W/BROWN GRAY 2063 JOINT COLOR MORTAR (ROUNDED CORNER ROLLED)
2 SAME COLOR AS A	B. EPS BRICK VENEER BRICK ROWLOCK: HARRISON HARRISON S94897 ROUNDED CORNERS, ROLLED, MORTAR JOINT COLOR CEMENT GREY 2058
3 TBD HIGH CONTRAST	C. EPS - 2x2 (COLOR: LIGHT GREY)
	D. BUTT GLAZED NSUL, GLASS & ALUMINUM FRAME (COLOR: MEDIUM BRONZE)
	E. FIBER CEMENT HORIZONTAL SIDING (COLOR: LIGHT GREY)
	F. MEMBRANE ROOF & PARAPET
	G. METAL CAP FLASHING & FIBER CEMENT FASCIA
	Gp. METAL CAP FLASHING
	H. GUTTER
	J. GAS CANOPY (TBD)
	K. SECURITY SHUTTER TRACK (MILL ALUM. FLOW)
	L. TBD
	M. TBD



3 REAR ELEVATION  
A201 1/4" = 1'-0"



2 FRONT ELEVATION 2  
A201 1/4" = 1'-0"



1 FRONT ELEVATION 1  
A201 1/4" = 1'-0"



**MARYVILLE PK., GAS STATION**  
**FOR SAM PATEL**  
 1102 Maryville Pike, Knoxville, Tn., 37920

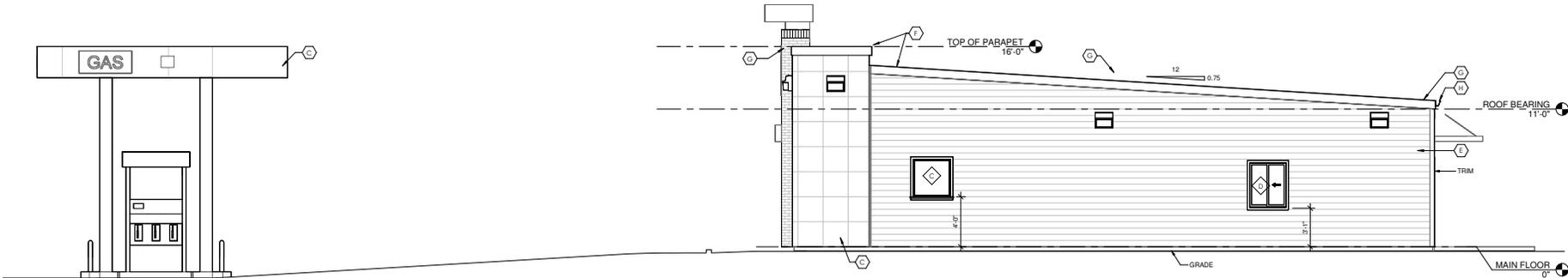
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DRAWN BY: KG + AB  
EXTERIOR ELEVATIONS

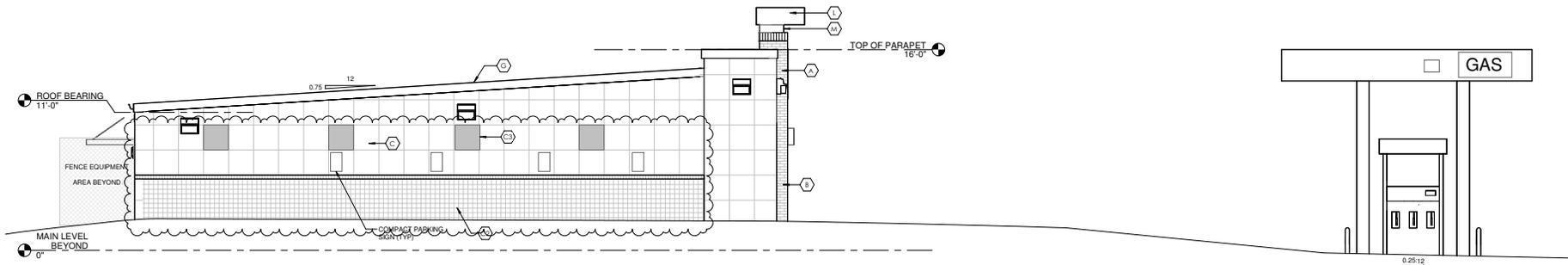
COLOR LEGEND	ELEVATION KEYNOTES
1 LIGHT GREY	A. BRICK VENEER - STO EPS, STO CAST BRICK; SUNDERLAND S8 4475 W/BROWN GRAY 2063 JOINT COLOR MORTAR (ROUNDED CORNER ROLLED)
2 SAME COLOR AS A	B. EPS BRICK VENEER BRICK ROWLOCK; HARRISON HARRISON S8 6897, ROUNDED CORNERS, ROLLED, MORTAR JOINT COLOR CEMENT GREY 2058
3 TBD HIGH CONTRAST	C. EPS - 2x2 (COLOR: LIGHT GREY)
	D. BUTT GLAZED NSUL, GLASS & ALUMINUM FRAME (COLOR: MEDIUM BRONZE)
	E. FIBER CEMENT HORIZONTAL SIDING (COLOR: LIGHT GREY)
	F. MEMBRANE ROOF & PARAPET
	G. METAL CAP FLASHING & FIBER CEMENT FASCIA
	Gp. METAL CAP FLASHING
	H. GUTTER
	I. GAS CANOPY (TBD)
	K. SECURITY SHUTTER TRACK (MILL ALUM. FLOW)
	L. TBD
	M. TBD



1/14/2026



2 RIGHT ELEVATION  
A202 / 1/4" = 1'-0"



1 LEFT ELEVATION  
A202 / 1/4" = 1'-0"

**MARYVILLE PK., GAS STATION**  
**FOR SAM PATEL**  
 1102 Maryville Pike, Knoxville, Tn., 37920

DATE	DESCRIPTION

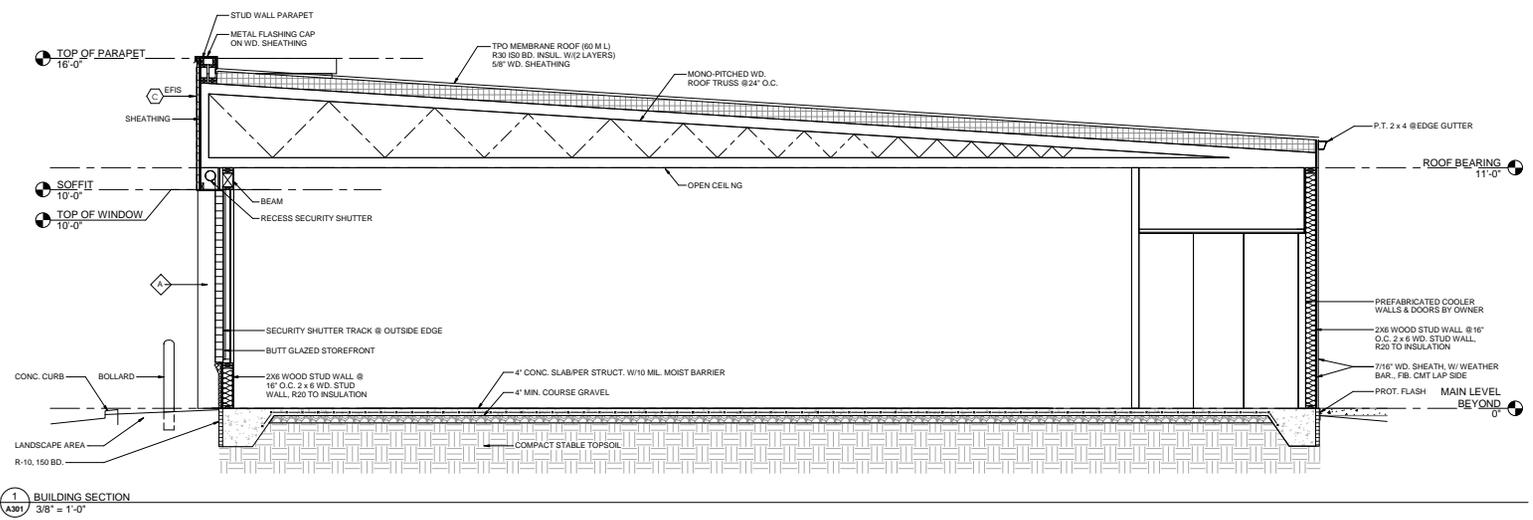
DRAWN BY: KG + AB

EXTERIOR ELEVATIONS

A202

PROJECT : 25107

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**1** BUILDING SECTION  
**A301** 3/8" = 1'-0"

**MARYVILLE PK., GAS STATION**  
**FOR SAM PATEL**  
 1102 Maryville Pike, Knoxville, Tn., 37920

DRAWN BY: KG + AB  
 BUILDING SECTION

**A301**  
 PROJECT : 25107  
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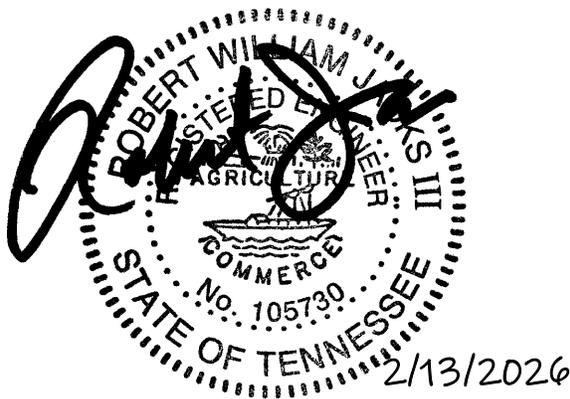


# Transportation Impact Study Maryville Pike Gas Station Knoxville, Tennessee



Revised February 13, 2026

Prepared for:  
oysk3 architects  
1545 Western Avenue, Suite 100  
Knoxville, TN 37921



## CONCLUSIONS & RECOMMENDATIONS

The following is an overview of recommendations to minimize the transportation impacts of the Maryville Pike Gas Station on the adjacent transportation system while attempting to achieve an acceptable traffic flow and safety level.



**Maryville Pike at Proposed West Entrance:** This intersection is projected to operate with average vehicle delays and minimal queues based on the current proposed layout, which shows dual exit lanes and no additional entering turn lanes on Maryville Pike. However, operationally, this will need to be modified based on the previously discussed results, which indicate that southbound left-turn movements into the development from Maryville Pike will require a separate turn lane due to meeting TDOT warrant thresholds.

The Proposed West Entrance at Maryville Pike, as shown in the site layout plan, is located roughly halfway along the development property's road frontage. The location of this entrance is constrained by the proximity of the traffic signal to the north and the buffer that will be required along the tributary to Goose Creek to the south.

This warranted southbound left-turn lane on Maryville Pike due to vehicle volumes would also be essential to prevent vehicle queues from blocking each other between this entrance and the signalized intersection to the north. Due to the signalized intersection having only single-lane approaches, as described in the existing conditions, a northbound left-turning motorist on Maryville Pike attempting to turn onto Edington Road is hampered by the lack of a protected green arrow and by opposing southbound vehicles. This situation causes vehicle backups on Maryville Pike to the south of the traffic signal. If a separate left-turn lane is not provided on Maryville Pike at the Proposed West Entrance, and due to its proximity to the traffic signal, this northbound vehicle blockage could easily block a southbound left-turning motorist from turning into the Proposed West Entrance, resulting in a "left-turn lockup" on Maryville Pike due to the conflicting and overlapping left-turn movements and queues.

Furthermore, due to their close proximity to each other, providing a separate southbound left-turn lane in the center of Maryville Pike at the Proposed West Entrance would also require providing a separate northbound left-turn lane at the traffic signal. If a separate northbound left-turn lane is provided at the traffic signal, this would cause a cascade of other issues and needs that would include requiring modifying the south approach of

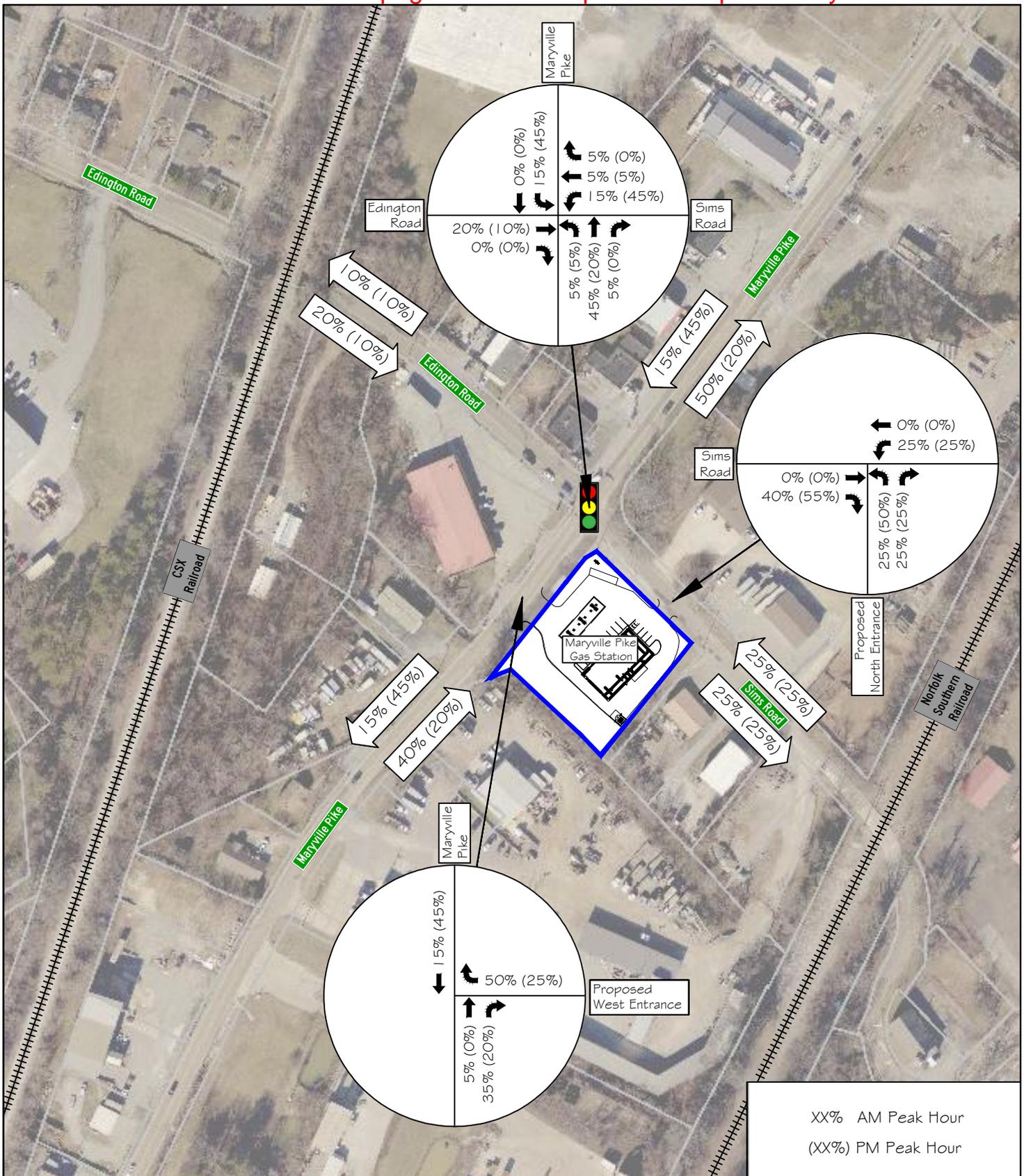
Maryville Pike at the traffic signal, replacing signal equipment and timing, and requiring utility pole and utility relocations, all within a limited amount of available right-of-way.

Thus, due to these issues and the high associated costs beyond the owner/developer's capability, it is recommended that the Proposed West Entrance be designed and constructed with a right-turn-in/right-turn-out (RIRO) configuration at Maryville Pike, rather than allowing full access as currently proposed. Modifying this entrance to operate as RIRO will necessarily require entering customers from the north on Maryville Pike to turn left at the traffic signal onto Sims Road, and exiting customers who wish to travel south will have to exit at Sims Road and then turn left at the traffic signal onto Maryville Pike.

Due to wholesale changes in traffic movements resulting from a RIRO configuration at the Proposed West Entrance to Maryville Pike, a revised analysis was undertaken to determine the effects of rerouting the projected customer paths into and out of the Maryville Pike Gas Station. This additional effort included a new capacity analysis and vehicle queue simulation, as performed previously; however, it used updated trip distributions and assignments, as shown in Figures 9, 10a, and 10b, along with updated projected traffic volumes shown in Figure 11. The results of the LOS and vehicle queue calculations with this revised RIRO configuration are included in Tables 11 and 12.

The results shown in Tables 11 and 12 indicate that all the studied intersections are expected to operate adequately with respect to vehicle delays and queues with the recommended revised RIRO design. None of the studied intersections are shown to be particularly affected by converting the Proposed West Entrance from a full-access entrance to a RIRO configuration. While single-lane approaches constrain flows at the existing traffic signal, the inclusion of the Maryville Pike Gas Station and the vehicle trips it will generate are expected to have minimal impact on the signalized intersection. Additionally, the Maryville Pike Gas Station development, with a RIRO entrance at Maryville Pike and a full-access entrance at Sims Road, is projected to be adequately served by these entrances with acceptable vehicle delays and queues.

It should be noted that with a RIRO configuration at the Proposed West Entrance, the number of pass-by trips is likely to be affected; however, for this revised analysis, it was assumed to remain the same, with an alternative route into and out of the proposed development site via the Proposed North Entrance at Sims Road and travel through the signalized intersection.



11812 Black Road  
 Knoxville, TN 37932  
 Phone: (865) 556-0042  
 Email: ajaxengineering@gmail.com

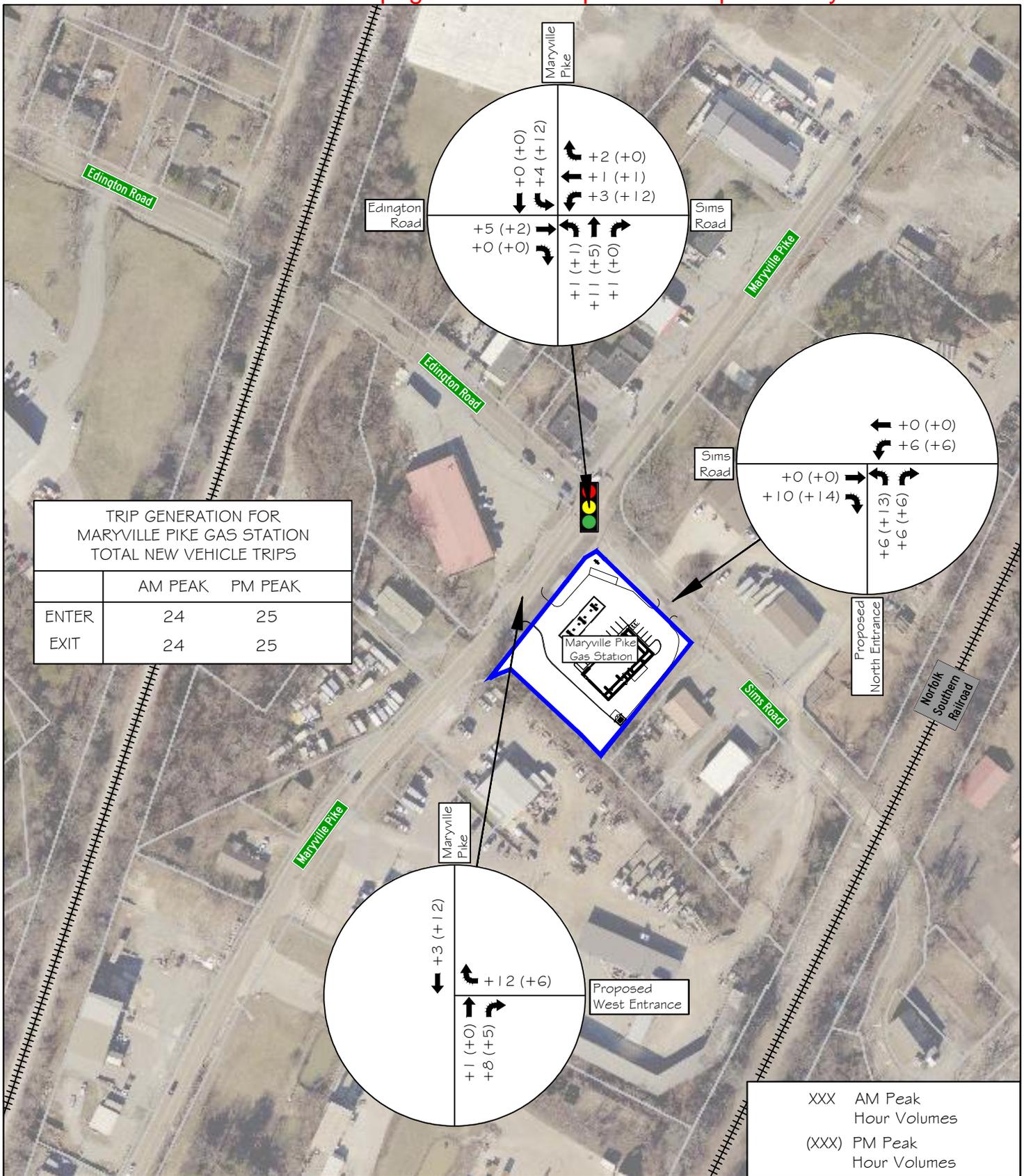
NOT TO SCALE



FIGURE 9

Maryville Pike Gas Station

Directional Distribution of Generated Trips during AM and PM Peak Hour for Maryville Pike Gas Station - RIRO ALT



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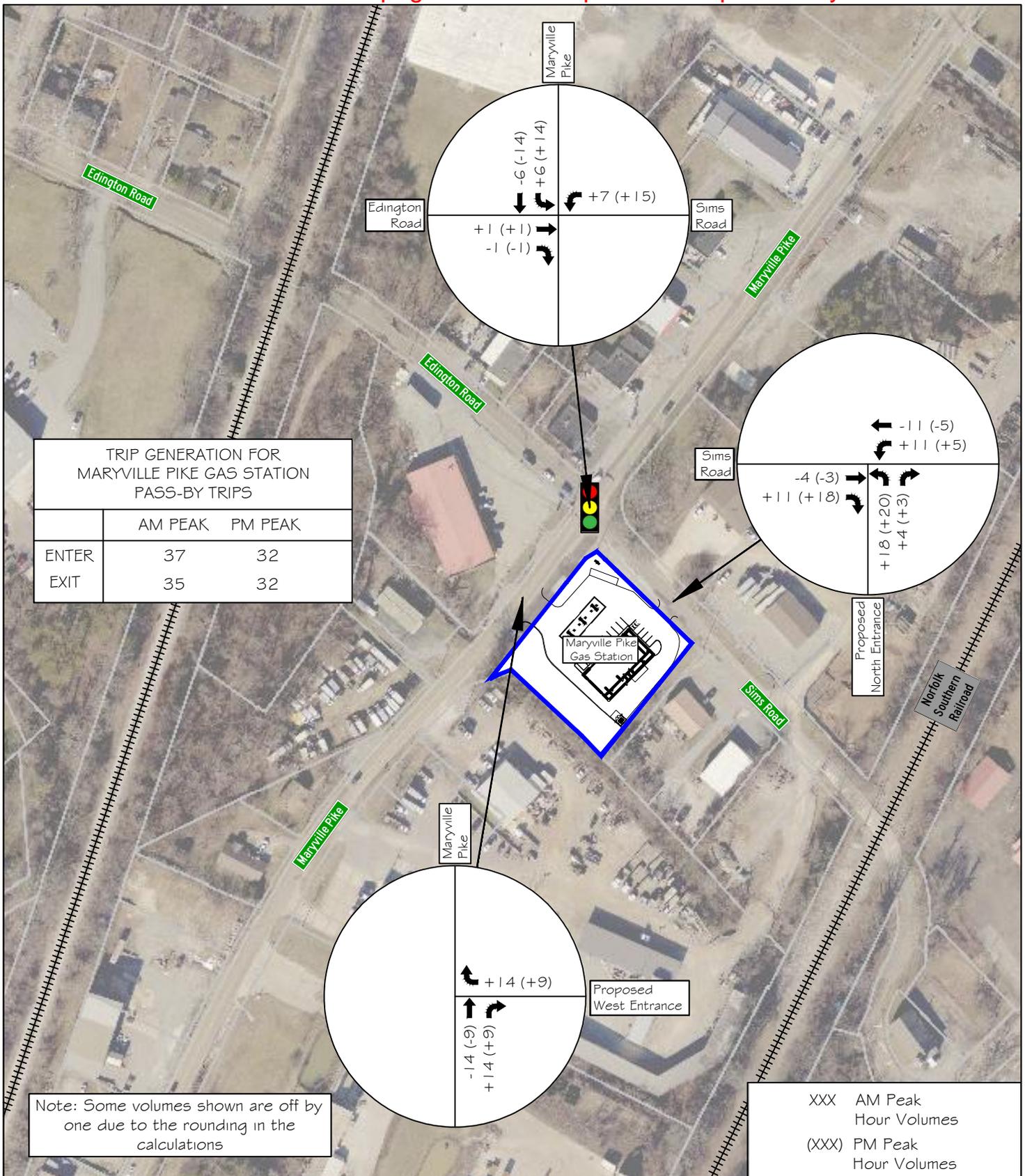
NOT TO SCALE



FIGURE 10a

Maryville Pike Gas Station

Traffic Assignment of Generated Trips during AM and PM Peak Hour - Total New Vehicle Trips for Maryville Pike Gas Station - RIRO ALT



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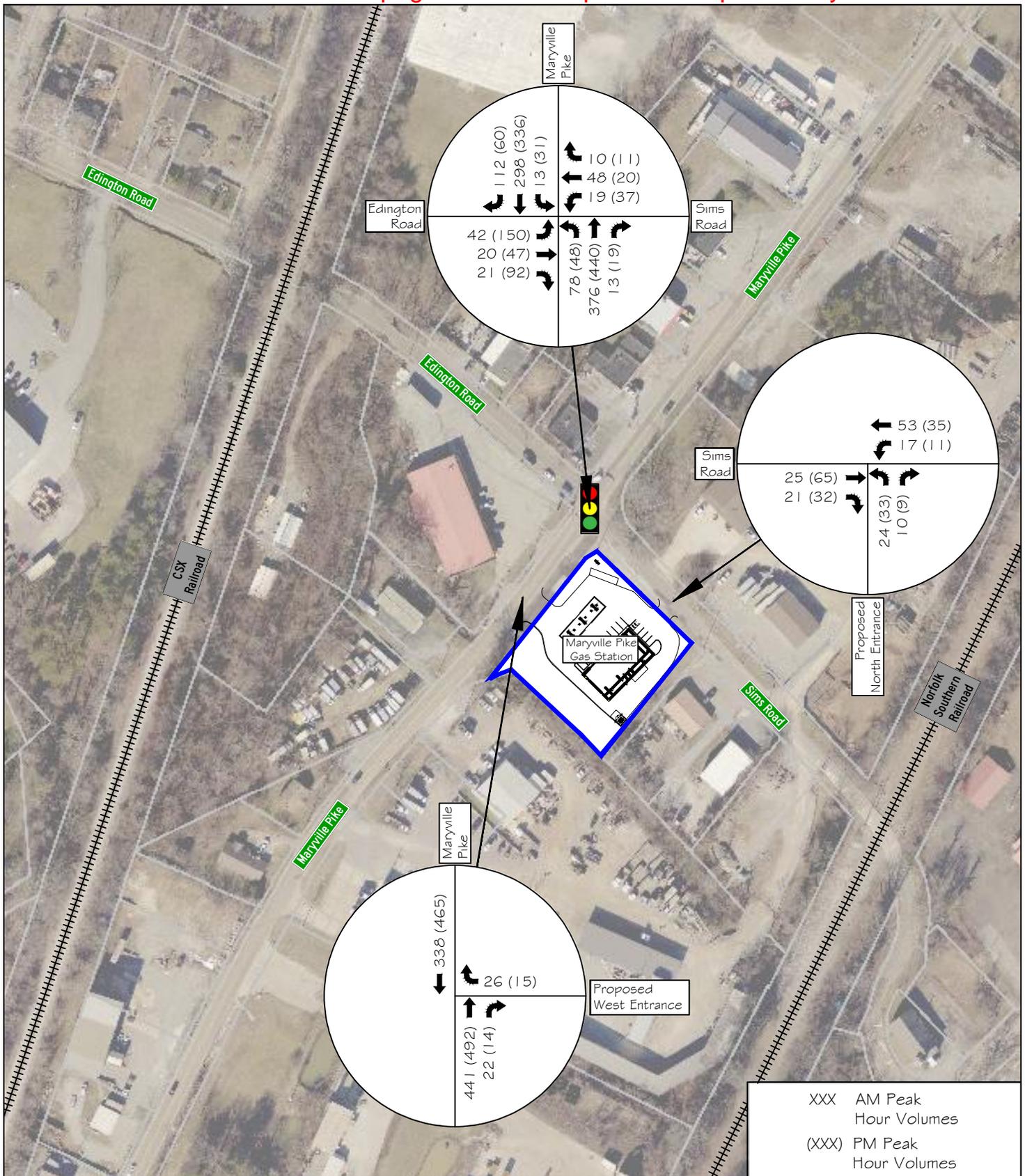
NOT TO SCALE



FIGURE 10b

Maryville Pike Gas Station

Traffic Assignment of Generated Trips during AM and PM Peak Hour - Pass-By Trips for Maryville Pike Gas Station - RIRO ALT



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FIGURE 11

Maryville Pike Gas Station

2027 Peak Hour Traffic Volumes -  
 Projected Traffic Conditions  
 With the Project - RIRO ALT

**TABLE 11**  
**INTERSECTION CAPACITY ANALYSIS RESULTS -**  
**2027 PROJECTED TRAFFIC CONDITIONS WITH THE PROJECT**  
**WITH RIRO ALTERNATIVE**

INTERSECTION	TRAFFIC CONTROL	APPROACH/ MOVEMENT	AM PEAK			PM PEAK		
			LOS <sup>a</sup>	DELAY <sup>b</sup> (seconds)	Max v/c <sup>c</sup>	LOS <sup>a</sup>	DELAY <sup>b</sup> (seconds)	Max v/c <sup>c</sup>
Maryville Pike (SB & NB) at Sims Road (WB) / Edington Road (EB)	 Signalized	Eastbound	B	18.2		D	38.9	
		Westbound	B	19.8		B	20.1	
		Northbound	B	16.5		B	18.1	
		Southbound	B	12.8		B	14.6	
		Summary	B	15.4	0.61	C	21.7	0.78
Sims Road (WB & EB) at Proposed North Entrance (NB)	 Unsignalized	Northbound Left/Right	A	9.3	0.043	A	9.6	0.056
		Westbound Left	A	7.3	0.012	A	7.4	0.008
Maryville Pike (SB & NB) at Proposed West Entrance (WB)	 Unsignalized	Westbound Right	B	11.5	0.050	B	11.8	0.031

Note: All analyses were calculated in Synchro 12 software and reported with HCM 7<sup>th</sup> Edition methodology

<sup>a</sup> Level of Service, <sup>b</sup> Average Delay (sec/vehicle), <sup>c</sup> Volume-to-Capacity Ratio

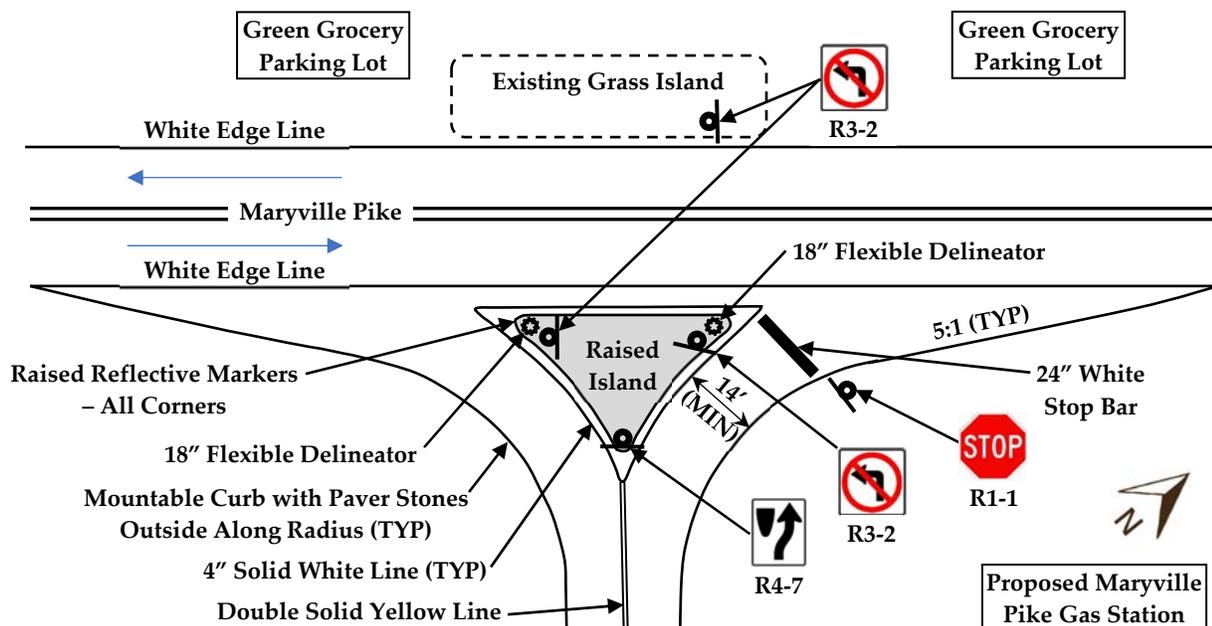
**TABLE 12**  
**VEHICLE QUEUE SUMMARY -**  
**2027 PROJECTED PEAK HOUR TRAFFIC CONDITIONS WITH THE PROJECT**  
**WITH RIRO ALTERNATIVE**

INTERSECTION	TRAFFIC CONTROL	APPROACH/ MOVEMENT	95 <sup>th</sup> PERCENTILE VEHICLE QUEUE LENGTH (ft)	
			AM PEAK HOUR	PM PEAK HOUR
Maryville Pike (SB & NB) at Sims Road (WB) / Edington Road (EB)	 Signalized	Eastbound Left/Thru/Right	77	196
		Westbound Left/Thru/Right	72	69
		Northbound Left/Thru/Right	189	193
		Southbound Left/Thru/Right	185	257
Sims Road (WB & EB) at Proposed North Entrance (NB)	 Unsignalized	Westbound Left/Thru	8	10
		Northbound Left/Right	46	49
Maryville Pike (SB & NB) at Proposed West Entrance (WB)	 Unsignalized	Westbound Right	51	39
		Northbound Thru/Right	112	145

Note: 95<sup>th</sup> percentile vehicle queues were calculated in SimTraffic 12 software

1a) It is recommended that the proposed RIRO entrance follow AASHTO and TDOT driveway entrance design standards, and the following guidelines should be considered in the design and construction to reduce the possibility of illegal left-turns at the entrance, both exiting and entering. The RIRO layout must be robust and command attention and respect to deter illegal left turns, especially given the observed motorists' propensity in the area to resort to risky, illegal maneuvers. A sketch of an RIRO layout is provided below (not to scale) and should incorporate the following:

- i) The entering and exiting lanes should have a width and an inner radius to accommodate the largest expected-sized vehicle entering and exiting at Maryville Pike. At a minimum, the lanes should be 14 feet in width.
- ii) The island should be constructed of raised concrete, with a minimum surface area of 100 square feet and a length of at least 40 feet along Maryville Pike.
- iii) The island should be offset by at least 4 feet from the thru lane's white edge line on Maryville Pike.
- iv) As shown below, traffic signage with breakaway posts at this intersection should include a Stop Sign (R1-1), a Keep Right Sign (R4-7), and No Left Turn Signs (R3-2). These signs should be installed facing the appropriate direction, with a minimum sign height of 7 feet above the ground surface. Three reflective raised markers should be located at each island's corner radius – one in the center of the island noses, then 2 foot spacing on either side.



1b) Based on the posted speed limit of 40 mph on Maryville Pike at the Proposed West Entrance location with a RIRO configuration, the required intersection sight distance is 385 feet for exiting right-turning vehicles. The existing sight distance to the south from the Proposed West Entrance location was visually estimated to be at least 425 feet. The civil site designer should ensure that the required intersection sight distance at the Proposed West Entrance is available and that vegetation, construction grading, and proposed signage do not interfere with this distance for exiting right turns with a RIRO configuration. Currently, the vegetation along the adjacent tributary, and especially where it crosses under Maryville Pike, is unmaintained. It is recommended that the site development include eliminating vegetation as necessary along the tributary at its crossing under Maryville Pike to maximize sight distance to the south and ensure it is maintained in future conditions.



**Existing Vegetation along Tributary at  
Maryville Pike  
(Looking South)**

- 1c) The construction of the Proposed West Entrance on Maryville Pike (SR 33) will require a TDOT Highway Entrance Permit. The developer will need to apply for this permit and coordinate with TDOT regarding their specific requirements for this entrance.
- 1d) The development is strongly recommended to install overhead roadway lighting to delineate the Proposed West Entrance at Maryville Pike for night or low-light travel, particularly to illuminate the RIRO configuration with the raised concrete traffic island.



**Sims Road at Proposed North Entrance:** The 2027 projected level of service calculations for this intersection indicated minimal vehicle delays and queues. Based on the capacity analysis, the intersection will be able to handle entering and exiting traffic effectively.

- 2a) It is recommended that a Stop Sign (R1-1) be installed, and a 24" white stop bar be applied to the Proposed North Entrance approach at Sims Road. The stop bar should be applied at least 4 feet from the pavement edge of Sims Road and placed at the desired stopping point that maximizes sight distance.
- 2b) Sight distances from the Proposed North Entrance at Sims Road must not be impacted by future landscaping, vegetation, construction grading, and signage. Based on a posted speed limit of 30 mph on Sims Road, the required ISD is 335 feet looking to the east and west at the Proposed North Entrance.

The distance between the Proposed North Entrance and its intersection with Maryville Pike to the west is 170 feet. The available sight distance to the west will be acceptable, as vehicles from the Maryville Pike intersection traveling east will start from a stop or at a lower turning speed, well below the posted 30 mph speed limit. Otherwise, for oncoming eastbound vehicles crossing Maryville Pike from Edington Road during a green indication, the view will be adequate, as a minimum visual sight distance of 350 feet to the west was estimated. The sight distance to the east was visually estimated to be 375 feet, at least to the at-grade railroad crossing on Sims Road.

- 2c) Based on the analyses and calculations, a single exiting lane at the Proposed North Entrance at Sims Road will be adequate. Separate entering lanes for the development on Sims Road will not be necessary due to the low expected traffic volumes.
- 2d) The development should consider installing overhead roadway lighting to delineate the Proposed North Entrance at Sims Road for nighttime or low-light conditions.



**Maryville Pike at Sims Road and Edington Road:** As described previously, this intersection operates with a single lane on each approach without dedicated left-turn green times and, as such, suffers undue delay when left-turn movements face significant oncoming traffic.

- 3a) Due to operational issues caused by single lanes and the availability of adjacent large paved areas, cut-through traffic and other illegal vehicle movements are regularly observed. Furthermore, this section of Maryville Pike was observed to have a fair amount of pedestrian and bicycle activity in and around the signalized intersection and has been identified by the City of Knoxville as being in Tier 2 of its High Injury Network (HIN). Due to existing operational and safety issues, it is recommended that the City of Knoxville and TDOT make improvements to this intersection when funding becomes available.

Improvements that would significantly increase vehicular flow and safety include providing separate turn lanes at the intersection. The greatest need for additional lanes at this intersection is for separate left-turn lanes on the northbound and southbound approaches on Maryville Pike. Providing turn lanes would necessitate signal timing and traffic signal upgrades, including new signal heads. In addition, because of the attractiveness of cutting through the Green Grocery parking lot, providing a separate lane for eastbound right-turn movement from Edington Road onto southbound Maryville Pike would be very beneficial.

Besides providing additional lanes, other improvements at the intersection should include providing increased road lighting and improved pavement marking visibility. Pavement marking visibility, however, is expected to be addressed in the interim, as this section of Maryville Pike is included in TDOT's "3-year Pavement Program" resurfacing list for fiscal year 2026. Pedestrian sidewalks identified in the City of Knoxville's 2020 "Sidewalk Study" should also be pursued, along with other enhancements, such as crosswalks, pedestrian buttons, and pedestrian signal heads at the traffic signal.

Overall, in the projected 2027 conditions at the signalized intersection of Maryville Pike at Sims Road and Edington Road, the proposed Maryville Pike Gas Station trip generation is expected to have only a minimal impact on the intersection. The proposed development is not expected to affect the traffic signal's operations unduly and will not require modifications in the short term.

In the interim, until the intersection can be reconstructed with additional turning lanes, a few other options are potentially viable at a lower cost and within a shorter timeframe to reduce the number of failed cycles, cut-through traffic, and unsafe maneuvers. These include the following:

- i) Vehicle detection: install loop or video detectors on all approaches to enable true actuated operation. Detection would allow any unused green time on Sims Road and Edington Road to gap out early, shortening the cycle and reducing vehicle delays on the busier north-south approaches on Maryville Pike.
- ii) Optimization of timing/time-of-day (TOD) plans: since the traffic signal currently operates in isolated, max-recall mode without any scheduling, introducing TOD plans would allow custom timings for peak vs. off-peak periods, reducing unnecessary green time on the lower-volume east-west approaches of Sims Road and Edington Road during peaks and thereby minimizing northbound vehicle delays that encourage cut-through movements occurring via the Green Grocery parking lot.
- iii) Separate Left-Turn Phase: it would be feasible to introduce a leading protected left-turn phase for northbound movements on Maryville Pike without a dedicated new turn lane, using protected-permissive left-turn phasing for the existing shared lane. The northbound approach has been shown to be the most critical movement due to its heavier volumes and long queues, particularly in the AM peak. Providing this phasing would give northbound left-turn movements a leading green arrow before the south thru phase, thus reducing blocking by allowing lefts to turn unopposed initially. This modification would require signal head upgrades (to a 5-section head) to allow the display of a green arrow, and ensure any phasing avoids a "yellow trap". If this modification is implemented for the northbound approach, a Left Turn Yield on Green Sign (R10-12) should be installed.



A preliminary modified signal timing plan was developed in the Synchro traffic software, incorporating a northbound leading left-turn phase with a concurrent green indication for northbound thru (and right-turn) movements before the southbound movements on Maryville Pike. The results of this signal modification reduced vehicle delays on the northbound approach and would no doubt reduce the number of cut-through vehicles on the Green Grocery property from the south towards the west, and would also, optimistically, reduce the number of illegal

passing movements. However, this modification, which provides additional green time for northbound movements, will negatively impact the other approaches by slightly increasing their vehicle delays. Unfortunately, increasing vehicle delays on the Edington Road approach would likely increase the potential for cut-through traffic from the west towards the south, as is currently occurring, unless and until a separate right-turn lane is provided at the signal.

Capacity analyses were conducted to determine the projected LOS with the development traffic in 2027, including a potential leading northbound left-turn phase. The intersection capacity results for the projected 2027 peak hour traffic are shown in Table 13, and Appendix E includes the capacity analysis worksheets from the software.

**TABLE 13**  
**INTERSECTION CAPACITY ANALYSIS RESULTS -**  
**2027 PROJECTED TRAFFIC CONDITIONS WITH THE PROJECT**  
**WITH RIRO ALTERNATIVE & LEADING NORTHBOUND LEFT TURN MOVEMENT**

INTERSECTION	TRAFFIC CONTROL	APPROACH/ MOVEMENT	AM PEAK			PM PEAK		
			LOS <sup>a</sup>	DELAY <sup>b</sup> (seconds)	Max v/c <sup>c</sup>	LOS <sup>a</sup>	DELAY <sup>b</sup> (seconds)	Max v/c <sup>c</sup>
Maryville Pike (SB & NB) at Sims Road (WB) / Edington Road (EB)	 Signalized	Eastbound	C	29.2		D	45.0	
		Westbound	C	27.7		B	18.1	
		Northbound	A	6.6		B	15.5	
		Southbound	A	16.1		B	29.4	
		Summary	B	13.8	0.63	C	26.8	0.86

Note: All analyses were calculated in Synchro 12 software and reported with HCM 7<sup>th</sup> Edition methodology

<sup>a</sup> Level of Service, <sup>b</sup> Average Delay (sec/vehicle), <sup>c</sup> Volume-to-Capacity Ratio

As shown in Table 13, the signalized intersection with the alternative phasing and timing reduces vehicle delays on the northbound approach compared to Table 11. However, it does increase vehicle delays on the other approaches to accommodate a leading northbound left-turn phase.

- 3b) With observed pedestrian and bicyclist activity in the area, the record of crashes occurring with these non-vehicular activities, and the potential for this to be increased with a new convenience market, it is recommended that the City install pedestrian warning signage on Sims Road, Edington Road, and the northbound approach of Maryville Pike in advance of the signalized intersection. A Pedestrian Sign (W11-2) is recommended to be installed on these approaches, a minimum of 125 feet from the intersection, and to

W11-2



match the sign currently posted for the southbound approach of Maryville Pike in advance of the traffic signal.

- 3c) Controlling or eliminating the cut-through traffic occurring on the Green Grocery property will be difficult with enforcement or better-regulated and defined entrances on Edington Road and Maryville Pike. Ultimately, this is private property and would require cooperation between the City and the property owner to reduce these movements, which can be detrimental to customer safety.



**Maryville Pike Gas Station Internal Drives/Parking Lot Aisleways:** The current site plan shows two entrances for the proposed development site, with pavement areas designated to facilitate customer, employee, food, and gasoline deliveries.

- 4a) All drainage grates and covers for the commercial development must be pedestrian and bicycle-safe.
- 4b) Internal sidewalks are proposed along the front and side of the convenience market. Sidewalks should include ADA-compliant ramps with detectable surfaces, and internal sidewalks should be at least 5 feet wide to comply with City of Knoxville regulations.
- 4c) According to the City of Knoxville regulations, bicycle spaces must be provided for this proposed development. Per City of Knoxville standards, with 13 proposed vehicle parking spaces, four bicycle parking spaces should be provided for this development. These spaces should be designed in accordance with the regulations in Section 11.9 of the City of Knoxville's Zoning Code User's Manual.
- 4d) All internal parking space lines are to be white, except for handicapped markings. White pavement lane markings and arrows should also be considered to facilitate orderly internal vehicular movements.
- 4e) Cut-through traffic from the westbound Maryville Pike approach is not expected to occur on the Maryville Pike Gas Station property due to lower volumes towards the east on Sims Road. Still, it is recommended that the civil site engineer consider including speed humps on the internal driveways within the development to reduce internal speeds and dissuade motorists from using the site as a cut-through route. Cut-through opposite flows from Sims Road to southbound Maryville Pike will not be available due to the RIRO configuration at the Proposed West Entrance.
- 4f) The site layout should consider a designated, defined marked path from the existing KAT bus stop on Maryville Pike to the convenience market building. Installing a defined path across the property to and from the building would facilitate pedestrian or bicycle traffic to and from the transit stop.
- 4g) The existing corner of the intersection of Maryville Pike at Sims Road is occupied by copious amounts of asphalt pavement. The construction of the Maryville Pike Gas

Station should remove excess pavement outside the State's right-of-way and return it to an unpaved and grassed area to reduce undesirable vehicle movements outside the established road lanes.

- 4h) All road and intersection elements should be designed in accordance with the American Association of State Highway and Transportation Officials (AASHTO) and the Manual on Uniform Traffic Control Devices (MUTCD), as well as the City of Knoxville and TDOT specifications and guidelines, to ensure proper roadway operations.



**Existing Pavement at the Southeastern Corner of  
Signalized Intersection of Maryville Pike at Sims  
Road & Edington Road  
(Looking Northwest)**

# Public Notice and Community Engagement

Planning strives to provide community members with information about upcoming cases in a variety of ways. In addition to posting public notice signs, our agency encourages applicants to provide information and offer opportunities for dialogue related to their upcoming case(s). The contact information you provide in your application may be used for that purpose. We require applicants to acknowledge their role in this process.

## Sign Posting and Removal

The Administrative Rules and Procedures of the Knoxville-Knox County Planning Commission require a sign to be posted on the property for each application subject to consideration by the Planning Commission.

**Planning staff will post the required sign.** If a replacement sign(s) is needed, the applicant is responsible for picking up the new sign(s) from Planning and will be charged \$10 for each replacement.

### Location and Visibility

The sign must be posted on the nearest adjacent/frontage street and in a location clearly visible to vehicles traveling in either direction. If the property has more than one street frontage, the sign should be placed along the street that carries more traffic. Planning staff may recommend a preferred location for the sign to be posted at the time of application.

### Timing

The sign(s) must be posted not less than 12 days prior to the scheduled Planning Commission public hearing and must remain in place until the day after the meeting. In the case of a postponement, the sign can either remain in place or be removed and reposted not less than 12 days prior to the next Planning Commission meeting. The applicant is responsible for removing the sign after the application has been acted upon by the Planning Commission.

### Acknowledgement

**By signing below,** you acknowledge that public notice signs must be posted and visible on the property consistent with the guidelines above and between the dates listed below.

02/20/2026

03/06/2026

Date to be Posted

Date to be Removed

**Have you engaged the surrounding property owners to discuss your request?**

Yes  No

No, but I plan to prior to the Planning Commission meeting



Steve Young

1/12/2026

Applicant Signature

Applicant Name

Date