



# APPEAL OF DECISION

(Please Note: Original application and staff report are made a part of this application.)

Type:  One Year Plan Amendment  Sector Plan Amendment  Rezoning  Variance  
 Street Name Change  Right-of-Way Closure  
 Certificate of Appropriateness  Other: Appeal of 8-A-24-OB

Decision by:  Planning Staff  Planning Commission  Other: \_\_\_\_\_ Date of Decision: 8/8/24

Jurisdiction:  City \_\_\_\_\_ Council District  County \_\_\_\_\_ Commission District

Hill & Locust Partners Original File Number: 4-D-24-DT / 8-A-24-OB

Original Applicant Name: \_\_\_\_\_

Name of Owner of Subject Property: Hill & Locust Partners, Victoria Gillenwater Trust, Barbara Welchel & Zenith Properties, LLC

Description of Subject Property (Include city block and parcel number or lot number):  
Ward 6; Blocks 02106 and 02105; Parcels 094MD018, 094MD022, 094MD024, 094MD025, 094ME033, and 094ME030

Zoning map of all property within 300 feet of the subject property is attached.

### DECISION BEING APPEALED

Planning Commission Decision to approve design of new structure.

### REASON FOR THE APPEAL

Attach additional pages, if necessary.  
Design violates multiple guidelines. Please see attached correspondence

### PETITIONER INFORMATION

Name of Petitioner: **Overlook Owners Association, Inc.**

Petitioner's Interest in the Matter (Include a description of affected property owned by Petitioner):  
The Petitioner is an association of property owners on abutting property located at 608 West Hill Ave, Parcel 094MD017. The members of association are aggrieved by the Planning Commission decision.

Application Authorization: I hereby certify that I am the applicant/authorized representative for the above named petitioner.

Signature: \_\_\_\_\_ *Daniel A. Sanders*

All correspondence should be sent to: Name (Print): Daniel A. Sanders  
920 Volunteer Landing, Suite 200, Knoxville, TN 37915

Street Address \_\_\_\_\_ City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

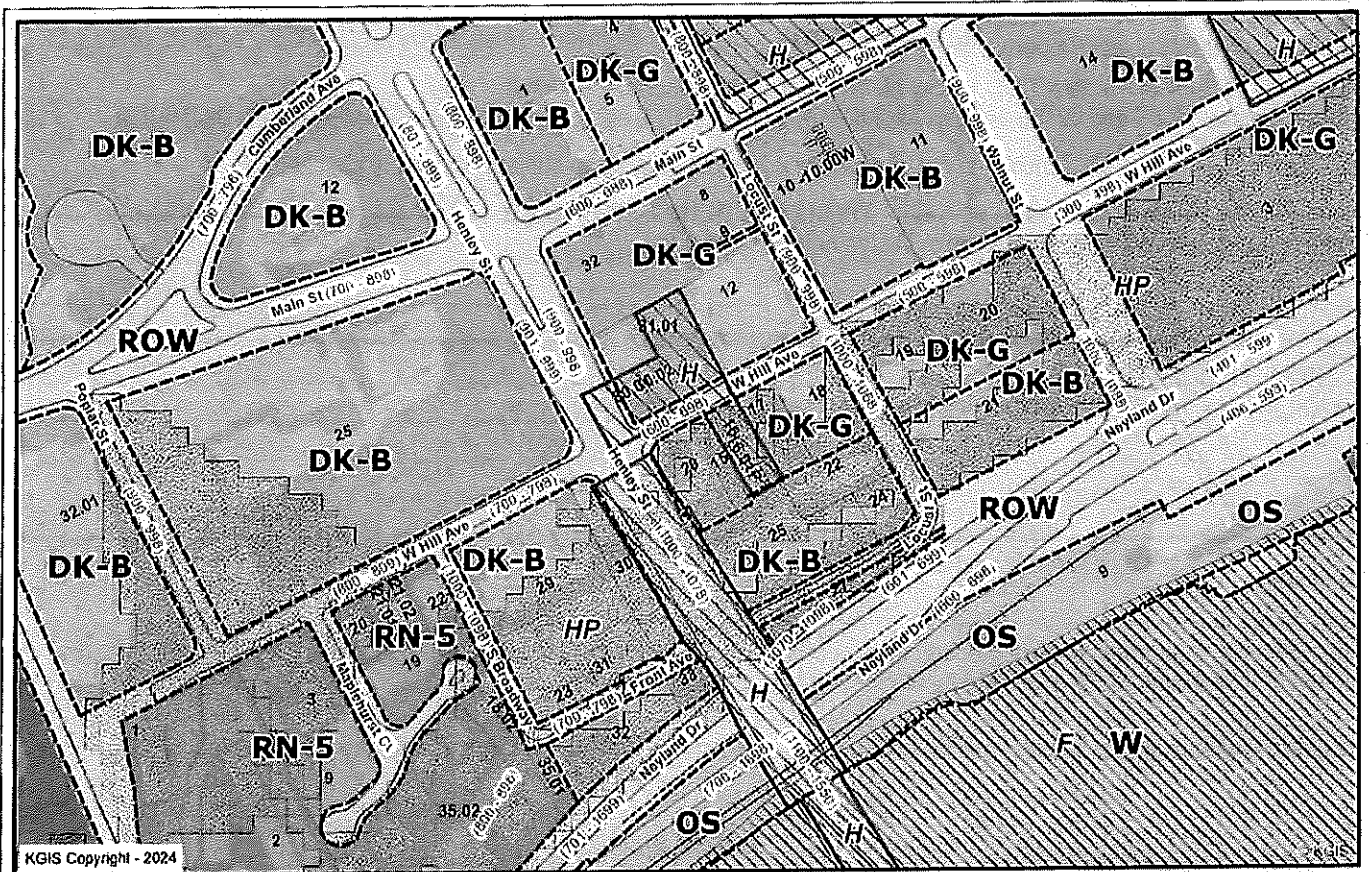
Phone: (865) 316-9626 Fax: \_\_\_\_\_ E-mail: das@lyblaw.net

#### For Planning Staff Use Only

Application Accepted by Planning Staff Member: *Shelley Gray*  
Appeal Fee Amount: *\$ 500.00* Date Appeal Received: *08/22/2024*

#### BODY WHO WILL HEAR THE APPEAL & MEETING DATE OF THE APPEAL

<input checked="" type="checkbox"/> City Council - 6 p.m. <i>Sept. 17 2024</i> Month • Date • Year	<input type="checkbox"/> County Commission - 7 p.m. _____ Month • Date • Year	<input type="checkbox"/> City BZA - 4 p.m. _____ Month • Date • Year	<input type="checkbox"/> Planning Commission - 1:30 p.m. _____ Month • Date • Year
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### Zoning Map

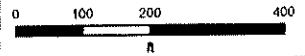
Appeal of 4-D-24-DT

All property within 300 feet of the subject property.

Knoxville - Knox County - KUB Geographic Information System



Printed: 6/26/2024 at 10:15:47 AM



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[www.lyblaw.net](http://www.lyblaw.net)

Direct: (865) 316-9626  
Email: [das@lyblaw.net](mailto:das@lyblaw.net)

August 22, 2024

Knoxville City Council  
c/o Will Johnson, City Recorder  
400 Main Street, Room 467  
Knoxville, TN 37902

**Re:** Appeal of 8-A-24-OB  
Proposed New Parking Garage and Residential Structure  
Fronting Hill Avenue, Locust Street, and Front Avenue

Dear City Council:

I am writing on behalf of the Overlook Homeowners Association to formally appeal the Planning Commission's decision rendered on August 8, 2024, regarding the proposed parking garage and apartment building project by Hill & Locust Partners at Hill Avenue and Locust Street. This project consists of two structures on separate lots. Our appeal is based on multiple violations of the guidelines. The project poses significant risks to the continued growth and revitalization of our city.

City Council approved the Downtown Design Guidelines on March 27, 2007, following a lengthy public process that included multiple public meetings involving a wide variety of stakeholders. This process was motivated by a community desire to hold downtown development to a standard that protected the City's historic resources and ensured that past mistakes were not repeated. While the guidelines are meant to provide some measure of flexibility they were never intended to be ignored, stretched, and massaged to enable projects because proposed developments are intended to meet other policy goals.

Knoxville's street fronts are essential to defining the city's character, vibrancy, and economic vitality. These areas serve as critical interfaces for commerce, culture, and community interaction. Allowing parking garages to dominate these frontages squanders their potential, replacing lively, pedestrian-friendly spaces with dull, uninviting facades that create a potentially dangerous situation for pedestrians, bicyclists, and other vehicles. The current proposal for a parking garage on Front Avenue directly contradicts Knoxville's design vision, threatening to create a pedestrian dead zone and derailing efforts to reconnect downtown with the Tennessee River. Preserving and enhancing our street fronts is crucial for maintaining a walkable urban environment that reflects Knoxville's unique charm and supports sustainable growth. Drawing inspiration from successful examples like Chattanooga, Knoxville should focus on transforming Neyland Drive and Front Avenue into a boulevard that seamlessly integrates downtown with the river. Front Avenue, being parallel to and feeding into Neyland Drive, directly impacts that transformation.

The project site is a gateway to our city, located adjacent to the historic Henley Street Bridge on the Tennessee River, with 32,000 vehicles passing daily. This prominent entryway to

Knoxville warrants a development that strictly adheres to our design guidelines, ensuring the city remains on a path toward sustainable growth, architectural excellence, and increased community appeal. The City has invested multiple tens of millions of dollars in its waterfront. The public process that placed the rear portion in the Boulevard District was intentional in its goal of preserving the character of the waterfront consistent with limits that prevent building on either side of the river from walling off the river from the City.

The proposed project violates several key guidelines, including:

#### 1. Violation of Knoxville Downtown Design Guidelines:

- **Guideline (A)(3)(a) and (b):** The project places a parking garage on the street frontage, contrary to the guidelines that require garages to be either underground or lined with retail, office, or residential space. This failure to engage pedestrians undermines the vitality of the street and contradicts the vision for a walkable, dynamic downtown.
- **Guideline (A)(3)(g):** Access to the proposed new parking garage limits options for new development of contiguous or adjoining space.
- **Guideline (B)(1)(a), (b), (c), (d), and (e):** The building lacks human or pedestrian scale along Front Avenue, presenting a blank, monolithic parking garage rather than a dynamic streetscape. The structure fails to create the "outdoor room" atmosphere envisioned in the guidelines, detracting from the pedestrian experience.
- **Guideline (B)(2)(c):** The proposed garage obstructs sight lines to the historic Henley Street Bridge. Despite modifications to the project renderings, the design continues to inaccurately represent the extent of this obstruction, which would have a detrimental impact on the visual connectivity of downtown with the riverfront.
- **Guideline (B)(2)(e):** The project does not include pedestrian-friendly plazas or amenities, further diminishing its potential to contribute to the vibrancy of the downtown area.

#### 2. Failure to Adhere to Boulevard District Guidelines:

- **Guideline (A)(1)(a):** The design lacks a landscaped area separating the building from the sidewalk, a critical feature for enhancing pedestrian appeal and maintaining a consistent streetscape.
- **Guideline (A)(1)(c):** The architecture and landscaping do not complement the adjoining properties or buildings, resulting in a development that is out of character with the surrounding area. **Guideline (A)(1)(d):** The design does not allow for a plaza or similar quasi-public spaces in a portion of the private open space.
- **Guideline (A)(2)(a):** The design does not clearly orient a pedestrian entrance to Front Avenue, failing to invite pedestrian interaction and engagement.
- **Guideline (A)(2)(b):** The design is not complementary to the mass of adjacent buildings. In particular, the rear 18-story structure in no way complements the two-to six-story buildings it directly abuts. The argument that "adjacent" structures to be considered for the rear building are the Bank of America Building on Main, Neyland Stadium, the First Tennessee Tower and the City County Building strain the definition of adjacency beyond any reasonable definition. Yet, these are the examples cited to justify such mass. These are neither adjoining nor separated only by a street, stream or alley.

- **Guideline (A)(2)(d):** The parking garage is not adequately screened from Henley Street, Front Avenue and Neyland Drive, leaving an unsightly and obtrusive structure that detracts from the aesthetic and functional quality of the area.
3. **Traffic Safety Concerns:** The project will significantly and negatively impact traffic safety at the intersection of Locust Street and Front Avenue. The current building design does not allow space for a necessary intersection redesign to accommodate increased traffic and public safety. The project design should not be approved until it can adequately address and accommodate these critical safety issues.

I am enclosing a presentation that provides detailed support for the issues outlined in this letter. The first section of the presentation tracks the evolution of the developer's renderings throughout the project. It offers a critical analysis, provided by our design review team consisting of professional architects, of how these renderings have changed and why we believe they continue to misrepresent the true impact of the development, particularly in relation to obstructing sight lines to the historic Henley Street Bridge.

The second section of the presentation outlines the specific Downtown Design Guidelines that the proposed project violates. Each guideline is analyzed in the context of the current proposal, with visual examples to illustrate how the project falls short of these standards.

The third section presents comparative renderings that juxtapose the proposed development with other buildings in the downtown area. This analysis emphasizes the stark contrast between the proposed structure and the established architectural character of Knoxville, underscoring the importance of maintaining a cohesive and pedestrian-friendly streetscape. It also reveals the precedent such a project will set for future downtown developments, further undermining and possibly erasing the downtown fabric of Knoxville set forth in the Knoxville Downtown Design Guidelines.

Finally, the presentation focuses on the forthcoming University of Tennessee Entertainment District, highlighting its potential to serve as a vibrant extension of downtown Knoxville. We urge the Council to exercise foresight by encouraging a design for the Hill Avenue and Locust Street project that fosters a seamless connection between downtown and this new district that continues East along the Riverfront to Historic Preservation Drive. The vision for this connection is the very reason why this property is zoned in the Boulevard district—intended to transform Neyland Drive and Front Avenue from a mere downtown bypass into an engaging boulevard that enriches the urban fabric of Knoxville and connects Downtown to the University and the neighborhoods East of Downtown (East Knoxville and Riverhill Gateway) along the Tennessee Riverfront.

We invite you, as Council members, to walk the full length of Front Avenue, note its actual location in relation to the Henley Street bridge, and imagine the potential for the underdeveloped land and aging buildings on all sides of this project's location. Front Avenue is the street that ties the Maplehurst neighborhood to Downtown and to the river. There is tremendous potential to create an engaging, pedestrian-friendly streetscape for both Front Avenue and Neyland Drive. This is what the Design Guidelines require. The Guidelines are there to make sure every building downtown positively impacts its surrounding neighborhood and ensure underdeveloped land becomes something better. As designed, this building will negatively impact its surrounding neighborhood, stifle future development, set a terrible precedent for future projects downtown, and damage the image of Knoxville, as a whole.

**Conclusion:**

We urge the City Council to uphold the standards set forth in the Downtown Design Guidelines and Boulevard District Guidelines by denying this project as currently proposed.

Knoxville's growth and revitalization depend on developments that enhance the city's unique charm and promote long-term economic vitality. The proposed project falls short of these goals and risks undermining the progress our city has made.

Knoxville deserves better. Let's work together to ensure that future developments align with our shared vision for a vibrant, connected, and pedestrian-friendly downtown.

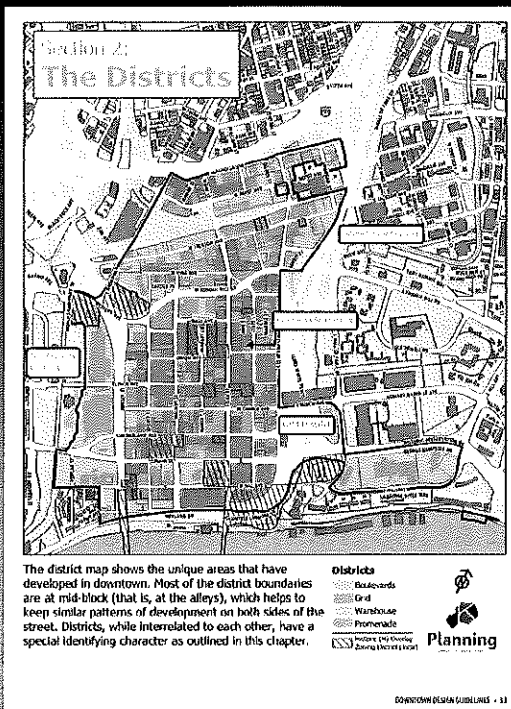
Thank you for your attention to this important matter. Please feel free to contact me if you have any questions or require additional information.

Very truly yours,

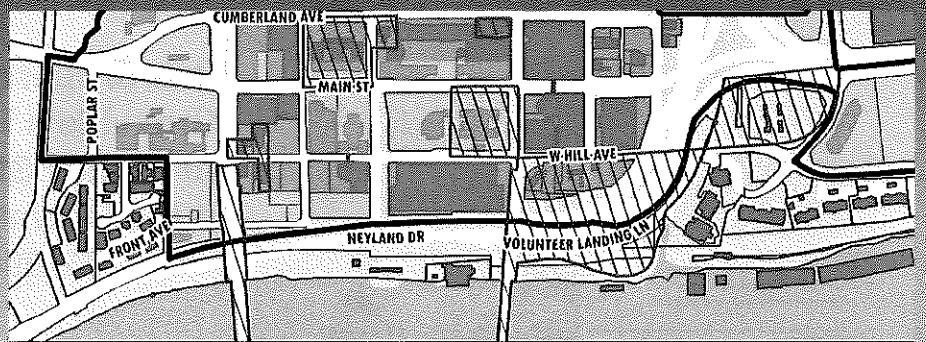
A handwritten signature in black ink, appearing to read "Daniel A. Sanders". The signature is fluid and cursive, with a prominent initial "D".

Daniel A. Sanders

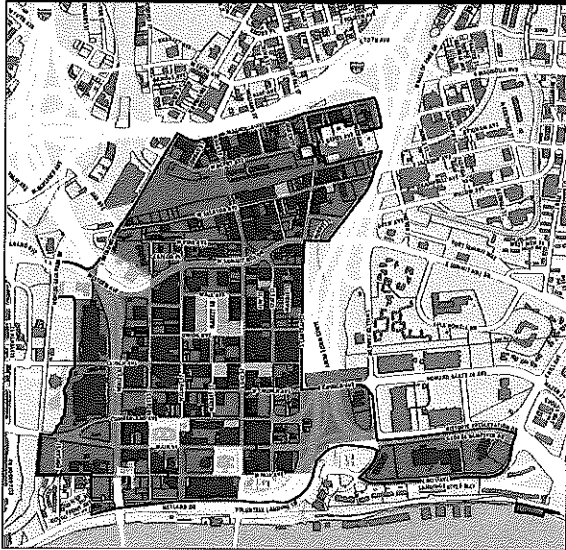
cc: Clients  
Indya Kincannon, Mayor of the City of Knoxville






## DK-G and DK-B base Zoning



MAP 1: Downtown Design Guidelines Boundary



**Map Information**

-  Downtown Design Guidelines Boundary
-  Downtown Design Guidelines Area
-  Historic (H) Overlay Zoning District (local)\*

Official maps are available for inspection at Municipal and Planning Department Map Room. The zoning laws do not constitute a warranty. Planning Department of City Council. Therefore, users of this map should be aware that any changes that have been approved by City Council may not be reflected here. Maps of zoning with Knoxville Area County Planning Staff at 1053 21st Street.

Scale: 1:9,600

0 500 1,000 1,500 Feet



**Planning**  
CITY OF KNOXVILLE

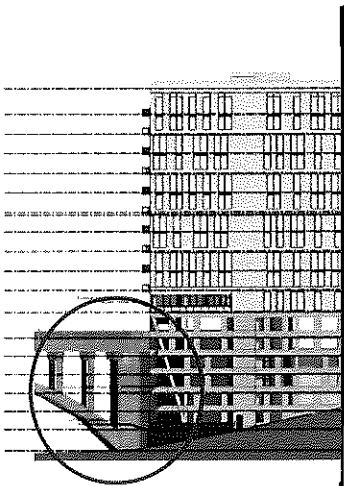
\*Any local historic overlay areas within these Design Guidelines Boundary are not subject to these Design Guidelines.

The Knoxville Downtown Design Guidelines supplement base zoning and increase the requirements for development within the Design Guidelines Boundary.

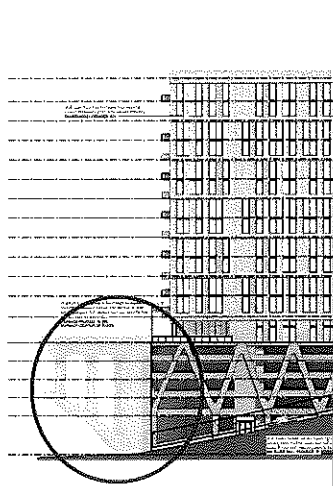
This property forms a part of Knoxville's Riverfront and is situated on Henley Street at the main entrance to the Downtown Knoxville Design Area – 32,000+ trips per day (2023)



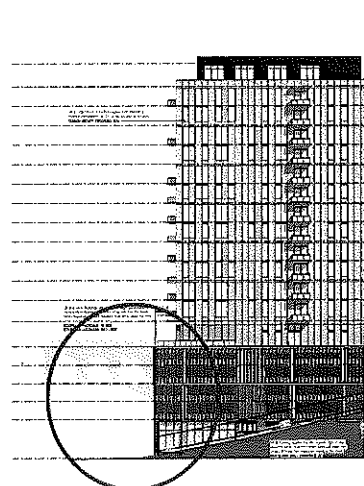
# *How is the Henley Street Bridge really impacted?*



April 17th



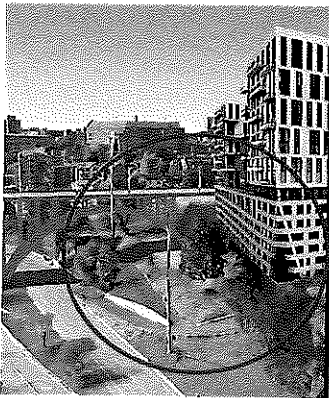
May 15th



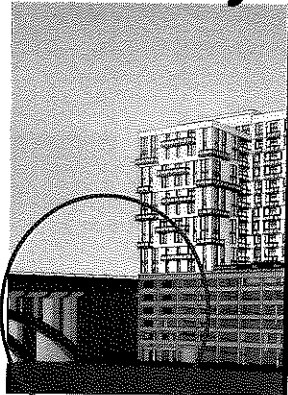
June 20th

Elevations and renderings were provided from applicant to the Design Review Board and Knox News Sentinel.

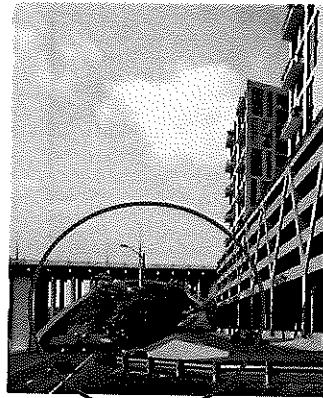
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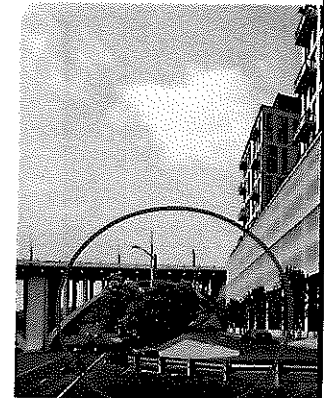
April 17th



May 1st

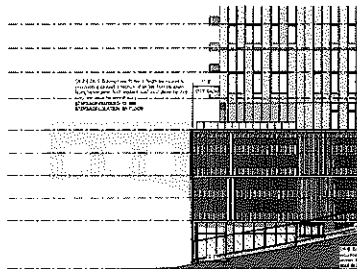


May 15th



June 20th

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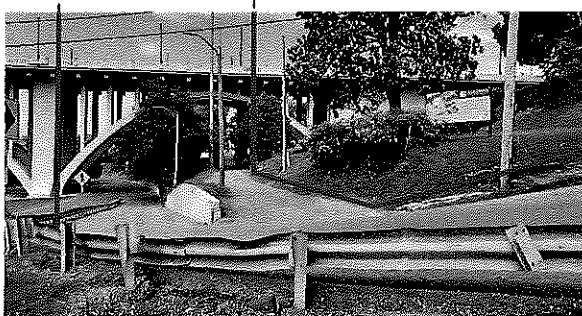


Are the renderings accurate?



### RENDERING

This image has been significantly modified to appear to not block the bridge.



### ACTUAL PHOTO

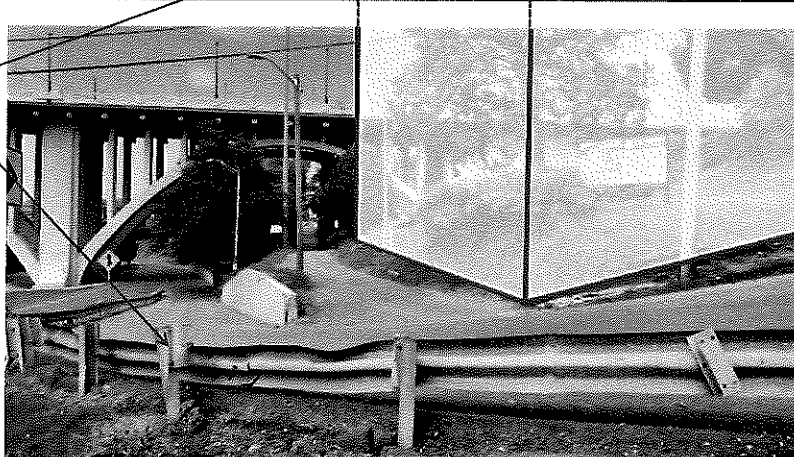
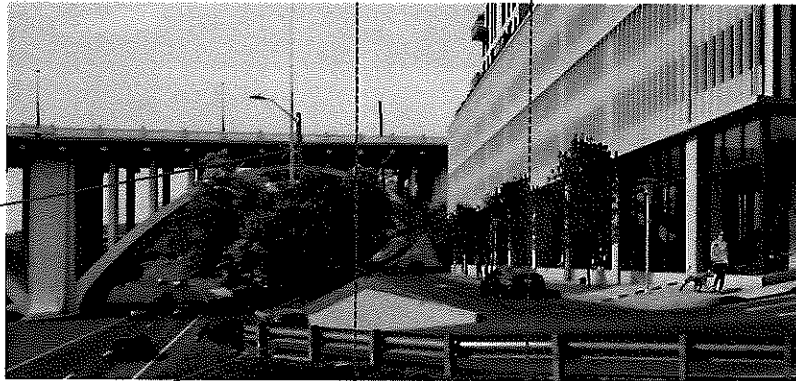
The Historic Henley Street Bridge will be obstructed by this building

The applicant provided a composite photo realistic rendering that misrepresents the location of the building to confuse the Design Review Board.

The above rendering and photo below are properly aligned. Note the relationship of the light pole, power pole, and bridge support in the center of the images.

Note the condition of the guardrail and the retaining wall. The above image is not a real photo – it is a false rendering showing false existing site conditions.

Neyland Drive, Front Avenue and all existing site elements in the applicant provided rendering have been manipulated relative to the bridge to intentionally give the false impression that the building will not block views of the Henley Street Bridge.



## FALSE RENDERING

This image has been significantly modified to appear to not block the bridge.

## ACTUAL PHOTO

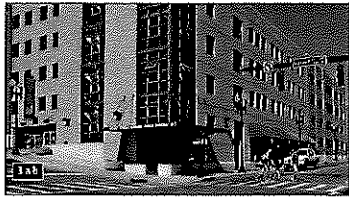
The Historic Henley Street Bridge will be obstructed by this proposal.

Photo matched comparison is based on location in rendering image. Alignment and location of building placement is based on Henley Street Bridge in background.

## A. PUBLIC REALM

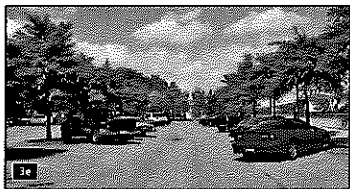
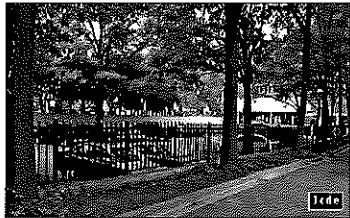
### 3. PARKING FACILITIES

It is important to ensure that parking facilities (both public and private) are safe, accessible and clearly marked. New parking facilities should be designed to be attractive, compatible additions to downtown. In general, new parking facilities should remain subordinate to the street scene.



#### GUIDELINES:

- 3a. Create parking garages that do not contain blank walls. Allow for future commercial uses that may not be feasible at the time of construction.
- 3b. Locate parking garages under structures, or provide for retail, residential or office uses that line the garage. Corner locations are preferable for commercial uses.
- 3c. Locate surface parking lots in the side or rear of buildings. No surface parking lots should be created in front of buildings.
- 3d. Screen surface lots, where they abut a public sidewalk, with decorative walls, fencing and landscaping.
- 3e. Distribute shade trees within surface lots at a ratio of 1 tree per 8 parking spaces. Trees may be planted in wells between spaces.
- 3f. Provide pedestrian-scale lighting (10-15 feet in height) that uniformly illuminates the lot.
- 3g. Access to parking garages should not limit options for future development of contiguous or adjoining space, especially on corners.



DOWNTOWN DESIGN GUIDELINES - 7

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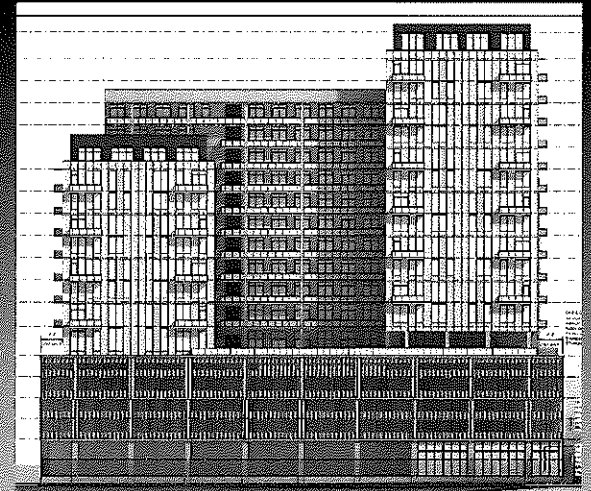
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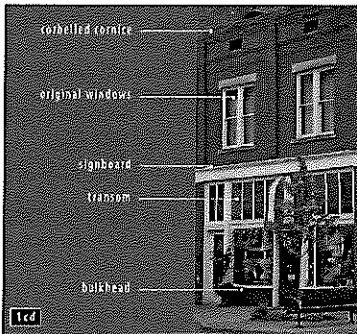
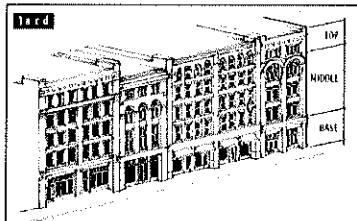
## B. PRIVATE REALM

### 1. BUILDING MASS, SCALE AND FORM

Building form should be consistent with the character of downtown as an urban setting and should reinforce the pedestrian activity at the street level. Creating pedestrian-scale buildings, especially at street level, can reduce the perceived mass of buildings. Historically, building technology limited height and subsequently created pedestrian-scaled buildings typically less than 10 stories. Building technology no longer limits the height of buildings, however, there is still a need for buildings that respond to pedestrians. The use of human-scale design elements is necessary to accomplish this. Human-scale design elements are details and shapes that are sized to be proportional to the human body, such as, upper story setbacks, covered entries, and window size and placement.

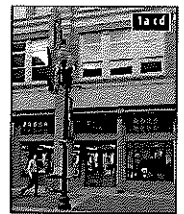
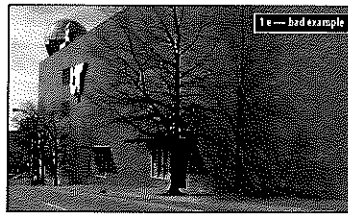
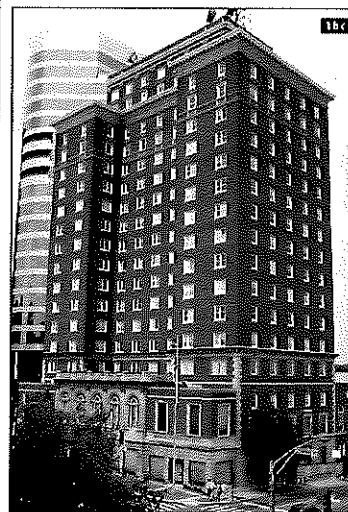
#### GUIDELINES:

- 1a. Maintain a pedestrian-scaled environment from block to block.
- 1b. Foster air circulation and sunlight penetration around new buildings. Buildings may be designed with open space, as allowed under existing DK zoning; or buildings may be 'stepped back' on upper floors with lower floors meeting the sidewalk edge.
- 1c. Use building materials, cornice lines, signs, and awnings of a human scale in order to reduce the mass of buildings as experienced at the street level.
- 1d. Divide larger buildings into 'modules' that are similar in scale to traditional downtown buildings. Buildings should be designed with a recognizable base, middle, and top on all exposed elevations.
- 1e. Avoid blank walls along street-facing elevations.



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## A. THE BOULEVARD DISTRICT

### 1. YARDS/SETBACKS

Create yards that compliment the green space of adjacent buildings.

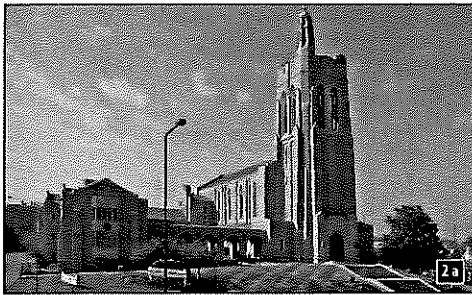


#### GUIDELINES:

- 1a. Separate new buildings from the sidewalk with lawn or other landscaped area.
- 1b. Plant native or naturalized trees and other landscape materials in the open spaces.
- 1c. Compliment the architecture and landscaping of adjoining property.
- 1d. Allow for plazas or similar quasi-public spaces in a portion of these private open spaces.

### 2. BUILDING CONSIDERATIONS

Enhance the architectural harmony of all buildings along the street.



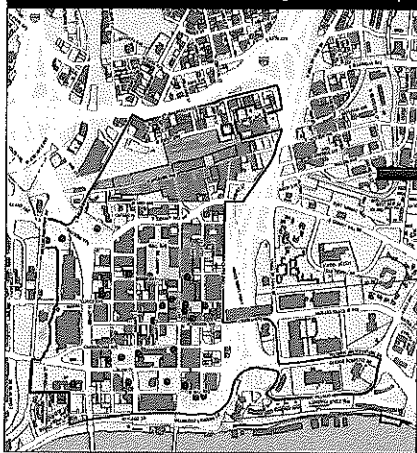
#### GUIDELINES:

- 2a. Design building entrances to be clearly oriented to the street.
- 2b. Encourage building forms that are complimentary to the mass of adjacent buildings.
- 2c. Design building elevations to compliment the buildings along the side or back streets when buildings are to face more than one street.
- 2d. Screen service facilities or incorporate them into the design of new buildings so that they are not obtrusive.

### 3. RECOMMENDED SIGNS



MAP 2: Historic Designations in the Downtown Design Guidelines Boundary

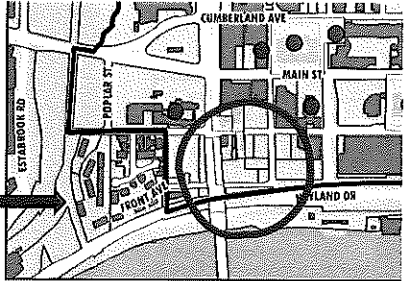


**Map Information**  
 [Symbol] Design Review Board Boundary  
 [Symbol] Listed National Register Site  
 [Symbol] National Register Historic District  
 [Symbol] Historic (H) Overlay Zoning District (local)\*  
 \*This is a preliminary map until the design review board meeting on 04/14/2014.  
 © 2014 Planning Department

Scale: 1:6,000  
 0 500 1000 1500 Feet



22 • DOWNTOWN DESIGN GUIDELINES



**B. PRIVATE PLAZA**

**2. BUILDING QUALITY**

It is important to maintain a strong relationship among buildings, sidewalks, and streets. This is typically achieved through consistent setbacks that locate buildings in the same way.



**GUIDELINES:**

- 2a. Set building back five feet to provide wider sidewalk space when new construction in non-historic areas is to be more than half the length of the block.
- 2b. Consider using landscape elements to define the sidewalk edge where a building is to be set back from the sidewalk.
- 2c. Maintain sight lines to historic buildings that were originally located in an open setting, providing setbacks for new buildings next to historic structures in order to preserve views.
- 2d. Limit grade separations above or below the sidewalk, generally no more than 3 feet. Allow for clear sightlines into and out of buildings and plazas.
- 2e. Design private plazas to be pedestrian-friendly. Provide human-scale amenities and include landscaping.

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- 2e. Design private plazas to be pedestrian-friendly. Provide human-scale amenities and include landscaping.

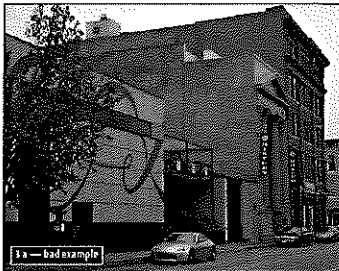
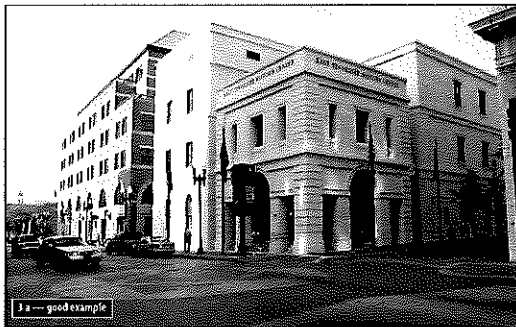
1471 FRONT ST



## B. PRIVATE REALM

### 3. BUILDING MATERIALS

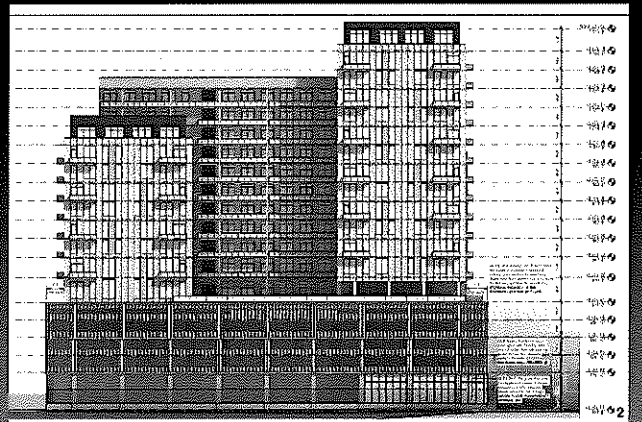
New building materials should relate to the scale, durability, color, and texture of the predominant building materials in the area.

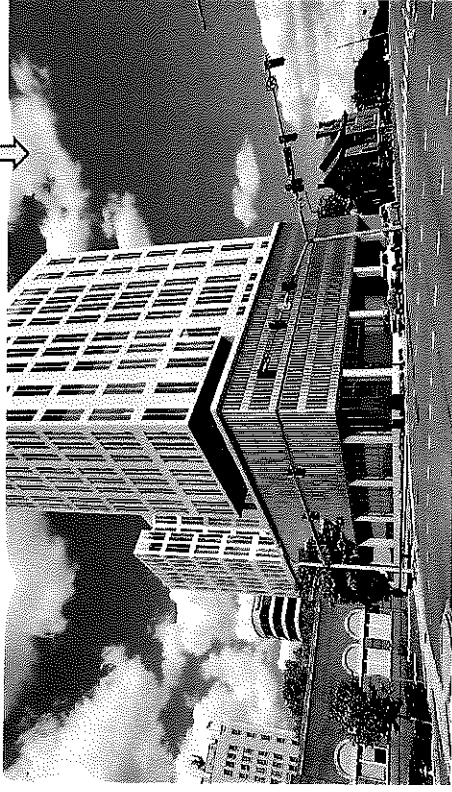
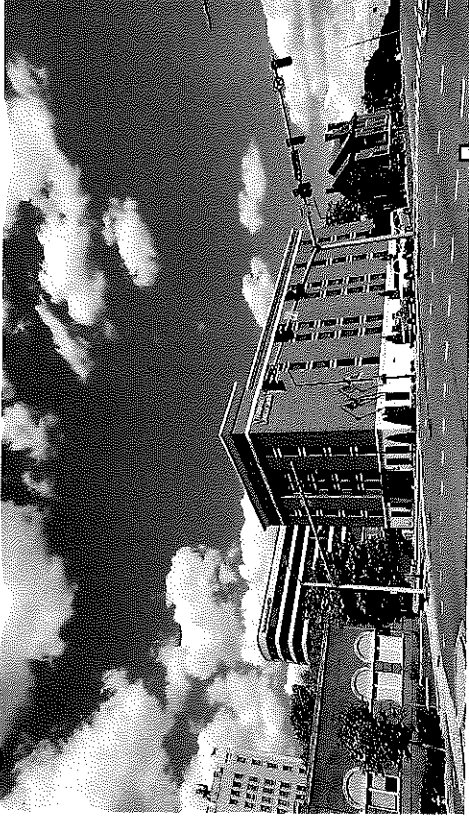


#### GUIDELINES:

- 3a. Use complementary materials and elements, especially next to historic buildings.
- 3b. Rehabilitate historic structures in accordance with the Secretary of Interior's Standards (see Appendix A).

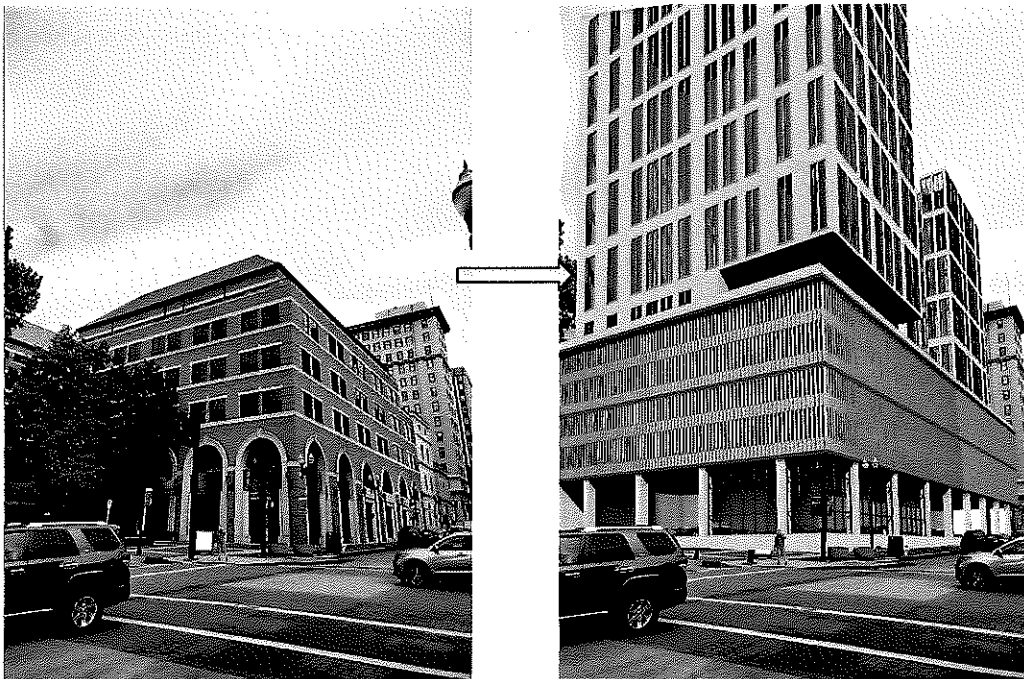
DOWNTOWN GUIDELINES • 11



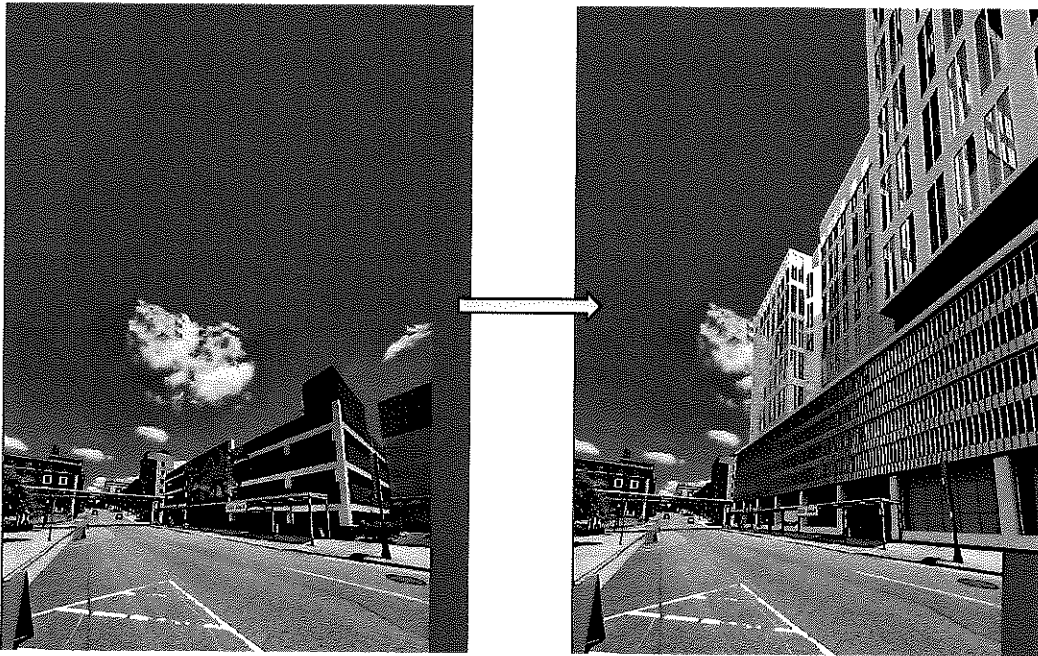


Almost 20 years ago the Hampton Inn was designed as a garage with a hotel on top. Knoxville stepped in and did the right thing to make sure our city's streetscape was protected. This set a precedent for quality development downtown and was the catalyst for the Design Review Board.

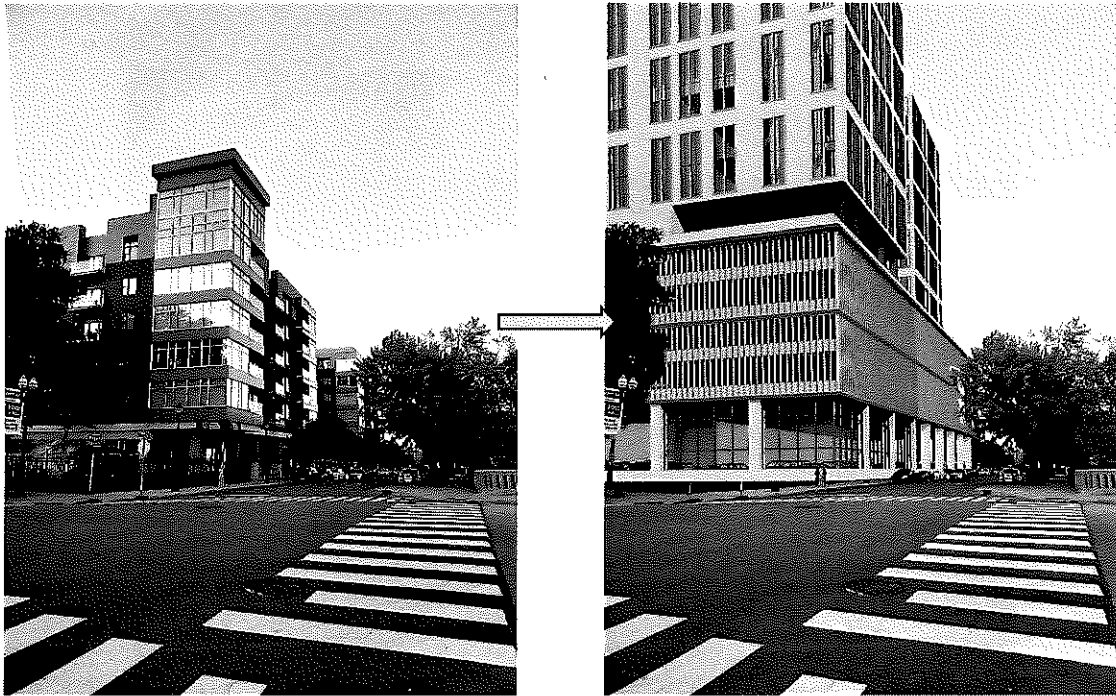
Shouldn't this project be held to the same standard?



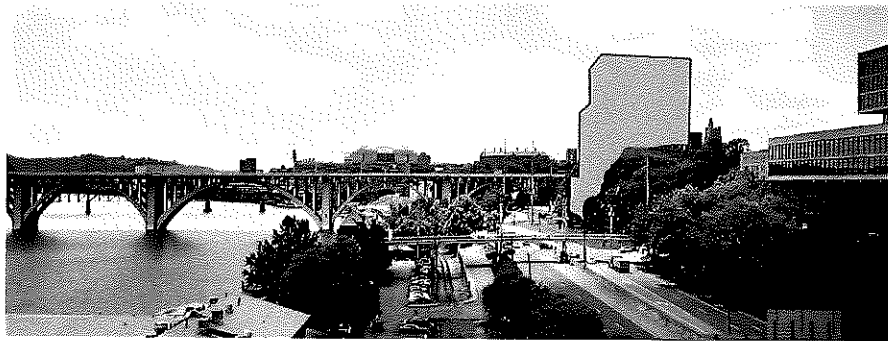
The corner of Gay and Church is enriched by a courtyard, landscaping, and public art. Shouldn't this project be held to the same standard?



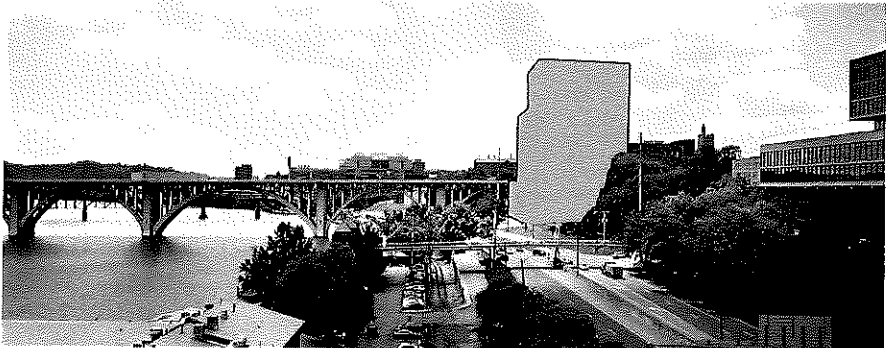
This is an old, existing garage that the owner has painted a mural on to reduce its negative impact on the pedestrian experience. That is a good attempt to repair poorly developed downtown property. But it should this be used as precedent for current development? No.



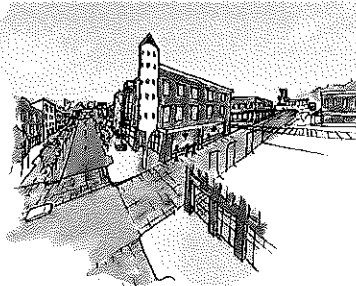
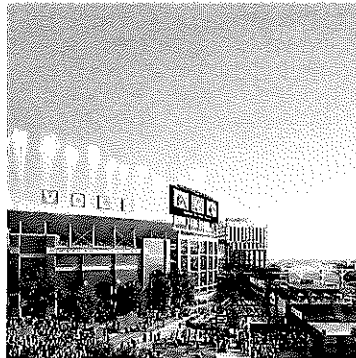
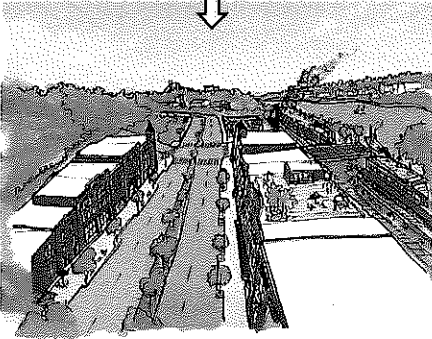
Marble Alley was a catalytic project that transformed the north side of Downtown. Shouldn't this project be held to the same standard?



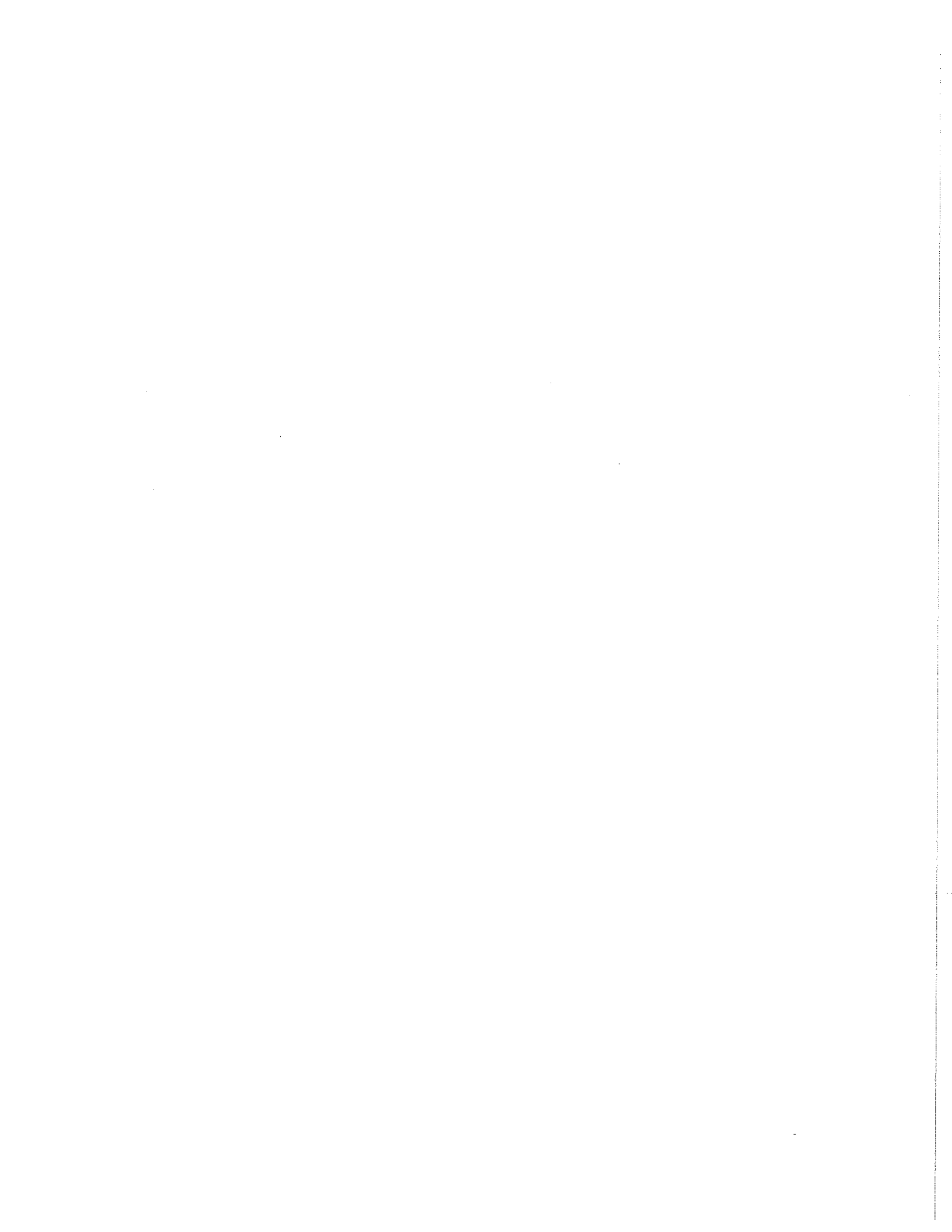
Location of building as shown in applicant provided rendering



Location of building as shown in applicant provided drawings



UT's new entertainment district will engage the streetscape of Neyland Drive and help it to transform into part of Downtown and the University. That is why this property is zoned in the Boulevard district – to help Neyland Drive transform from a downtown bypass into an engaging boulevard.





## MEMORANDUM

**TO:** Knoxville-Knox County Planning Commission  
**FROM:** Lindsay Crockett, AICP, Principal Planner/Design Review Program Manager  
**DATE:** July 31, 2024  
**SUBJECT:** Consideration of an appeal of the Design Review Board approval of 4-D-24-DT  
**FILE #:** 8-A-24-0B; Agenda Item #5

8-A-24-0B is an appeal of the Design Review Board’s approval of a proposed new construction building at 0 W. Hill Avenue ([4-D-24-DT](#)), filed by an attorney on behalf of “an association of property owners on abutting property located at 608 W. Hill Avenue,” stating that “the design violates multiple guidelines.”

Article 16.5.F states that appeals of Design Review Board decisions may be filed in accordance with Article 16.12. In accordance with Article 16.12.A.4.b, the Knoxville-Knox County Planning Commission must evaluate the application based upon the evidence presented at the public hearing. The Commission may affirm, modify, impose restrictions, or overrule the initial decision.

## BACKGROUND

The Design Review Board (DRB) is a ten-member Board appointed by the City of Knoxville Mayor with an approval of a majority of City Council. Members include one architect and one urban design professional recommended by AIA East Tennessee, two downtown residents, two business or development professionals “whose work is largely focused downtown,” a member of the Downtown Knoxville Alliance, a member of the Historic Zoning Commission, a “City of Knoxville resident knowledgeable in design and development,” and a representative of the East Tennessee Community Design Center.

Per [Article 16.5](#), Design Review is intended to foster attractive and harmonious development and rehabilitation in Downtown Knoxville. The Design Review Board’s role is not to impose any architectural preference, and the [Downtown Design Guidelines](#) are not intended to bring uniformity in design or approach or to require specific materials. The Downtown Design Guidelines are intended to be applied in a flexible manner to meet the needs of the development while encouraging the design to respect the context of nearby buildings and the streetscape, The Downtown Design Guidelines are thus not a rigid set of rules, but rather a set of key principles to guide development. The Board’s role is to provide certainty that both immediate surroundings as well as Downtown as a whole are taken into account with each building project.

At the June 20, 2024 meeting, the DRB approved Certificate [4-D-24-DT](#) subject to five conditions. The enclosed case file includes the meeting minutes, the staff report, and the application. The staff report details the relevant design guidelines alongside staff comments on the site, parking, massing and scale, the pedestrian experience, and additional design elements. The application includes site plans, elevation drawings, and renderings.





# APPEAL OF DECISION

(Please Note: Original application and staff report are made a part of this application.)

Type:  One Year Plan Amendment  Sector Plan Amendment  Rezoning  Variance  
 Street Name Change  Right-of-Way Closure  
 Certificate of Appropriateness  Other: \_\_\_\_\_

Decision by:  Planning Staff  Planning Commission  Other: \_\_\_\_\_ Design Review Board  
Date of Decision: 6/20/24

Jurisdiction:  City \_\_\_\_\_ Council District  County \_\_\_\_\_ Commission District

Original Applicant Name: 3/29/24 Original File Number: 4-D-24-DT

Name of Owner of Subject Property: Hill & Locust Partners, Victoria Gillenwater Trust, Barbara Welchel & Zenith Properties, LLC

Description of Subject Property (Include city block and parcel number or lot number): \_\_\_\_\_

Ward 6; Blocks 02106 and 02105; Parcels 094MD018, 094MD022, 094MD024, 094MD025, 094ME033, and 094ME030

Zoning map of all property within 300 feet of the subject property is attached.

### DECISION BEING APPEALED

Design Review Board decision to approve certificate of appropriateness.

### REASON FOR THE APPEAL

Attach additional pages, if necessary. \_\_\_\_\_  
Design violates multiple guidelines.

### PETITIONER INFORMATION

Name of Petitioner: The Overlook Owners Association, Inc.

Petitioner's Interest in the Matter (Include a description of affected property owned by Petitioner): \_\_\_\_\_

The Petitioner is an association of property owners on abutting property located at 608 West Hill Ave, Parcel 094MD017. The members of the association are aggrieved by the decision of the Design Review Board.

Application Authorization: I hereby certify that I am the applicant/authorized representative for the above named petitioner.

Signature: Daniel A. Sanders

All correspondence should be sent to: Name (Print): Daniel A. Sanders

Low Yeager & Brown PLLC, 920 Volunteer Landing, Suite 200, Knoxville, TN 37915

Street Address \_\_\_\_\_ City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

Phone: (865) 316-9626 Fax: \_\_\_\_\_ E-mail: DAS@lyblaw.net

### For Planning Staff Use Only

Application Accepted by Planning Staff Member: [Signature]

Appeal Fee Amount: \$ 500.00 Date Appeal Received: 6/26/24

### BODY WHO WILL HEAR THE APPEAL & MEETING DATE OF THE APPEAL

<input type="checkbox"/> City Council - 6 p.m. _____ Month • Date • Year	<input type="checkbox"/> County Commission - 7 p.m. _____ Month • Date • Year	<input type="checkbox"/> City BZA - 4 p.m. _____ Month • Date • Year	<input type="checkbox"/> Planning Commission - 1:30 p.m. <u>08/08/24</u> Month • Date • Year
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A video of this meeting will be available in the meeting archive pages here:

<https://knoxplanning.org/zoning/design-review>

<p><b>BOARD MEMBER – PRESENT</b> Perry Childress Jared Worsham Josh Wright, Chair John Thurman Rick Blackburn Laura Lenn Susanne Tarovella, Vice-Chair Matthew DeBardelaben Chris Ford Cameron Bolin</p>	<p><b>BOARD MEMBERSHIP</b> East Tennessee Community Design Center Downtown Resident Urban Design Representative Downtown Development Representative Historic Zoning Commission Representative Downtown Resident City of Knoxville Resident Representative Downtown Knoxville Alliance Representative Business Development Representative AIA Representative</p>
<p><b>BOARD MEMBER – EXCUSED</b> None</p>	<p><b>BOARD MEMBERSHIP</b></p>
<p><b>EX-OFFICIO &amp; STAFF MEMBERS</b> Lindsay Crockett Dallas DeArmond Mike Reynolds Christina Magrans-Tillery Mark Riehl</p>	<p><b>DEPARTMENT/ORGANIZATION</b> Knoxville-Knox County Planning Knoxville-Knox County Planning Knoxville-Knox County Planning City Law Department City Plans Review and Inspections</p>

## *Downtown Design Review* **Certificates of Appropriateness**

**0 W. Hill Ave. / Parcel ID 94 M D 018**

New primary structure (4-D-24-DT)

*Mr. Blackburn and Chairman Wright recused themselves from discussing or voting on this application and left the table.*

**Staff Recommendation:** Staff recommends approval of Certificate 4-D-24-DT, subject to the following conditions:

- 1) Final site plan, pedestrian improvements, and parking garage to meet City Engineering standards;
- 2) Final landscaping plan to meet standards of City zoning code;
- 3) Any mechanical equipment or service utilities not shown on plans to be placed on secondary elevations and receive screening as necessary;

4) Signage to return to DRB as a separate application;

**Discussion:** Lindsay Crockett reviewed the application and the staff recommendation.

The following people spoke in favor of the item:

Garry Rodgers, 803 White Oak Valley Rd., Cleveland, TN 37312

Ben Hudgins, 530 Means St. Ste. 105, Atlanta, GA 30318

Joshua Thompson, 3824 Fox Hills Dr., Marietta, GA 30067

Taylor Forrester, 1111 N. Northshore Dr. Ste. S-700, Knoxville, TN 37919

The following people spoke in opposition to this item:

Josh Wright, 608 W Hill Ave. #401, Knoxville, TN 37902

Daniel Sanders, 920 Volunteer Landing Ln., Knoxville, TN 37915

Thomas Goldsby, 608 W Hill Ave., Knoxville, TN 37902

Dina Markakis, 608 W Hill Ave., Knoxville, TN 37902

Wally Shaw, 608 W Hill Ave., Knoxville, TN 37902

***A motion was made by Mr. DeBardelaben and seconded by Mr. Thurman to approve certificate 4-D-24-DT, per staff recommendation, with the additional condition that the applicant provide final material specifications for the parking garage screening to staff for review and approval. The motion carried unanimously, 8-0.***

**Meeting:** 6/20/2024  
**Project:** Hill and Locust  
**Applicant:** Ben Hudgins Brock Hudgins Architects

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## Property Information

**Location:** 0 W. Hill Ave. **Parcel ID** 94 M D 018  
**Zoning:** DK (Downtown Knoxville)  
**Description:**  
New construction residential building proposed for multiple vacant parcels.

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## Description of Work

Level III Construction of a New Building/Structure

**SUMMARY:** Proposed new 18-story residential construction (9-14 stories of residential apartments above 5 stories of structured parking on the rear massing). The building is proposed for an L-shaped site at the intersections of W. Hill Ave., Locust Street, and Front Ave., located immediately north of Neyland Drive and the river. The site is significantly sloped towards the south and the river. The building is L-shaped, with a 6-story massing fronting W. Hill. Ave and extending south along Locust Street, and the larger massing facing Front Ave.

**SITE LAYOUT AND ACCESS:** The primary access point to the garage extends from W. Hill Ave via a two-way drive accessing the parking garage, along with ride-share spots, and short term spaces. A secondary access point is located to the west/southwest side of the garage, which will extend north off Front Ave and turn right into the parking garage. The primary pedestrian access fronts Hill Avenue at the northeast corner of the building.

### DESIGN ELEMENTS:

The 7-story massing fronting Hill Avenue is clad in brick veneer, featuring vertical metal accents. As the building extends south along Locust Street, a central section (increasing in number of stories at grade, due to the slope) is clad in vertical-ribbed, prefinished metal panels. A flat-roof metal canopy extends around the first story at the corner of Hill Ave and Locust Street. On this section, the windows are evenly spaced and symmetrically arranged single-light storefront systems. Five full-light entry systems are spaced along the Locust Street elevation.

The massing towards the river features a cast-in-place concrete parking garage clad in metal vertical fin screening. The residential section above the garage is C-shaped, surrounding an amenity level with a pool immediately above the garage, fronting the river. The residential section is proposed to be clad in hard-coat stucco and features single-light storefront windows, aluminum and glass balconies on all elevations.

Floor plans show the mechanical, electrical, service, and trash functions located at the southwest corner.

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## Applicable Design Guidelines

Downtown Design Guidelines

A. Public Realm

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## 1. Pedestrian and Bicycle Safety

1g. Consolidate curb-cuts and locate driveways near mid-block, when necessary; alley access should be provided for service and parking, if feasible.

## 3. Parking Facilities

3a. Create parking garages that do not contain blank walls. Allow for future commercial uses that may not be feasible at the time of construction.

3b. Locate parking garages under structures, or provide for retail, residential or office uses that line the garage. Corner locations are preferable for commercial uses.

3g. Access to parking garages should not limit options for future development of contiguous or adjoining space, especially on corners.

## 4. Downtown Beautification

4a. Foster downtown beautification with landscaping and plantings, public art, and public open space.

4c. Plant street trees where possible. Choose tree planting locations that will not significantly alter the setting of or harm the materials of historic buildings.

## B. Private Realm

### 1. Building Mass, Scale and Form

1a. Maintain a pedestrian-scaled environment from block to block.

1b. Foster air circulation and sunlight penetration around new buildings. Buildings may be designed with open space, as allowed under existing DK zoning; or buildings may be 'stepped back' on upper floors with lower floors meeting the sidewalk edge.

1c. Use building materials, cornice lines, signs, and awnings of a human scale in order to reduce the mass of buildings as experienced at the street level.

1d. Divide larger buildings into 'modules' that are similar in scale to traditional downtown buildings. Buildings should be designed with a recognizable base, middle, and top on all exposed elevations.

1e. Avoid blank walls along street-facing elevations.

### 2. Building Location

2a. Set buildings back five feet in order to provide wider sidewalk space when new construction in non-historic areas is to be more than half the length of the block.

2b. Consider using landscape elements to define the sidewalk edge where a building is to be set back from the sidewalk.

2d. Limit grade separations above or below the sidewalk, generally no more than 3 feet. Allow for clear sightlines into and out of buildings and plazas.

2e. Design private plazas to be pedestrian-friendly. Provide human-scale amenities and include landscaping.

### 3. Building Materials

3a. Use complimentary materials and elements, especially next to historic buildings.

### 4. Architectural Character

4a. Encourage first floor uses that draw walk-in traffic; businesses that do not require pedestrian traffic should be located on other floors.

4b. Enhance pedestrian interest in commercial and office buildings by creating a largely transparent and consistent rhythm of entrances and windows.

4c. Scale first floor signs to pedestrians.

4d. Differentiate the architectural features of ground floors from upper floors with traditional considerations such as show-windows, transoms, friezes, and sign boards.

4e. Design top floors to enhance the skyline of the block through cornices and details that are harmonious with adjacent architecture. 4f. Encourage the use of 'green roofs' and other sustainable practices, while minimizing the

visual impact from the street.

#### 5. Ground Floor Doors and Windows

- 5a. Use consistent rhythm of openings, windows, doorways, and entries.
- 5b. Orient primary front entrances to the main street; secondary entrances should be clearly defined and oriented to streets or alleys, as appropriate.
- 5c. Design entrances according to the proportions of the building's height and width.
- 5d. Consider corner entrances at the ends of blocks.
- 5e. All windows at the pedestrian level should be clear
- 5f. Recess ground floor window frames and doors from the exterior building face to provide depth to the facade.

#### 6. Residential Buildings

- 6a. Elevate the first floor of townhouses and apartment buildings so that pedestrians cannot look directly into the residence from the sidewalk level.
- 6b. Design entrances to residential buildings so that access is separated from pedestrian flow on the sidewalk.
- 6c. Encourage the development of mixed-use buildings with apartments over lower story commercial uses.

#### 7. Mechanical Equipment and Service Utilities

- 7a. Minimize the visual impact of mechanical equipment through screens or recessed/ low-profile equipment.
- 7b. Do not locate units on a primary façade.
- 7c. Screen rooftop vents, heating/ cooling units and related utilities with parapet walls or other screens. Consider sound-buffering of the units as part of the design.
- 7d. Locate utility connections and service boxes on secondary walls.
- 7e. Reduce the visual impacts of trash storage and service areas by locating them at the rear of a building or off an alley, when possible.
- 7f. Screen dumpsters from view.
- 7g. Locate satellite dishes out of public view, where possible.
- 7h. Allow solar panels and other technological advances on rooftops and other unobtrusive locations. Solar panels should not be considered on the elevations of historic buildings.

#### The Boulevard District:

- 2a. Design building entrances to be clearly oriented to the street.
- 2b. Encourage building forms that are complimentary to the mass of adjacent buildings.
- 2c. Design building elevations to compliment the buildings along the side or back streets when buildings are to face more than one street.
- 2d. Screen service facilities or incorporate them into the design of new buildings so that they are not obtrusive.

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## Comments

- 1. **SITE:** the building is proposed for a series of adjacent vacant parcels, which have historically been cleared and used as occasional surface parking. Part of the building (the Hill Avenue massing) is zoned DK-G (Grid subdistrict), while the south massing is zoned DK-B (Boulevard subdistrict). The block on W. Hill Avenue contains an eclectic mix of significant historic houses, historic multi-family structures, and a contemporary multi-family building. The site is on the edge of downtown, fronting Neyland Drive and the riverfront, with the Maplehurst neighborhood to the west and the City-County Building to the east. The architectural context is varied in time period, detail, and scale.
- 2. **PARKING:** the application includes a 5-story structured parking garage, with access off Front Avenue and W. Hill Avenue. The primary garage access point is located on W. Hill Avenue, and will feature two separated lanes for vehicle ingress and egress. The revised application includes a secondary access point off Front Ave. The parking garage generate an increase in vehicle traffic for W. Hill Avenue and Front Ave. Guidelines recommend "consolidating curbs-cuts and locating driveways near mid-block, when necessary," and prioritizing pedestrian safety. The access point on Hill Ave does meet the guidelines (consolidating the driveways into one curb cut) but additional



information may be necessary for permitting related to mitigation of any potential sight distance issues for cars exiting the garage, pedestrian safety measures on the sidewalk on Hill Avenue, and any necessary safety measures for cars using Front Ave to access Locust or Neyland Drive. The final site plan and garage should meet City Engineering standards.

3. MASSING AND SCALE: guidelines state "building form should be consistent with the character of downtown as an urban environment and should reinforce the pedestrian activity at street level." Previous submissions separated the building into two large massings, one fronting Hill Avenue and running south along Locust Street, and one U-shaped massing fronting the river. Revisions for the June meeting incorporate variations in height, including new penthouse levels, to vertically break the large massing into what resembles five separate adjoining buildings. The setbacks required by the zoning code (incorporated along Henley Street, Front Ave, and Locust Street) further contribute to this effect. The massing alongside Henley Street has been reduced in height.

Viewing the massings horizontally, the south elevation was previously divided into two large sections of the parking garage and the residential massing. The parking garage has received substantial revisions since the initial application; revisions submitted for June incorporate vertical metal fins as screening for the parking garage. The vertical fins obscure the parking garage and are divided into two two-story sections, which further reduces the visual scale of the garage.

The guidelines recommend dividing buildings into "modules" similar in scale to traditional downtown buildings with a recognizable base, middle, and top. The north massing fronting Hill Ave is similar in scale to adjacent buildings and other historic buildings downtown, using a largely transparent ground level separated from the residential upper levels. The incorporation of recessed penthouse levels serve to further divide the massings on the north, east, and south elevations into top, middle, and bottom sections.

At previous meetings, the Board has extensively discussed the scale of the building's south massing in relation to the context of the area. The Boulevard District guidelines recommend "building forms that are complimentary to the mass of adjacent buildings." The building section along Hill Avenue is generally compatible in scale with the adjacent building, measuring one story taller. The buildings along Hill Avenue steps from six stories, to three stories, to two stories, to 1.5 stories. The broader context is varied in scale, including taller historic structures, surface parking lots, the Bank of America building, and the City-County Building. In the opinion of staff, the revised proposal is compatible with the context.

4. PEDESTRIAN EXPERIENCE: guidelines encourage maintaining a pedestrian-scaled environment from block to block, and encourage incorporating first-floor uses that are open to pedestrians and draw walk-in traffic. The project includes one café space on the first story of the Hill Avenue massing; there are no other public areas on the Locust St or Front Ave elevations. Revised drawings responding to the zoning code have incorporated five storefront entries along Locust Street. Guidelines also recommend using building materials and entries at a human scale to create an engaging pedestrian experience, avoiding blank walls along street-facing elevations, and using a "consistent rhythm of entrances and windows."

At the April meeting, the Board discussed potential additional pedestrian engagement on the building's south elevation (facing Front Avenue). The DK Design Standards in the zoning code require a minimum transparency of 30%, measured between 2' and 10' in height, on ground floor front facades. For the May meeting, the applicant incorporated an additional open level of parking along Front Avenue to open to the street and meet transparency requirements. Revisions for the June meeting show the rightmost (southeast) section of the ground level along Front Avenue incorporating a series of storefront windows enclosing interior amenity space.

The June revisions include additional landscaping detail, street trees, sidewalks, and information on a proposed crosswalk along Locust Street, along with a proposed multi-use path to connect to the existing pedestrian bridge. Coordination with the City of Knoxville will be necessary for additional off-site pedestrian improvements.

5. MATERIALS: guidelines recommend the use of building materials that "relate to the scale, durability, color, and texture of the predominate building materials in the area." The surrounding area is characterized by a wide array of exterior materials, including contemporary and historic brick masonry, stucco, and wood siding, along with contemporary office buildings and parking structures. Overall, the proposed hard-coat stucco as a primary exterior material is compatible with the context. The massings on Locust St. and Hill are broken up via different siding, with brick veneer fronting Hill Avenue. The parking garage design has been further revised to include vertical metal fins as screening, along with vertical concrete columns and a section of storefront glass.

6. MECHANICAL: the floor plans indicate mechanical and service utilities to be located on secondary elevations. Any rooftop mechanical fixtures not depicted on plans or elevations should be set back from the roof edge and receive screening to meet the City zoning code.

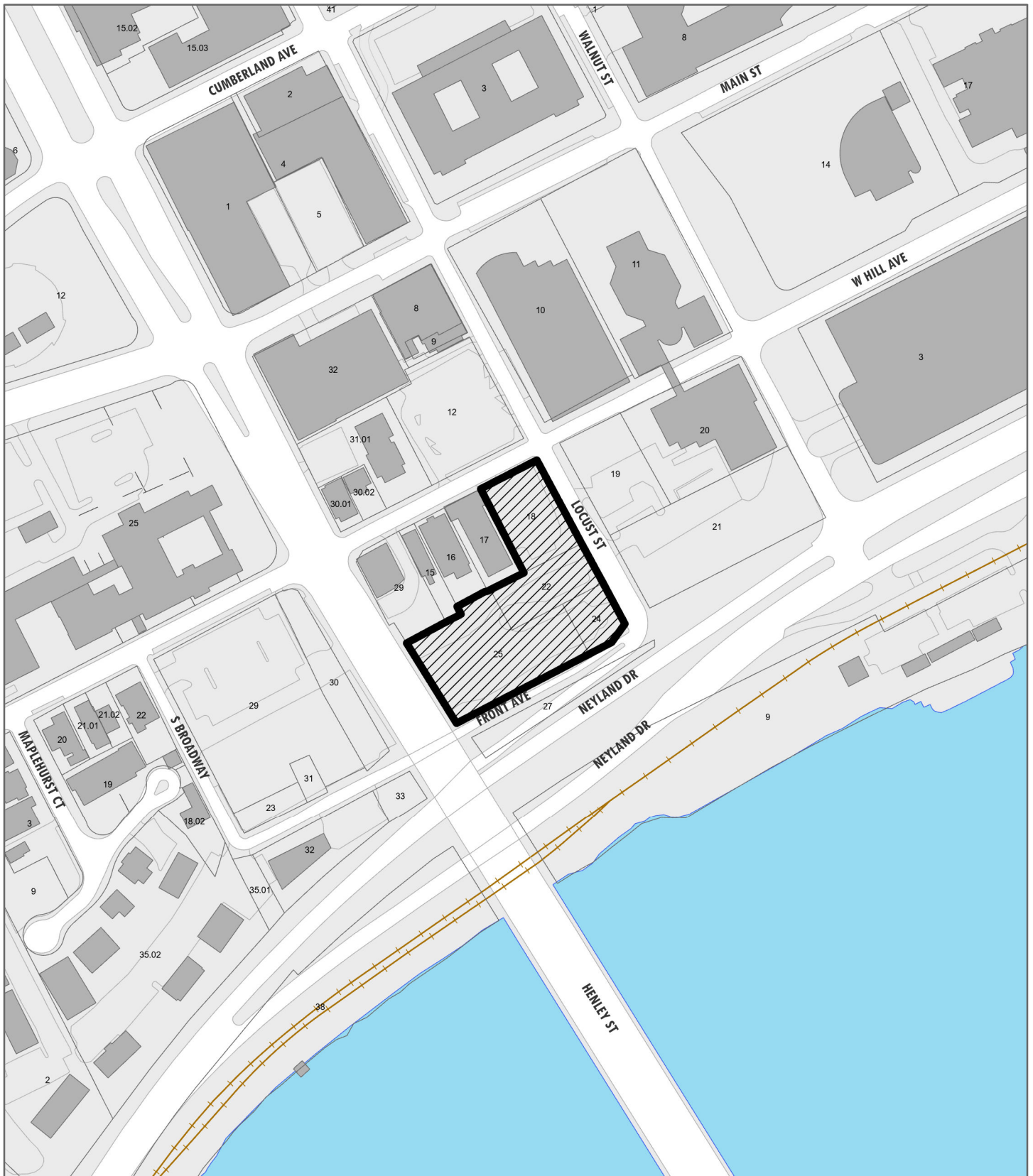
7. SIGNS: the signs depicted on the elevation drawings do not contain sufficient information for Board review at this time; a separate signage application should be submitted to the DRB for further review.

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## Recommendation

Staff recommends approval of Certificate 4-D-24-DT, subject to the following conditions:

- 1) Final site plan, pedestrian improvements, and parking garage to meet City Engineering standards;
- 2) Final landscaping plan to meet standards of City zoning code;
- 3) Any mechanical equipment or service utilities not shown on plans to be placed on secondary elevations and receive screening as necessary;
- 4) Signage to return to DRB as a separate application.



**4-D-24-DT**

**APPLICATION FOR CERTIFICATE OF APPROPRIATENESS**

Petitioner:

**DOWNTOWN  
DESIGN  
REVIEW  
BOARD**

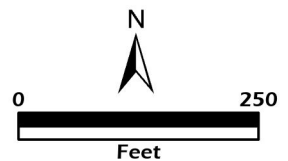


0 W. Hill Ave.

Level 3: Construction of new building/  
structure

Original Print Date: 4/3/2024  
Knoxville/Knox County Planning · Downtown Design Review Board

Revised:  
Knoxville/Knox County Planning · Downtown Design Review Board





# DESIGN REVIEW REQUEST

DOWNTOWN DESIGN (DK)

HISTORIC ZONING (H)

INFILL HOUSING (IH)

BEN HUDGINS

Applicant

3/29/24

Date Filed

4/17/24 | 5/15/2024 | 6/20/2024

Meeting Date (if applicable)

4-D-24-DT

File Number(s)

## CORRESPONDENCE

All correspondence related to this application should be directed to the approved contact listed below.

Owner  Contractor  Engineer  Architect/Landscape Architect

BEN HUDGINS

Name

BROCK HUDGINS ARCHITECTS

Company

530 MEANS STREET, SUITE 105

Address

ATLANTA

City

GA

State

30318

Zip

404-213-5271

Phone

BHUDGINS@BROCKHUDGINS.COM

Email

## CURRENT PROPERTY INFO

HILL & LOCUST PARTNERS, GP

WHELCHER / GILLENWATER TRUST

WHELCHER BARBARA H & ZENITH PROPERTIES LLC

Owner Name (if different from applicant)

109 CIRCLE LN

7723 MICKELSON CT

7723 MICKELSON CT

Owner Address

KNOXVILLE TN 37919

NAPLES FL 34113

NAPLES FL 34113

Owner Phone

0 W. HILL AVE, 1015 LOCUST ST, 0 LOCUST ST, 0 FRONT AVE, 0 W. HILL AVE

Property Address

094MD018, 094MD022, 094MD024, 094MD025, 094ME033, 094ME030

Parcel ID

103

Neighborhood

DK-G, DK-B

Zoning

## AUTHORIZATION

LLC

Staff Signature

Lindsay Crockett

Please Print

4/1/24

Date

Applicant Signature

BEN HUDGINS

Please Print

3/29/24

Date

# REQUEST

DOWNTOWN DESIGN

**Level 1:**

- Signs     Alteration of an existing building/structure

**Level 2:**

- Addition to an existing building/structure

**Level 3:**

- Construction of new building/structure     Site design, parking, plazas, landscape

**See required Downtown Design attachment for more details.**

Brief description of work: NEW MULTIFAMILY DEVELOPMENT AT THE INTERSECTION OF HILL AVE AND LOCUST ST. THE PROPOSED NEW DEVELOPMENT INCLUDES A STRUCTURED PARKING GARAGE, A HIGHRISE RESIDENTIAL COMPONENT ALONG FRONT AVE, AND A MIDRISE RESIDENTIAL COMPONENT FRONTING HILL AVE. RESIDENTIAL AMENITIES INCLUDE A POOL TERRACE, CLUBROOM, FITNESS, AND LOBBY.

HISTORIC ZONING

**Level 1:**

- Signs     Routine repair of siding, windows, roof, or other features, in-kind; Installation of gutters, storm windows/doors

**Level 2:**

- Major repair, removal, or replacement of architectural elements or materials     Additions and accessory structures

**Level 3:**

- Construction of a new primary building

**Level 4:**

- Relocation of a contributing structure     Demolition of a contributing structure

**See required Historic Zoning attachment for more details.**

Brief description of work: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

INFILL HOUSING

**Level 1:**

- Driveways, parking pads, access point, garages or similar facilities     Subdivisions

**Level 2:**

- Additions visible from the primary street     Changes to porches visible from the primary street

**Level 3:**

- New primary structure  
      Site built     Modular     Multi-Sectional

**See required Infill Housing attachment for more details.**

Brief description of work: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

STAFF USE ONLY

**ATTACHMENTS**

- Downtown Design Checklist  
 Historic Zoning Design Checklist  
 Infill Housing Design Checklist

**ADDITIONAL REQUIREMENTS**

- Property Owners / Option Holders

**Level 1:** \$50 • **Level 2:** \$100 • **Level 3:** \$250 • **Level 4:** \$500

<b>FEE 1:</b>		<b>TOTAL:</b>
250.00		
<b>FEE 2:</b>		
<b>FEE 3:</b>		250.00

## **HILL & LOCUST – DRB RESUBMITTAL**

6/7/24

### **REFINEMENTS MADE IN REPOSE TO DRB FEEDBACK ON 5/15/24:**

#### **1. REDUCTION OF MASSING ALONG HENLEY STREET**

In addition to the 10' building setback along the Henley Street Bridge (incorporated just before the May DRB meeting), the building massing has been further reduced along the western portion of the building adjacent the Henley Street Bridge, creating distinct building modules within the overall building composition. The white portion of the Western wing of the building fronting Henley has been reduced by two stories, with a partial penthouse floor added to that wing of the building. The change in material and the setback at the upper penthouse floor results in a visual reduction of two floors of the white massing. The overall building composition now terraces from Locust Street down to Henley Street, resulting in a more pedestrian-scaled building for those walking across the bridge.

#### **2. INTRODUCTION OF PENTHOUSE FLOORS W/ STEPBACKS**

In order to accommodate the reduction in building massing and subsequent reduction in residential program along Henley Street, an additional partial penthouse floor has been added to both the Hill Ave module and the Locust Street module of the building. The change in material at the upper penthouse floors on all three modules provides some massing relief at each of the upper stories of the modules, while also providing a clearly delineated tripartite composition (base/middle/top) per the Downtown Design Guidelines. The modified roofline, enhanced by the added building steps and penthouse setbacks, reinforces the appropriateness of the building's scale in two ways: 1. From a pedestrian perspective, the upper penthouse setbacks recede from the building façade giving the appearance of one less floor; 2. From a distance, the upper penthouse story provides more articulation in the roofline, providing massing relief while making the building feel less monolithic.

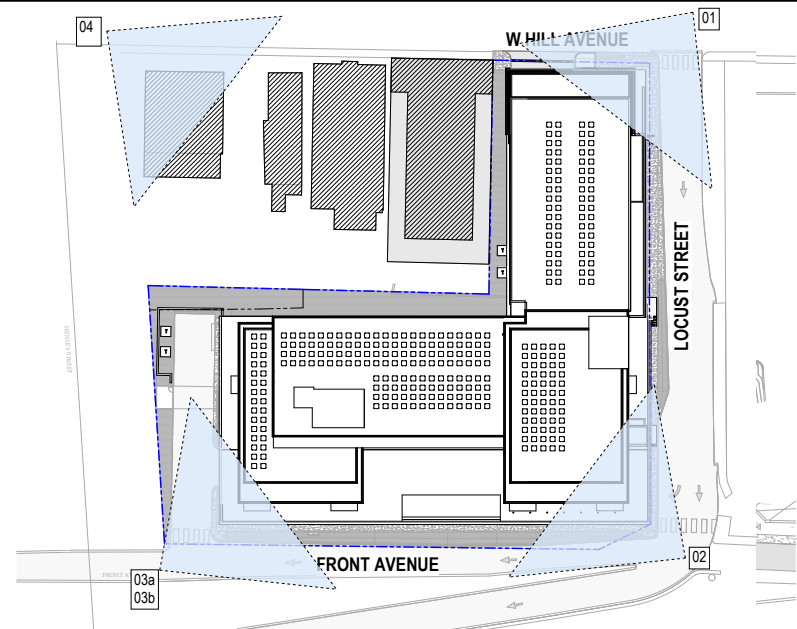
#### **3. GARAGE SCREENING**

The parking garage has been screened with vertical metal fins, angled to visually obstruct the garage structure while maintaining the required amount of openness to achieve natural ventilation of the parking garage. The metal fins are two stories in height, giving the appearance of two stories of parking rather than four, while maintaining a scale complementary to the Henley Street Bridge. The composition of concrete columns at street-level with vertical fins above is subtly referential to the massive concrete bridge piers touching down along the river with the repetitive rhythm of its pedestrian level guardrail above. The result is a thoughtful, timeless design approach to the garage treatment that complements the architectural language of the building above.

#### **4. FRONT AVE ACTIVATION**

The street-level along Front Ave has been further activated with the introduction of interior space at the intersection of Front Ave and Locust Street. This space is intended to be used for artist studio space, bike room, and pet spa. The glazed storefront expression enhances the pedestrian sidewalk experience while providing a commercial look and feel until Front Ave becomes viable for retail space.





VIEW 04



VIEW 03b



VIEW 02



VIEW 03a



VIEW 01

Print Record		
NO.	DATE	ISSUE

HILL AND LOCUST

LOCUST STREET AND HILL  
AVE  
KNOXVILLE, TN  
Project Number 2005

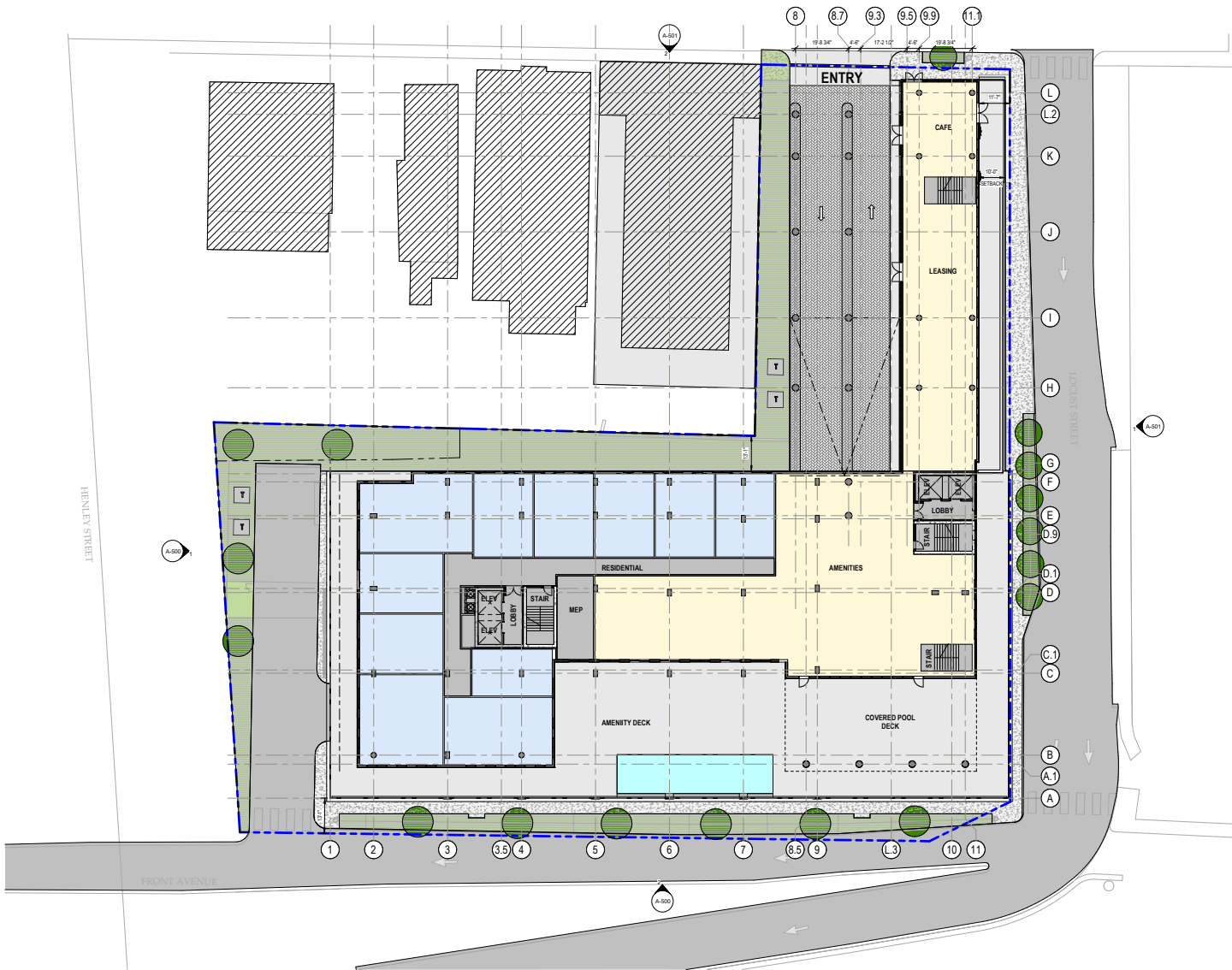
SITE CONTEXT

**G-023**

NOT FOR CONSTRUCTION







**FLOOR PLAN NOTES**

1. VERIFY DIMENSIONS, GRADES, BOUNDARIES AND CONSTRUCTION BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY REPORT ANY DISCREPANCIES TO ARCHITECT.
2. DO NOT SCALE DRAWINGS. USE DIMENSIONS ONLY.
3. TYPICAL DRAWINGS, DIMENSIONS, NOTES, FINISHES AND FIXTURES INDICATED ON TYPICAL PLANS, SECTIONS OR DETAILS SHALL APPLY TO SIMILAR, SYMMETRICAL OR OPPOSITE PLANS, SECTIONS OR DETAILS.
4. MATERIALS AND WORK SHALL COMPLY WITH APPLICABLE CODES AND REGULATIONS. VERIFY THAT SUBSTITUTIONS COMPLY WITH APPLICABLE CODES AND REGULATIONS.
5. PROVIDE ACCESS PANELS AT CEILING AND WALLS AS PER MECHANICAL, PLUMBING/ELECTRICAL, AND SPRINKLER DRAWINGS. ACCESS PANELS SHALL HAVE SAME RATING AS WALL OR CEILING IN WHICH THEY ARE LOCATED.
6. GUARD AND RAIL HEIGHTS AND CONSTRUCTION SHALL COMPLY WITH APPLICABLE CODES AND REGULATIONS. DIMENSIONS INDICATED ON PLANS ARE FACE OF STUD. TO CONCRETE WALLS AND/OR TOWER WALLS UNLESS NOTED OTHERWISE.
7. EXTERIOR DIMENSIONS ARE GIVEN TYPICALLY TO OUTSIDE FACE OF STOREFRONT. STUD, CONCRETE, OR MASONRY UNLESS NOTED OTHERWISE.
8. ALL DIMENSIONS, NOTES, FINISHES, AND FIXTURES SHOWN ON TYPICAL PLANS, SECTIONS OR DETAILS SHALL APPLY TO ALL SIMILAR, SYMMETRICAL OR REVERSED PLANS, SECTIONS OR DETAILS.

⬡ WINDOW SYSTEM. REFER TO WINDOW SCHEDULE SHEET FOR SPECIFICATION.

⦶ STOREFRONT SYSTEM. REFER TO STOREFRONT SCHEDULE SHEET FOR SPECIFICATION.

**PARKING SCHEDULE**

NO.	DATE	ISSUE

**HILL AND LOCUST**

LOCUST STREET AND HILL AVE  
ANDOVERVILLE, TN  
Project Number: 20205

**LEVEL 1 OVERALL FLOOR PLAN**

**A-101**

NOT FOR CONSTRUCTION

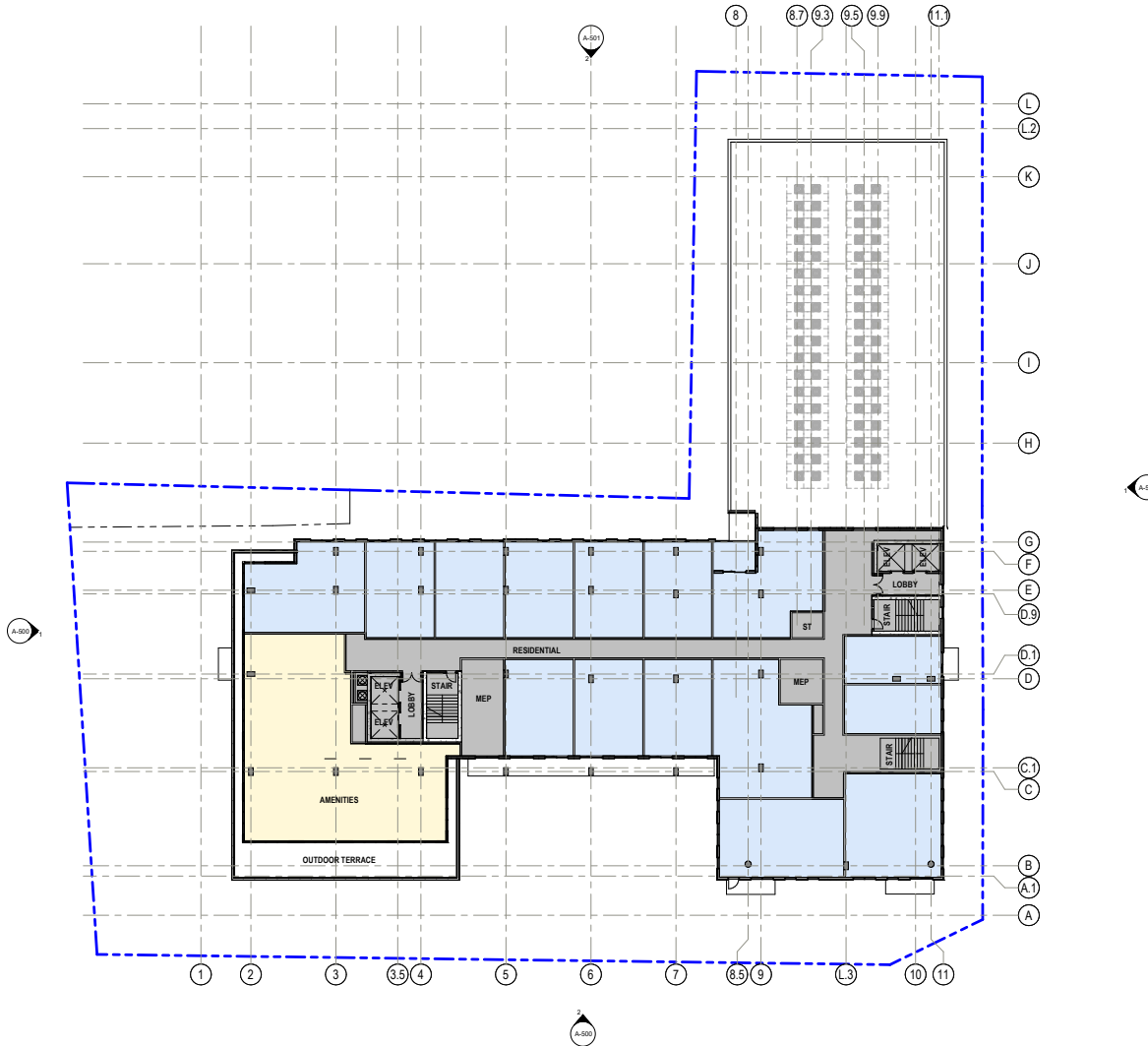


**FLOOR PLAN NOTES**

1. VERIFY DIMENSIONS, GRADES, BOUNDARIES AND CONSTRUCTION BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY REPORT ANY DISCREPANCIES TO ARCHITECT.
2. DO NOT SCALE DRAWINGS. USE DIMENSIONS ONLY.
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7. EXTERIOR DIMENSIONS ARE GIVEN TYPICALLY TO OUTSIDE FACE OF STOREFRONT. STUD, CONCRETE, OR MASONRY UNLESS NOTED OTHERWISE.
8. ALL DIMENSIONS, NOTES, FINISHES, AND FIXTURES SHOWN ON TYPICAL PLANS, SECTIONS OR DETAILS SHALL APPLY TO ALL SIMILAR, SYMMETRICAL OR REVERSED PLANS, SECTIONS OR DETAILS.

**FLOOR PLAN LEGEND**

- ◻ WINDOW SYSTEM. REFER TO WINDOW SCHEDULE SHEET FOR SPECIFICATION.
- ◯ STOREFRONT SYSTEM. REFER TO STOREFRONT SCHEDULE SHEET FOR SPECIFICATION.



Print Record	DATE	ISSUE

**HILL AND LOCUST**

LOCUST STREET AND HILL AVE  
INDYVILLE, TN  
Project Number: 2205

**LEVEL 9 OVERALL FLOOR PLAN**

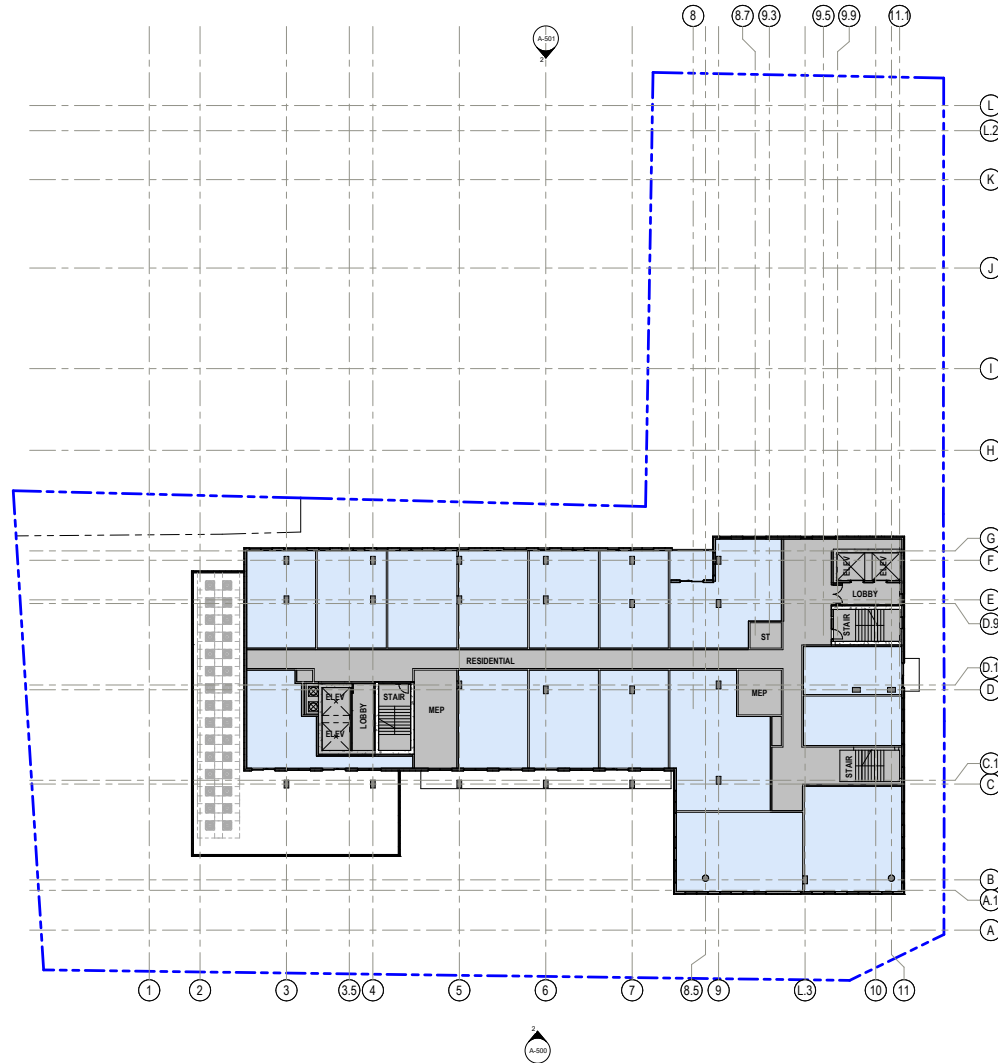
**A-109**  
NOT FOR CONSTRUCTION

**FLOOR PLAN NOTES**

1. VERIFY DIMENSIONS, GRADES, BOUNDARIES AND CONSTRUCTION BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY REPORT ANY DISCREPANCIES TO ARCHITECT.
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**FLOOR PLAN LEGEND**

- ◻ WINDOW SYSTEM. REFER TO WINDOW SCHEDULE SHEET FOR SPECIFICATION.
- STOREFRONT SYSTEM. REFER TO STOREFRONT SCHEDULE SHEET FOR SPECIFICATION.



Print Record	DATE	ISSUE

HILL AND LOCUST

LOCUST STREET AND HILL  
AVE.  
INDOEVILLE, TN  
Project Number \_\_\_\_\_ 2005

LEVEL 10-11 OVERALL  
FLOOR PLAN

**A-110**

NOT FOR CONSTRUCTION

### ELEVATION NOTES

- ALL METAL CAPS, COPING, FLASHING, SCOPERS, FLASHING, AND OTHER RELATED ROOF ITEMS SHALL BE INSTALLED PER THE LATEST SBMACA REGULATION.
- REFER TO PROJECT MANUAL FOR SPECIFICATIONS.
- COORDINATE BUILDING HEIGHT LOCATIONS AND REQUIREMENTS WITH OTHER ELECTRICAL PRIOR TO FABRICATION. REFER TO RCP FOR SPECIFICATION OF EXTERIOR LIGHTING.
- MATERIAL, MANUFACTURERS AND COLORS INDICATED ON DRAWINGS ARE SUBJECT TO CHANGE DURING THE CONSTRUCTION PROCESS. ANY DEVIATIONS FROM APPROVED PLANS SHALL FOLLOW ESTABLISHED ADMINISTRATIVE PROCEDURES FOR APPROVAL.

### ELEVATION LEGEND



### EXTERIOR MATERIAL LEGEND

- B1-A HARD COAT STUCCO (SMOOTH FINISH) COLOR: TRD
- B1-B HARD COAT STUCCO (SMOOTH FINISH) COLOR: TRD
- C-1 CAST IN PLACE CONCRETE (RUBBED FINISH) COLOR: TRD
- BBR-1 BRICK VENEER (STANDARD OF DESIGN) COLOR:
- MTL-1 PREFINISHED ALUMINUM (STANDARD OF DESIGN) COLOR:
- MTL-2 PREFINISHED ALUMINUM STOREFRONT (STANDARD OF DESIGN) COLOR:
- MTL-3 PREFINISHED METAL PANEL (SMOOTH) (STANDARD OF DESIGN) COLOR:
- MTL-4 PREFINISHED METAL PANEL - VERTICAL RIBBED (STANDARD OF DESIGN) COLOR:

Print Record  
DATE: ISSUE:

NO. DATE: ISSUE:

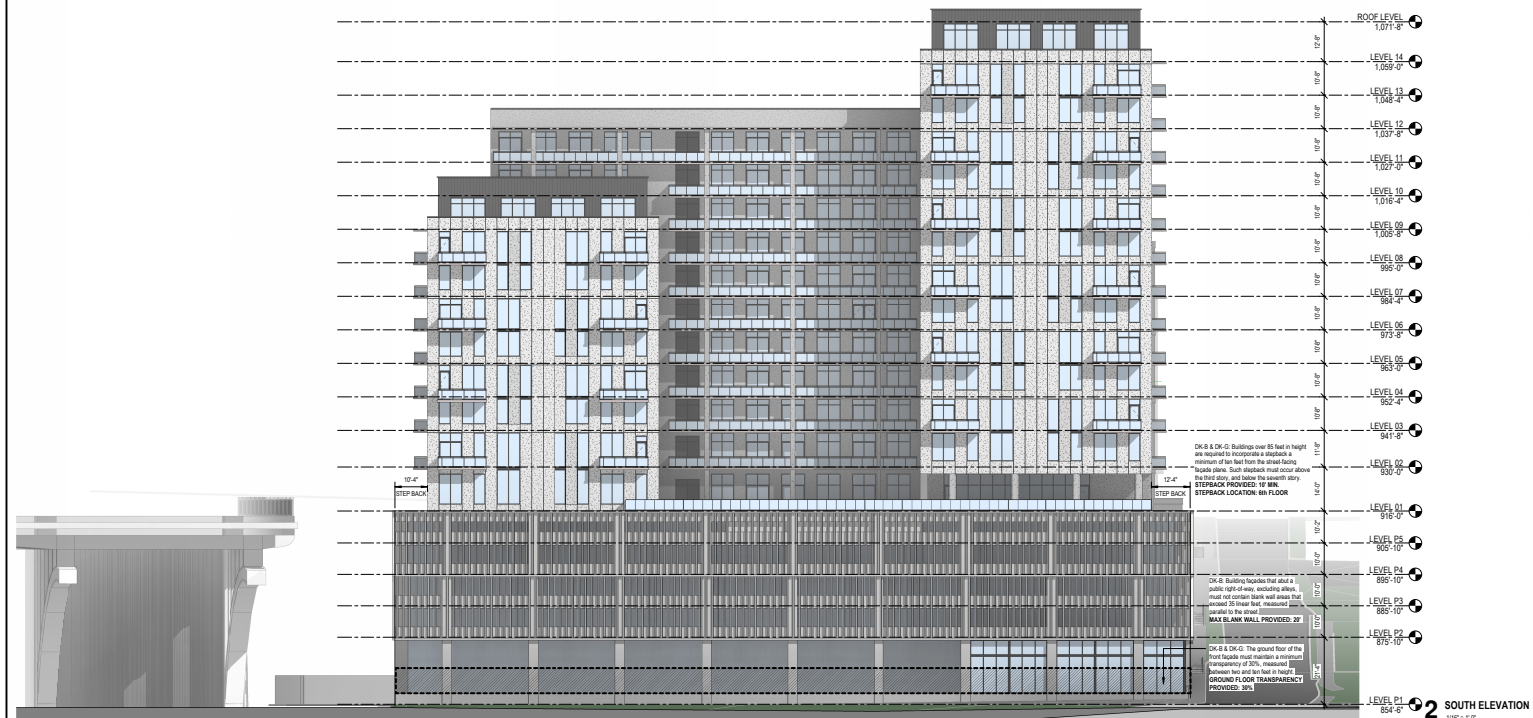
### HILL AND LOCUST

LOCUST STREET AND HILL AVE  
INDYVILLE, TN  
Project Number: 2005

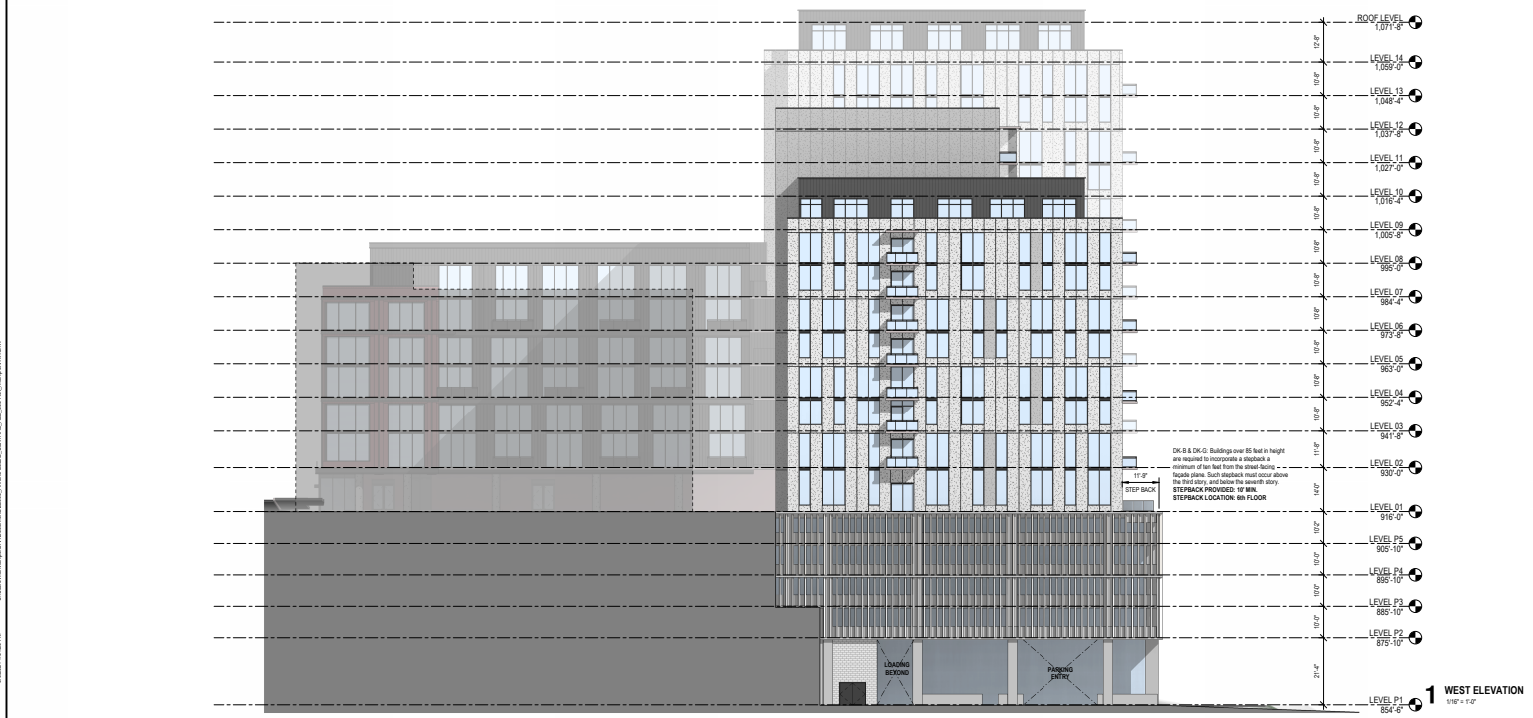
### EXTERIOR ELEVATIONS

## A-500

NOT FOR CONSTRUCTION



**2 SOUTH ELEVATION**  
1/16" = 1'-0"



**1 WEST ELEVATION**  
1/16" = 1'-0"













HILL & LOCUST: PLAN VIEW



PLANT_SCHEDULE			
CODE	QTY	BOTANICAL / COMMON NAME	CONT
<b>TREES</b>			
CO	7	Carpinus caroliniana 'Orange Crush' / Orange Crush American Hornbeam	4" cal.
LA	10	Liriodendron tulipifera 'Arnold' / Arnold Tulip Poplar	4" cal.
<b>SHRUBS</b>			
AM	12	Azalea encore 'Autumn Amethyst' / Autumn Amethyst Azalea	5 gal.
CG2	24	Camellia sasanqua 'Shishi Gashira' / Shishi Gashira Camellia	5 gal.
HA	36	Hydrangea paniculata Little Lime / Little Lime Hydrangea	5 gal.
IP	24	Illicium parviflorum / Anise Tree	5 gal.
IS	160	Ilex crenata 'Soft Touch' / Soft Touch Japanese Holly	5 gal.
MS	34	Miscanthus sinensis 'Adagio' / Adagio Eulalia Grass	3 gal.
<b>VINES</b>			
HD	7	Hedera helix / English Ivy	1 gal.
<b>GROUND COVERS</b>			
CD	580	Carex oshimensis 'Everest' / Japanese Sedge	10" pot
LB	2,976	Liriope muscari 'Big Blue' / Big Blue Lilyturf	10" pot
<b>BIORETENTION</b>			
RR	1,135 sf	river rock / river rock	-

HILL & LOCUST: PLANTING PLAN AND SCHEDULE



BENCH



POLE LIGHT



TRASH RECEPTICLE



ENGLISH IVY



HORNBEAM



ARNOLD TULIP POPLAR



ANISE



AUTUMN AMETHYST AZALEA



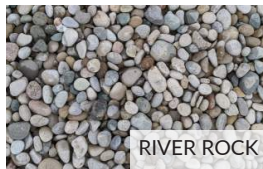
LITTLE LIME HYDRANGEA



LIRIOPE



CAREX



RIVER ROCK



SHISHI GASHIRA CAMELLIA

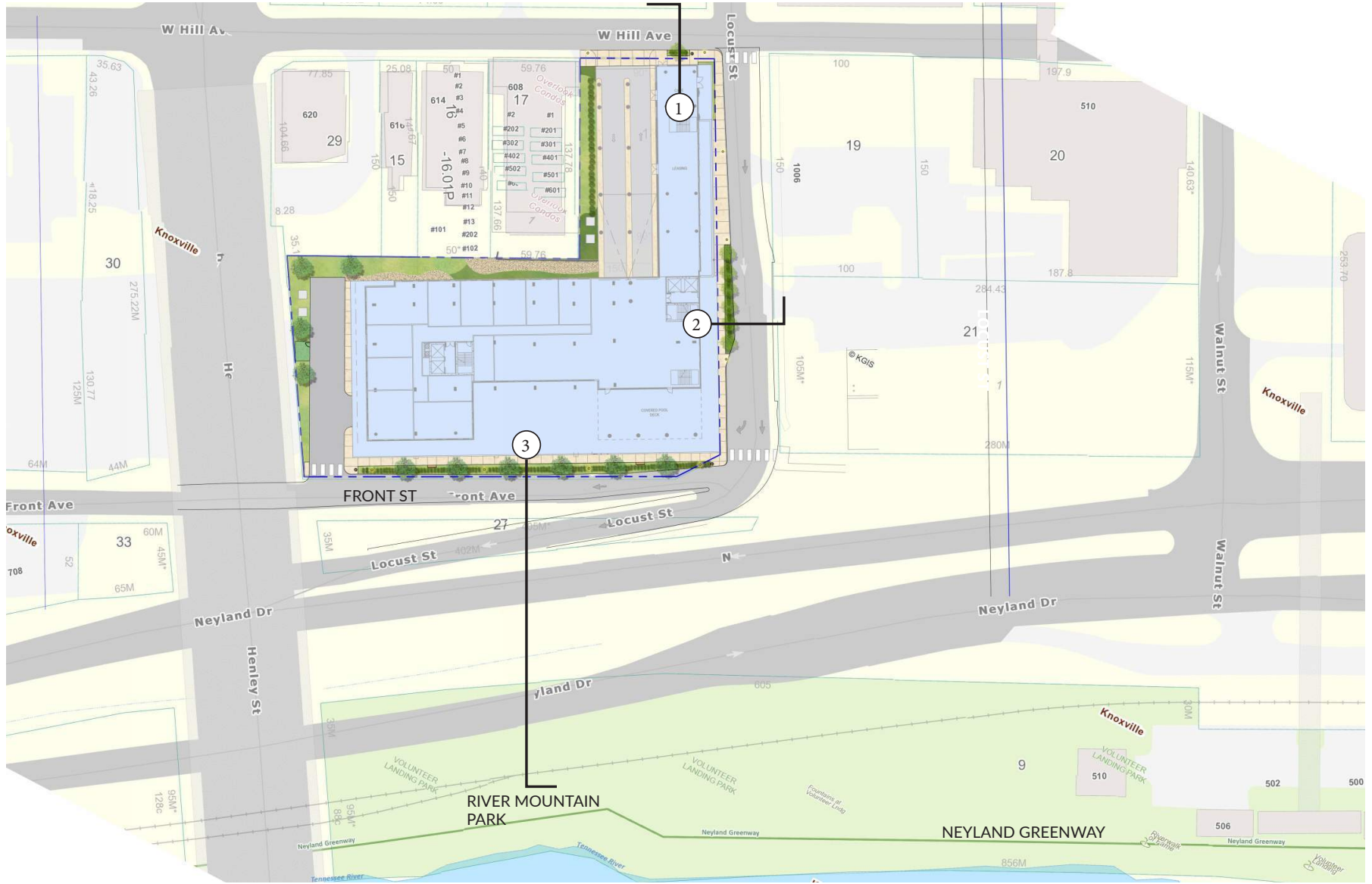


MISCANTHUS

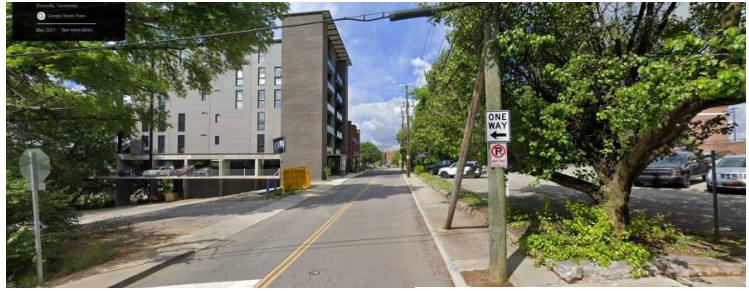
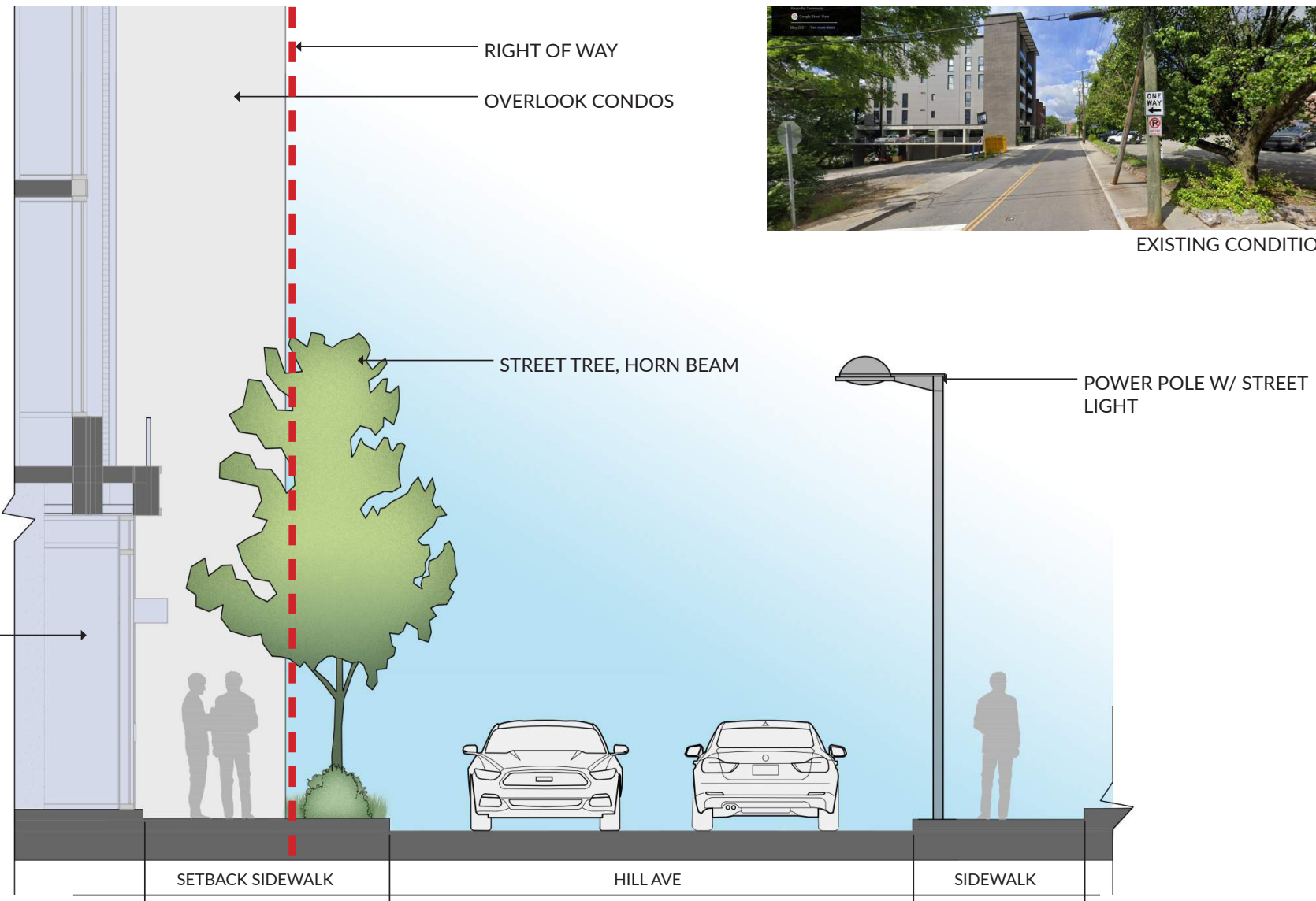


SOFT TOUCH HOLLY

# HILL & LOCUST - STREETScape FURNISHINGS & PLANT PALETTE

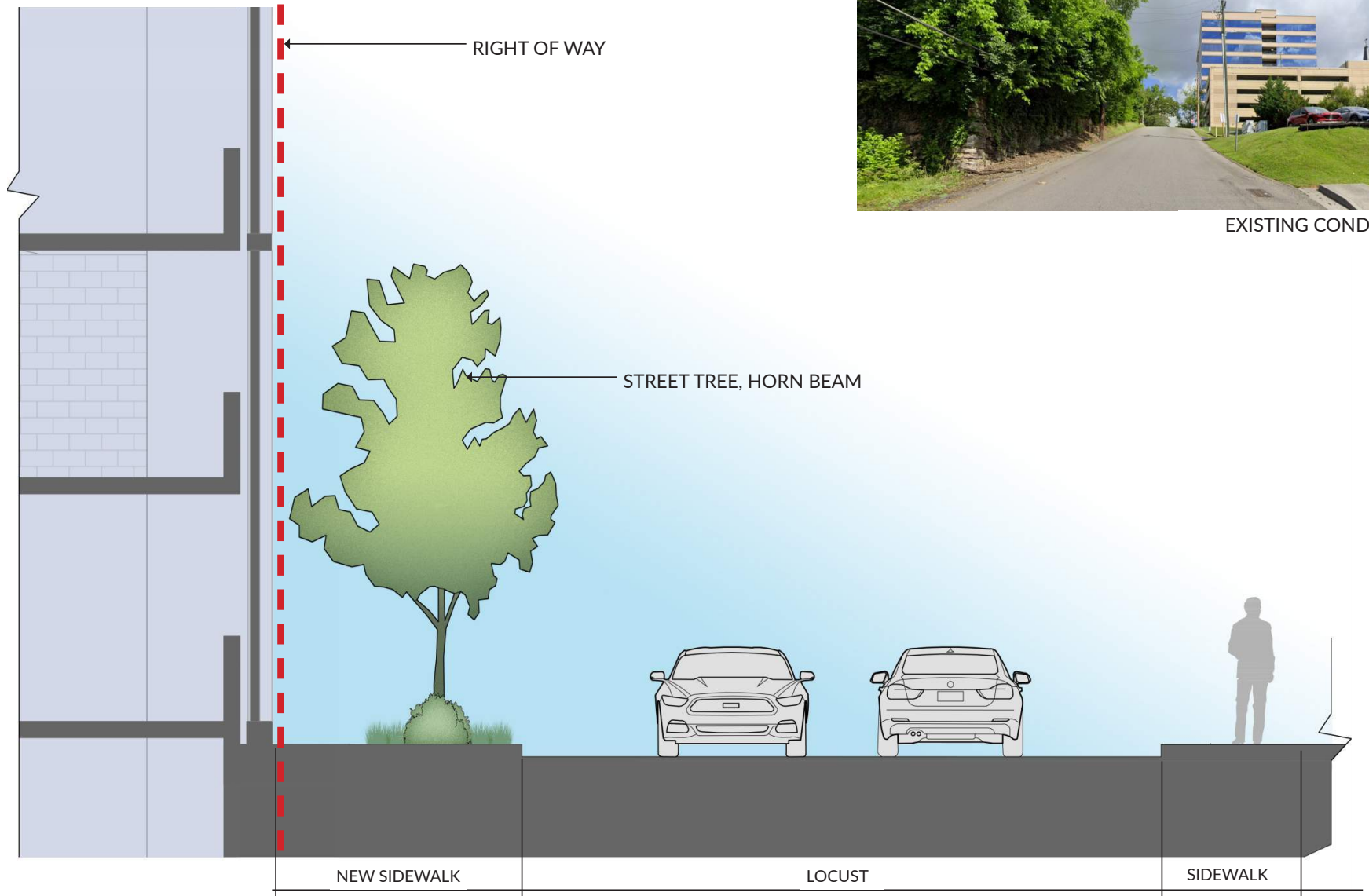


HILL & LOCUST: SECTION DIAGRAM



EXISTING CONDITIONS

HILL & LOCUST: SECTION 1



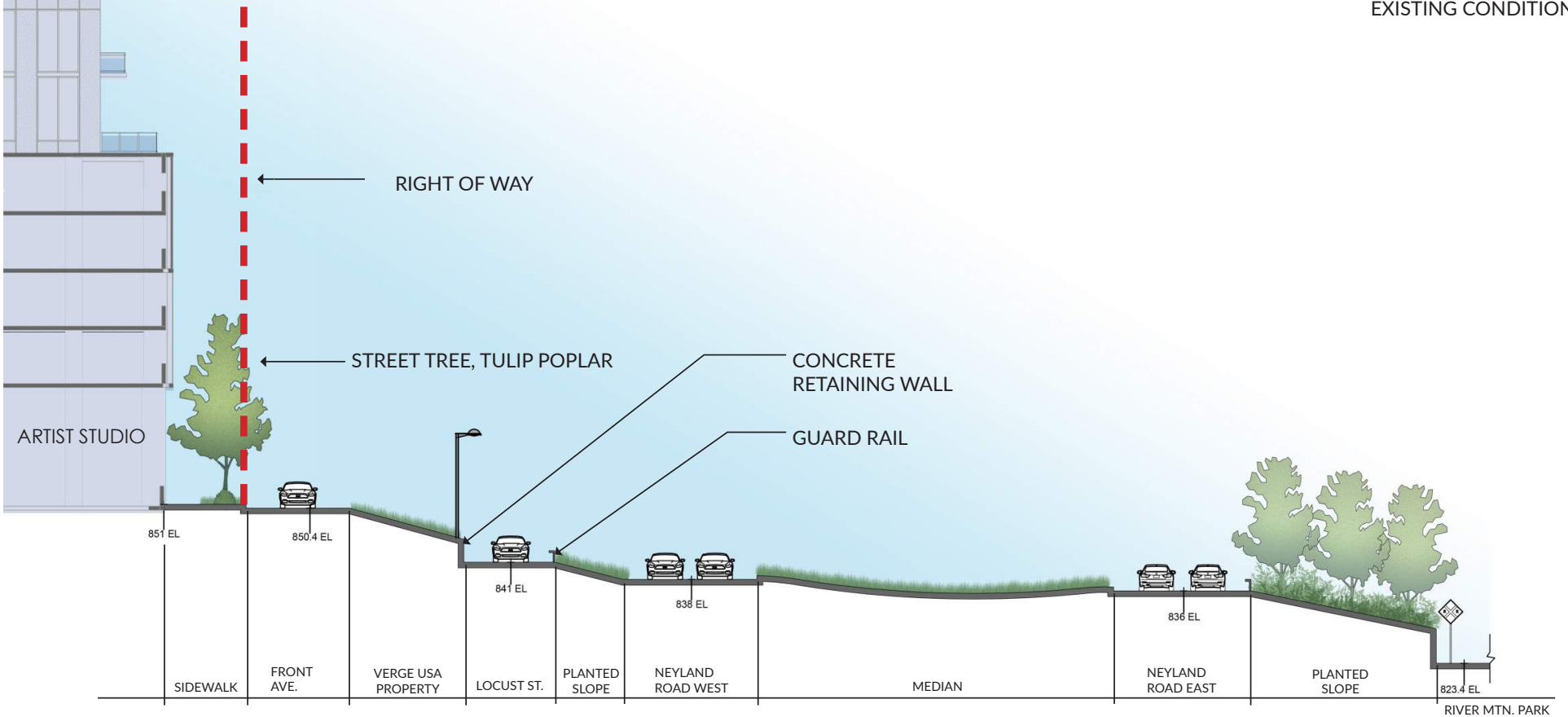
EXISTING CONDITIONS

HILL & LOCUST: SECTION 2

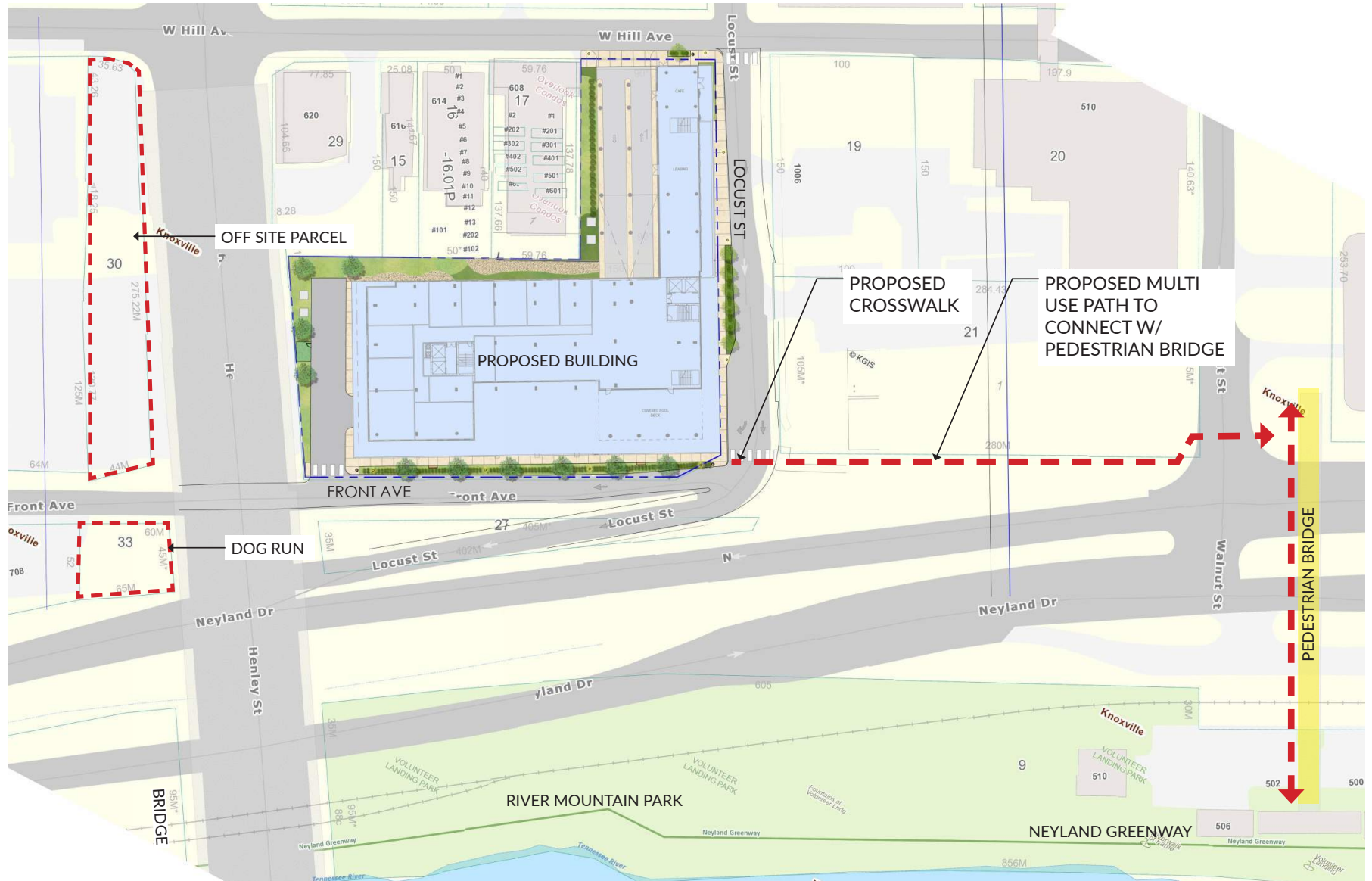




EXISTING CONDITIONS



HILL & LOCUST: SECTION 3



HILL & LOCUST: RIVERFRONT PARK PEDESTRIAN ACCESS

August 8, 2024

Planning Commission meeting

# Public Comments

## 5 Comments for 8-A-24-OB

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**Planning Staff** (37902), July 23, 2024 at 10:29 AM

Attached comments were submitted to the Design Review Board for case 4-D-24-DT prior to the item's approval on June 20, 2024. The item has been appealed to the Planning Commission as case 8-A-24-OB.

[View Attachment](#)

---

**Taylor** (37919), August 2, 2024 at 12:21 PM

Please find attached correspondence in support of the DRB's approval on the design plans for the Project on behalf of the applicant, Brock Hudgins Architects.

[View Attachment](#)

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**Taylor** (37919), August 2, 2024 at 12:26 PM

Attached are the Exhibits referenced in letter submitted on behalf of the DRB applicant, Brock Hudgins Architects. This attachments provides clearer renderings of the Exhibits.

[View Attachment](#)

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**Daniel** (37915), August 5, 2024 at 2:53 PM

Please see the attached correspondence. The Overlook Owners Association opposes this item due violation of numerous Downtown Knoxville Design Guidelines. We urge the Planning Commission to deny the application.

[View Attachment](#)

---

**Rebekah Jane** (37902), August 6, 2024 at 9:56 AM

The City Administration supports the Design Review Board's (DRB) unanimous vote for approval of the Certificate of Appropriateness for the case 4-D-24-DT, at the corner of Hill and Locust. The applicant has responded to recommendations and feedback from Knoxville Knox County Planning staff, members of the DRB, and a community meeting of adjacent neighbors in an initial workshop and three separate DRB meetings. Based on that input, design revisions were made specifically focusing on scale, massing, materiality, and pedestrian experience. We agree with and support the DRB decision for Certificate of Appropriateness and that the project meets the intent of the design guidelines.

---

The Planning Commission met in regular session on August 8, 2024, at 1:30 p.m. in the Main Assembly room of the City County Building.

Item No.

File No.

### 1. ROLL CALL, INVOCATION AND PLEDGE OF ALLEGIANCE

Ms. Karyn Adams	Ms. Nancy Barger*	Mr. Miles Biggs
Ms. Tamara Boyer	Mr. Louis Browning	Mr. Logan Higgins*
Mr. Tim Hill, Chair	Mr. John Huber, Vice-Chair	Mr. Richard Levenson
Ms. Amy Midis	Ms. Kara Daley	Ms. Katie Overton
Ms. Marité Pérez	Mr. Matt Anderson	Mr. Nick Gill

A – Absent from the meeting, \*Left meeting early

### 5. OVERLOOK OWNERS ASSOCIATION, INC.

8-A-24-OB

Consideration of an appeal of the Design Review Board’s decision to approve the request of Ben Hudgins for a Certificate of Appropriateness for a new primary structure located at 0 W Hill Ave. / Parcel ID 094MD018, 094MD022, 094MD024, 094MD025, and 094ME033. Council District 6.

#### 1. DESIGN REVIEW BOARD DECISION

Approve subject to the following conditions:

- 1) Final site plan, pedestrian improvements, and parking garage to meet City Engineering standards;
- 2) Final landscaping plan to meet standards of City zoning code;
- 3) Any mechanical equipment or service utilities not shown on plans to be placed on secondary elevations and receive screening as necessary;
- 4) Signage to return to DRB as a separate application;
- 5) Final material specifications for parking garage screening to be submitted to staff for review.

**Item No.**

**File No.**

2. **MOTION (OVERTON) AND SECOND (LEVENSON) WERE MADE TO DENY THE APPEAL, AFFIRMING THE DECISION OF THE DESIGN REVIEW BOARD.**

**MOTION CARRIED UNANIMOUSLY 14-0. DENIED**

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